

MID-RISE DEVELOPMENT (5 TO 12 STOREYS)

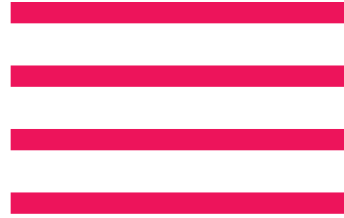
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C4.1 INTRODUCTION



This section addresses the design of Mid-Rise buildings, generally 5 to 12 storeys in height, as defined by Brampton Plan / CZBL and including all forms of Mid-Rise building development, including residential, mixed-use, commercial uses and employment.

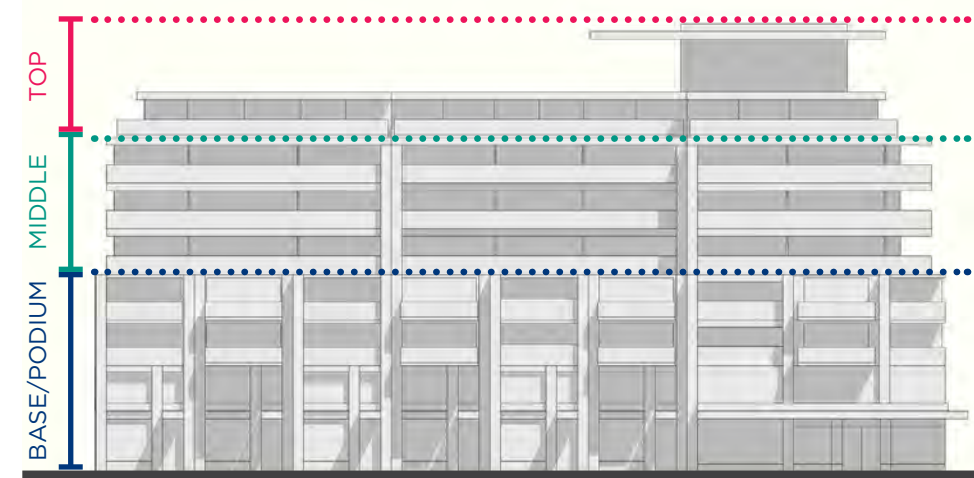
address site planning and built-form conditions common to all Mid-Rise developments. They are organized to include recommendations related to site planning, built form design, and landscape design, ensuring a comprehensive approach to the overall development process.

The guidelines provide a framework for the development of Mid-Rise buildings as individual buildings on a lot and/or part of a larger development (several buildings on a site). The guidelines recognize fundamental principles of good urban design, while allowing for creativity and innovation that responds to the site and use-specific contexts. As such, these guidelines

While Section '4.1.1 Mid-rise Building Components' defines the base/podium, middle, and top portions of mid-rise buildings as referenced in this document, proposals are not required to strictly adhere to these elements in all designs. Each project will be evaluated on a case-by-case basis by Urban Design Staff, considering its specific context.

C4.1.1 MID-RISE BUILDING COMPONENTS

The Top of a building is a combination of the uppermost storey(s) and the rooftop, where typically, mechanical equipment and/or roof gardens are located. Tops should be carefully sized with respect to the building's overall height, and designed to complement their style; where appropriate, the top of mid-rise buildings should be used and designed to highlight important locations within the community.



The Middle component of a building is located between the base/podium and the top. The middle should be distinguished from the base through setbacks, massing/wall articulation and material changes, while complementing the buildings overall design. Depending on the scale of the building, the middle component could include one building or be broken into two or more buildings over a single base/podium.

The base or podium of a building refers to the lower portion above grade, preceding any setbacks to or wall articulation of the middle component. Its role is to ground the building while framing and activating ground-level spaces and the street. Key considerations such as building location, configuration, setbacks, height, scale, and articulation are essential for achieving appropriate architectural expression and defining the pedestrian realm. As the primary interface with the surrounding context, the base or podium should be designed to engage and enhance the public realm

C4.2 DESIGN GUIDELINES

C4.2.1 SITE ORGANIZATION

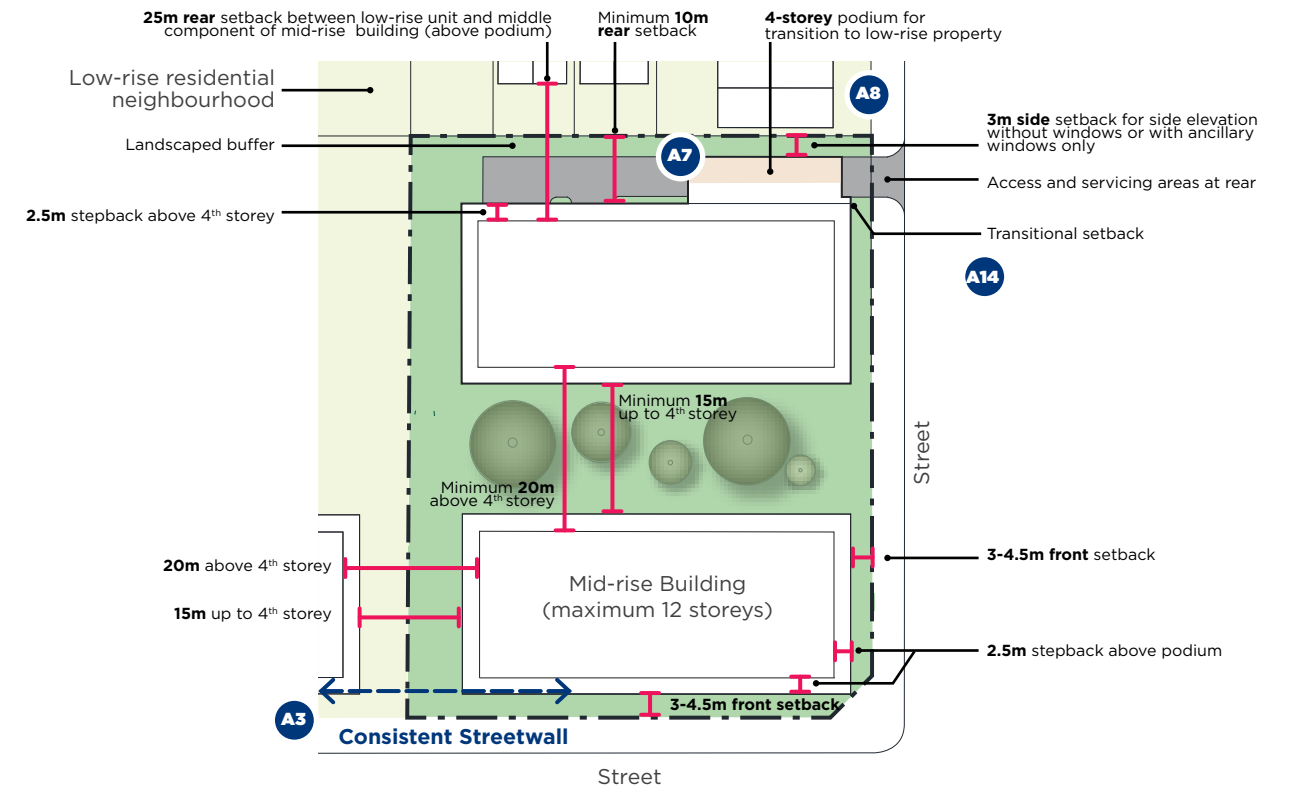
A. ORIENTATION, PLACEMENT AND SETBACKS (A)

For the purpose of this document, a setback refers to the minimum distance that a building or structure must be located from a property line or other designated point.

- 1 Place and orient buildings to maximize view corridors, privacy for residents, opportunities for open/green spaces on site, as well as natural light exposure and ventilation.
- 2 Create strong, well-proportioned and continuous building frontage (streetwall) and enhance the streetscape/public realm through buildings bases (podiums) that:
 - a. Are oriented to and frame the adjacent public realm including streets and open spaces (both streets in the case of corner lots).
 - b. Are placed close to the front property line and/or to reflect the placement of existing adjacent building to form a generally consistent streetwall.



- c. Consider 0m side setbacks when abutting buildings with no windows on their side elevations, especially along commercial corridors, to create continuous streetwalls.
 - d. Are pedestrian-scaled and reflect their context.
- 3 Where there is not a well-established setback pattern along the streetscape, or there are plans to change it, place building to:
 - a. Ensure a minimum front setback of 3m from the property line parallel to the street, to effectively animate the street edge through a seamless connection between interior and public spaces.
 - b. Encourage setbacks between 3m to 4.5m along elevations with residential units at grade, to allow for privacy and appropriate transition between the public and private realms, while also providing 'eyes-on-the-street'.
 - c. Consider greater setbacks for retail uses that incorporate outdoor spaces such as patios, where appropriate.



- 4 Where existing boulevards/sidewalks are narrow, consider greater front setbacks to allow for minimum (AODA) sidewalk widths and a row of street trees within a combined private/public realm.-
 - 5 Where open spaces (private or public) are provided, arrange buildings to face and frame the space, with primary elevations and entrances oriented toward the space.
 - 6 Allow for greater front setbacks for entrances, displays and outdoor places such as cafes, patios and courtyards, to a maximum of 25% of the overall building frontage. Consider moderate front setbacks along retail frontages for outdoor seating or product displays.
 - 7 In order to ensure appropriate transitions in height/massing and privacy for adjacent lower density residential areas, as well as continuous sunlight penetration/exposure on adjacent parks and open space, and natural areas, design new development to:
 - a. Provide a minimum rear setback of 7.5m from the rear property line.
 - b. Provide a minimum rear setback of 10m from the rear property line on sites abutting low-rise residential lots.
- c. Provide a minimum 25m setback from the middle component of the mid-rise building (above podium) to the wall of the adjacent low-rise building on sites abutting low-rise residential lots.
 - d. Provide a minimum rear setback of 6m from the rear property line on sites abutting parks and open space.
 - e. Incorporate new local streets, service lanes, open spaces and/or landscaped buffers along the abutting rear property line and required setbacks, where appropriate and possible.
 - f. Apply angular planes where mid-rise developments are proposed outside of the permitted areas (Brampton Plan), or within sensitive contexts.



- 8 On corner lots that abut existing or planned low-rise residential properties, design podiums to:
 - a. Provide a minimum setback of 3m for the podium where no windows or only ancillary windows are included on the side elevation facing the adjacent low-rise building. If the side elevation has main windows, ensure a podium setback of a minimum of 7.5m from the rear property line.
 - b. Above the 4th storey, ensure the podium follows a 45° angular plane taken from a height of 10.5m at the property line.
- 9 Provide mid-rise buildings that are no more than 80m in length. Site specific exceptions may be considered on a case-by-case basis.
- 10 Provide appropriate building separation distances to ensure adequate privacy and protect/frame views. Provide at least:
 - a. 20m separation distance between residential elevations above the 4th storey that include main windows.
 - b. 15m separation distance between residential elevations up to the 4th storey that include main windows.



- c. 15m separation distance between residential elevations that include ancillary windows only (windows related to servicing areas and not related to main living spaces; usually smaller in size and located on side/narrower elevations).
 - d. 0m for side blank walls (no windows).
 - e. Responsibility for providing separation distances as listed above will be shared equally between owners of all properties.
 - f. 7.5m setback to the side property line for elevations with windows facing a blank wall.
- 11 For mid-rise buildings taller than 6 storeys, design the upper levels above the base/podium to:
 - a. Incorporate smaller floor plates.
 - b. Consider two or more separate structures with small floor plates over a podium structure rather than one large slab for larger or longer sites.
- 12 At gateway or corner sites, consider front setbacks (along both frontages of the intersecting streets) that are either:
 - a. Minimal setbacks to provide an enhanced sense of enclosure to the public realm. Extend the same setback along both elevations.
 - b. Where appropriate and in response to the community's structure, greater setbacks to allow for 'plaza' type, civic spaces uniquely designed to complement the building and provide for focal points in the community.
 - c. In both cases, consider incorporating main entrances at the corner.



- 13 For corner lots, ensure both frontages are treated as main or front elevations with enhanced designs.
 - a. Avoid blank elevations oriented toward the daylight triangle.
 - b. Discourage 'chamfered' building corners.
 - c. Coordinate the building design with landscaping that creates/contributes to a pedestrian-oriented street environment.
- 14 Articulate the elevation of mid-rise developments on corner lots abutting low-rise residential built form to incorporate transitional front setbacks so that:
 - a. The portion of the building closest to the low-rise building generally reflects the front setback of it and is at least 4.5m.
 - b. The portion of the wall closest to the corner (intersection) is allowed to project and sit closer to the property line, within the required minimum front setback.
- 15 Locate taller buildings along primary streets and intersections (corners), and greatest height at the corner of the overall development.
- 16 Design buildings at gateways as landmark buildings, with focus on prominent and distinct built form/massing that is proportionate to the scale of the adjacent public realm.
- 17 Contribute to an active public realm by ensuring that active uses, such as residential, commercial, or live-work units, are located at the ground level of buildings.
 - a. Provide individual direct access from ground-related units to the adjacent public sidewalk.
 - b. Wherever possible and where grading permits, these units should be universally accessible.
 - c. Where direct physical access is not possible, design ground-related units to have a presence on the street through windows, balconies, landscaped areas, etc.



- 18** For larger sites (i.e., with capacity for several blocks/buildings), reinforce and extend a fine-grained public realm and circulation network, as well as the surrounding block configuration structure, if existent and desirable:
- a. Where appropriate, divide larger sites through streets/lanes and/or pedestrian routes that provide for high level of permeability (public circulation).
 - b. Plan and design blocks with a preferred maximum size of 80m by 150m to support excellent pedestrian walkability and permeability, as outlined in the SNCP. Longer block lengths, generally up to 250m, may be considered where they continue to support a connected and walkable public realm, such as through the inclusion of a mid-block park or other pedestrian-oriented amenities. 🌿
 - c. Encourage high-quality mid-block pedestrian connections, with or without vehicular access, where block lengths exceed 150m. These connections should be sufficiently wide (minimum 6m) to accommodate all types of users (cyclists and pedestrians), incorporate proper lighting and AODA access, and be lined with active uses for natural surveillance. 6m correspond to the width of the mid-block connection and not to the separation distance between buildings. 🌿
 - d. Connect new pedestrian routes and open spaces (public or private) to the broader neighbourhood and the existing adjacent network of sidewalks, trails and parks and open spaces. 🌿

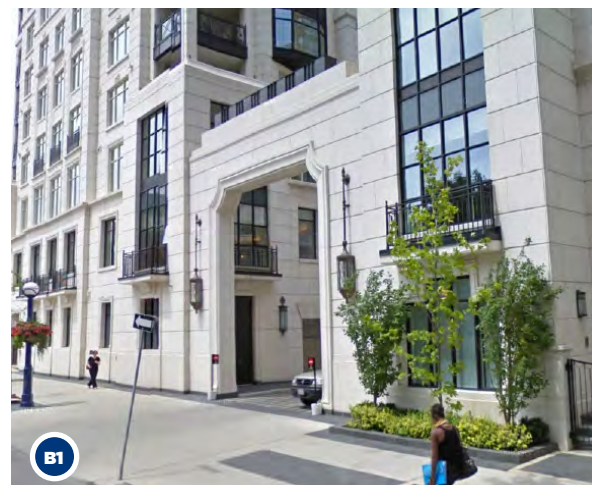


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- e. Execute both public and private streets and pathways to a high degree of quality, in terms of materials, and overall design, exceeding municipal standards.
- f. The City' BCSG should be referred to for guidance on the design of public/private local roads and lanes.

B. ACCESS, PARKING AND SERVICING (B)

- 1** Minimize the presence and impact of parking servicing and loading areas on the public realm by:
 - a. Providing parking underground.
 - b. If underground parking is not feasible, consider, above-grade structures lined/ wrap by occupied-active spaces, or well-screened surface parking at the rear or side of the building.
 - c. Incorporating servicing areas (including waste storage and recycling areas) and ramps preferably within the building. Where this is not feasible, locate such areas at the rear or side of the site/ building, screened from public view. 🌿



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- d. Screening at-grade parking and servicing areas through a combination of architectural and landscape elements. Ensure screening structures complement the design of the building and incorporate similar, complementary materials.
- e. Do not place parking areas along front setbacks.

- 2** Provide access to parking and service areas from lanes and/or secondary/side streets, where possible, and away from corners.
- 3** Minimize the visual and functional impact of multiple vehicular accesses on the public realm, including multiple curb cuts within a small area and frequent interruptions of the building street wall.
 - a. Wherever possible, prioritize the consolidation of vehicular accesses (shared driveways), on site and/or across several sites/properties, either at the rear (lane configuration) or along the shared property line.
 - b. Reduce the access driveway width at the property line.



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- 4** Integrate vehicular entrances into the elevation design and de-emphasize them through recessed walls and doors that complement the overall design.
- 5** Where the consolidation of vehicular accesses is not possible, locate individual site access driveways either:
 - a. One beside the other (parallel configuration), to minimized curb cuts; coordinate their design as one element; or,
 - b. As far apart as possible, to maximize the space between curb cuts (continuous, safer streetscapes).
 - c. In both cases, enhance pedestrian safety through traffic calming measures including a combination of enhanced pedestrian and driveway pavement at the throat of the access driveway, additional signage, enhanced landscaping elements, etc.
- 6** Avoid mid-block vehicular access. Where unavoidable, access points should be shared by residents and servicing, and integrated into the building massing (refer to the City's BCSG for 'Lanes').



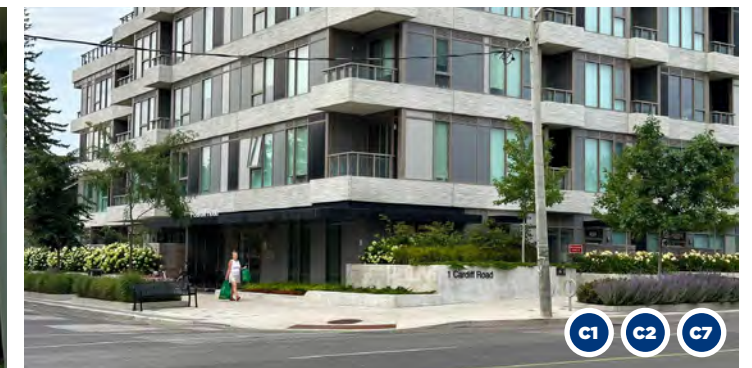
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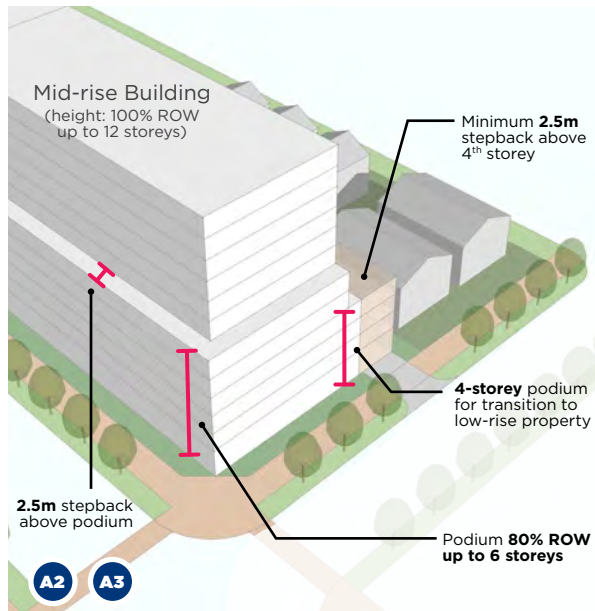
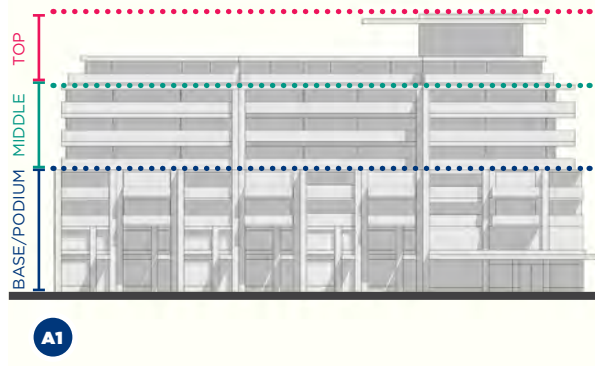
- 7 Ensure continuous pedestrian connectivity (e.g., sidewalks) by clearly delineating/differentiating linkages crossing driveways and shared lanes.
- 8 Locate passenger pick-up/drop-off areas internal to the site, and/or at the rear or side of buildings.
- 9 Cover ramps to underground parking to prevent long-term weather damage.
- 10 Minimize surface parking. Where necessary, design surface parking areas to:
 - a. Minimize their environmental impact by reducing parking lot/garage size.
 - b. Consider shared parking facilities with adjacent buildings and provide preferential parking for fuel efficient vehicles.
 - c. Disperse surface parking throughout the site and/or split it into parking courts.
 - d. Incorporate clearly delineated and landscaped pedestrian walkways and crossings which are buffered from parked and moving vehicles.
 - e. Incorporate landscaped medians.
 - f. Incorporate minimum 3m width buffers around the edges.
 - g. Include tree planting within islands and buffers to increase tree coverage/shading and to reduce heat island impact.
 - h. Use bioswales and permeable paving materials.
 - i. Reduce heat island effect through light materials and canopy coverage.
 - j. Address LEED sustainability standards and the City of Brampton Sustainable Community Development Guidelines.
- 11 Provide preferential parking for bicycles, car sharing and alternative energy vehicles.
- 12 Provide ample, accessible, secure bicycle parking and supporting facilities. Refer to C2.1.1 General Guidelines for additional guidelines regarding bicycle facilities.
- 13 Locate storage rooms/units interior to the building, preferably, and on elevations away from public frontages. Ensure walls related to these areas do not encroach into front or exterior yards, or project from the main wall of the building.

C. LANDSCAPING AND COMMON AMENITY AREAS (C)

- 1 Ensure a comprehensive landscape approach that supports the immediate adjacent uses and includes:
 - a. An overall planting strategy.
 - b. Hardscape areas related to entrances.
 - c. Shaded and sitings areas, where appropriate.
 - d. Consistent, high quality landscape elements such as planters, pavings, fences and walls.
 - e. Coordinated, high quality street furnishings, such as lighting, benches, bollards, bike racks and garbage receptacles.
 - f. Landscape elements and plantings that mitigate micro-climatic impacts.
- 2 Coordinate the landscaping between private and public areas, ensuring that it:
 - a. Enhances the character of the development and the community.
 - b. Complements the building uses at grade.
 - c. Reinforces the structure, nature and use of the site with a focus on creating safe, comfortable and animated pedestrian environments (streets, edges, corners, gateways, transitions, public spaces, building entrances, etc.)
 - d. Incorporates high-quality landscaped areas and element associated with main entrances and/or walkways.
 - e. Coordinates hard and soft landscape elements, special paving materials, site furniture and pedestrian lighting.
- 3 Locate common amenities and open spaces away from areas of high vehicular activity and from servicing, garbage storage and loading areas.
- 4 Pair indoor and outdoor common amenity areas, and design/program such spaces to serve the needs of all age groups and to consider all four seasons.
- 5 Where possible and appropriate, encourage the creation of common spaces such as POPS and mid-block connections, to promote connectivity/permeability, and to reinforce a sense of place. Consider mid-block connections between buildings and/or through covered building arcades/lobbies.
- 6 Design open spaces, pathways and mid-block connections with safety in mind, including active frontages, adequate lighting and visible security features.
- 7 Enhance the urban forest with the use of a diverse range of canopy trees; ensure they are hardy, tolerant, climate resilient and high-branching.
- 8 Provide fully planted landscape strips (minimum 3m wide) to screen parking, service, loading areas from adjacent uses and public view.



Refer to chapter C2 for additional guidelines regarding Private Open Space, Amenity Areas, Lighting, Accessibility, Safety, Public Art, Built Heritage Context, Bird-Friendly Design, and Garbage and Recycling.



Mid-rise development adjacent to low-rise residential built form



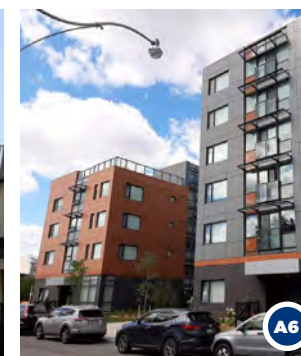
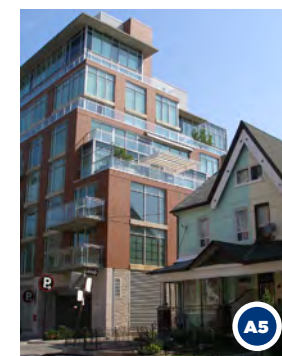
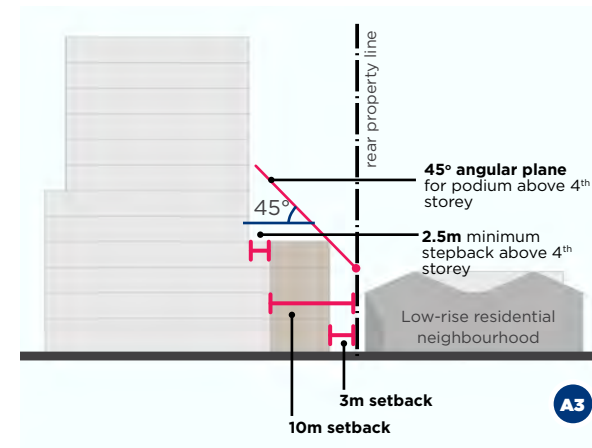
C4.2.2 BUILT FORM

A. HEIGHT AND MASSING (A)

- 1 Design buildings cohesively in terms of massing, proportions, architectural style, rhythm and materials, while clearly differentiating between the base/podium, middle and top components of the building through massing articulation, strategic use of materials and varied but complementary fenestration.
- 2 Achieve pedestrian oriented environments by providing building heights and streetwalls that are proportionate to the width of the street.
 - a. Ensure the maximum building height matches the adjacent ROW, up to a limit of 12 storeys.
 - b. Ensure the maximum base height (podium) is no greater than 80% of the width of the adjacent ROW, or 6 storeys, whichever is less.
 - c. Provide a building stepback of a minimum of 2.5m at the podium (between the 2nd and 6th storey on the front and flankage elevations. Refer to 4.2.2.B for alternative treatments.
 - d. For mid-rise buildings abutting low-rise residential built form or parks and open spaces, provide a building stepback of a minimum of 2.5m above the 4th storey on the elevation facing these uses.
- 3 For corner developments fronting onto a major and a local street, consider transitional podium heights. Ensure:
 - a. Podium height is within the maximum height related to the adjacent ROW.
 - b. Height transition/change is incorporated along the local road, preferably, and at logical places such as changes in planes related to setback variations.
 - c. Allow for higher podiums related to the major street frontage to wrap around the corner and extend along the local street for a distance of between 20% to 30% of the length of the building elevation.

- d. For buildings abutting existing or planned low-rise residential properties, allow podiums of maximum 4 storeys to extend closer to the low-rise building to provide for similar massing along the street frontage. Above the 4th storey, the podium should follow a 45° angular plane taken from a height of 10.5m at the property line. Refer to guideline 4.2.1.A.8 for information on setbacks.
- 4 On a site specific basis, depending on the context, the City may consider floor plate controls on floors above 6 storeys on right-of-ways 26m or less, and floors above 9 storeys on right-of-ways over 26m.
- 5 Provide appropriate transitions in height and massing toward lower adjacent stable neighbourhoods by:
 - a. Limiting base (podium) height to 2 floors above the overall height of the adjacent low-rise development.
 - b. Incorporating setbacks at various levels above the base/podium and towards existing lower built form.
 - c. Terracing building down towards the adjacent low-rise development, where appropriate.

- 6 For developments/blocks with more than one building, encourage a range of heights (variation) is provided and establish a height hierarchy/strategy related to site conditions and context (existing and planned).
- 7 Ensure the height of ground floors (floor to ceiling) is minimum 4.5m to enhance visibility and safety, while strengthening the relation between internal uses and the adjacent public realm.
- 8 Design mid-rise infill developments to:
 - a. Incorporate a base/podium that generally reflects that of other adjacent mid-rise buildings and/or the overall height of adjacent low rise structures.
 - b. Locate the tallest component of the building (or buildings) away from any adjacent pre-existing low density areas to avoid visual crowding and adverse shadow impacts.
- 9 For mid-rise buildings adjacent to or within heritage contexts, maintain a consistent cornice line and ground floor height.





10 Provide visual interest and emphasize corners, street intersections and view terminus through prominent massing, additional height, and enhanced architectural design/treatment, including projecting/recessed components, wrap around elements, substantial fenestration and unique roof/top designs.

11 Consider buildings of equal or similar scale and architectural style at gateways to emphasize the sense of entry and enhance place-making.

B. ARCHITECTURAL DESIGN AND BUILDING ARTICULATION (B)

1 Design floor plans to accommodate the building's program and to break its mass, creating/contributing to interesting and articulated building elevations.

2 Design the building elevations to:

- a. Be articulated, both vertically and horizontally, through changes in planes and materials, stepbacks, ample fenestration (windows and balconies), bands, as well as other types of architectural details.
- b. Display the same architectural style, proportions and materials; however, the level of detail might differ in relation to each elevation's exposure to the public realm.
- c. Include break in wall planes at least every 55m (long elevations).
- d. Include active uses, fenestration and articulated walls along proposed mid-block connections (publicly accessible walkways).

3 Promote high quality and safe pedestrian environments through articulated and animated podium elevations along streets and public places. Provide:

- a. Active uses along elevations facing the public realm such as commercial/retail units, residential and live/work units, amenity spaces, lobbies, porches, etc.
- b. Articulated elevation walls, including projections, and recessions that create a rhythm of minor breaks and reflect the different uses and interior program components (i.e., entrances, rhythm of residential/retail units, private amenities - balconies). Favour wall breaks at least every 10m.
- c. Highest level of architectural details and materials.
- d. High proportion of glazing at the ground level (visual permeability). For frontages including entrances, lobbies, common amenities or commercial uses, aim for at least 75% clear glazing; darkly tinted glass is not permitted.

4 For longer buildings, especially those exceeding 80m, elevation design should incorporate:

- a. Major multi-storey vertical massing breaks, ideally ground to top, of minimum 6m width by 2m deep, and generally every 55m.
- b. Highly articulated elevations that include changes in planes.
- c. Double-height mid-building/block connections, where appropriate.

5 For mid-rise buildings under 6 storeys, maintain the established streetwall or introduce a new one that enhances a pedestrian-scaled streetscape. Achieve this through podiums defined by a 2.5m stepback; alternatively, consider wall articulation and architectural features that create a physical and/or visual break between the 2nd and 4th storeys; consider:

- a. A material change within the same plane.
- b. A cantilevered podium.
- c. A minor or transitional stepback of at least 1.5m.





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- 6 Mid-rise buildings that are taller than 6 storeys and do not include defined podiums will be assessed on a case-by-case basis. The assessment will consider their contextual fit and design, including:
 - a. A clearly defined streetwall within the first 4 storeys of the building that provides for an enhanced pedestrian-oriented interface. This may be achieved through architectural details, wall articulation and materials.
 - b. A distinctive top component.
 - c. Vertical massing breaks and highly articulated elevations, as outlined in guideline B4.
- 7 For mid-rise buildings taller than 6 storeys, ensure the upper levels above the base/podium incorporate significant, multi-storey massing breaks.
- 8 For corner buildings, design both elevations to display the same level of wall articulation, architectural detail, level of fenestration and quality of materials.

- 9 Design gateway buildings to be of the highest architectural quality.
 - a. Provide special attention to the design of the (podium/streetwall) and top components.
 - b. Include active uses at grade.
 - c. Incorporate enhanced fenestration to animate the elevation and reinforce the community character at these locations.
 - d. Provide and design landscape planting and features to accentuate gateway locations.
- 10 For side elevations exposed to public view, continue frontage features such as windows, wall articulation and materials to avoid blank walls on side streets, lanes and/or walkways.
- 11 Design mid-rise developments within existing built contexts, or those adjacent to or within heritage contexts, to:
 - a. Take cues from neighbouring buildings in terms of scale, wall articulation (rhythm of projections/recessions), and fenestration proportions and placement.
 - b. Incorporate compatible building materials.
- 12 Blank walls are prohibited on elevations directly visible from streets or public areas.
- 13 Where blank walls are unavoidable, mitigate their presence through:
 - a. Wall articulation (i.e., recesses and projections).
 - b. Art such as installations, paintings, sculptures, lighting, etc.
 - c. Special wall treatments such as screens (e.g., perforated metallic panels), green walls, metallic/wooden textures, etc.
 - d. A combination of the above.
- 14 Design building and storefront signage to complement the building's elevation, animate the ground level and enhance the overall character of the streetscape.
 - a. Integrate signage to the elevation design.
 - b. Incorporate signage bands for multi-store frontages.
 - c. Design signage in conjunction with weather protection elements where these are proposed.
 - d. Ensure complementary materials and lighting.



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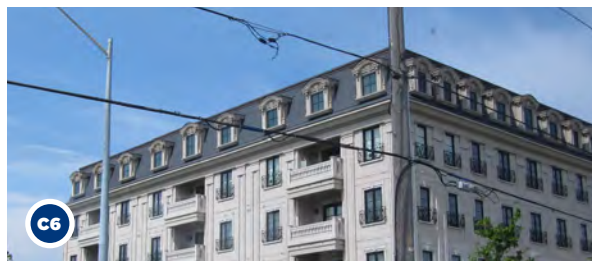
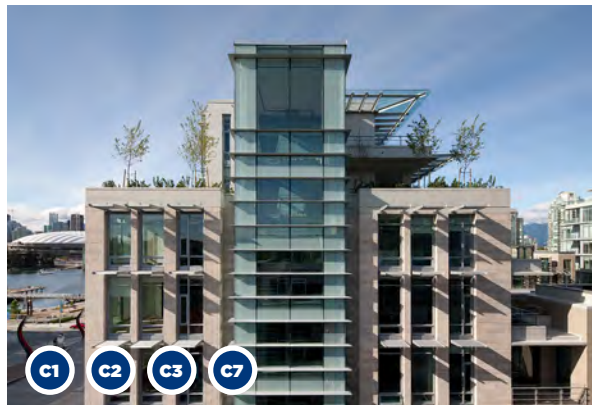
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C. ROOFS (C)

- 1 Create a consistent and visually appealing roofline, while maintaining an adequate view of the sky from ground level.
- 2 Design the top component of the building to be clearly discernible, of consistent architectural style to that of the building and proportionate/complementary to the overall building scale.
- 3 Integrate mechanical penthouse and rooftop elements/spaces into the overall building design, avoiding the appearance of an afterthought or add-on element.
- 4 Ensure mechanical penthouses are located away from the top edge, and screened from public view through structures complementary to the building design and or amenity spaces.
- 5 Favour reflective, low intensity colours to reduce heat island effect, and reduce HVAC loads. 🌿
- 6 In the case of mansard or peaked roofs, use exceptionally high quality and visually appealing roofing materials. Choose a colour that contrasts and complements that of the building elevation.
- 7 Provide green rooftops as a way to enhance the building appeal from the street, reduce urban heat island effects and improve air quality and noise insulation. Only consider green rooftops where planting could thrive. 🌿
- 8 Encourage incorporating white roofs and solar panels, where possible/feasible. 🌿



D. ENTRANCES, WINDOWS AND BALCONIES/ TERRACES (D)

- 1 Strategically locate and orient main pedestrian entrance(s) to address the public realm. Where a transit stop is located within 100m of the proposed development, place and orient the main entrance to provide direct and convenient access.
- 2 Ensure entrances are directly accessible and connected to the adjacent pedestrian network of sidewalks and trails through clearly delineated walkways and/or entry areas.
- 3 Design main pedestrian entrances to be highly visible, prominent and focal elements on the elevation by:
 - a. Being properly scaled in relation to the overall main elevation and building massing.
 - b. Incorporating articulated massing including cantilever configurations or projecting elements .
 - c. Incorporating weather protection elements such as canopies.
 - d. Providing high level of glazing along lobby areas.
- 4 Consider locating and designing main entrances in conjunction with covered mid-block connections.
- 5 Make entrances universally accessible.
 - a. Encourage entrances that are at the same level of the adjacent sidewalk.
 - b. Where ramps are unavoidable, integrate them seamlessly into the design of the building.
- 6 If secondary entrances are required (e.g., where parking or common amenities are located on the rear or interior frontages, or along mid-block/building connections and courtyards), incorporate these entrances into the elevation design and highlighted through architectural details such as glass doors and canopies. Consider through lobbies/halls connected to the main entrance.



- 7 Use and design entrances to ground related units (residential or commercial) to emphasize individual units and further animate and articulate the streetwall. Highlight entrances through architectural elements such as porches, canopies, awnings, as well as steps or stoops, and enhanced landscaped spaces designed to complement the unit's use.
- 8 Add visual variety and interest to the building elevation, and enhance inside-to-outside connections by incorporating windows, bay windows, balconies, and outdoor terraces.
 - a. Ensure all elevations exposed to public view include windows, and, in the case of residential uses, also incorporate balconies.
 - b. Consider different but proportionate window sizes, and varied but complementary treatments to animate the elevations, reflect internal uses, and further differentiate various elements of the design.
 - c. Integrate these elements into the overall shape and design of the building.
 - d. Consider slight wall recesses to accommodate projecting elements without encroaching into the ROW.



- 9 Maintain balcony projections within the lot.
- 10 Allow balconies to encroach in minimum setbacks and separation distances between buildings, while not contributing excessively to the building massing.
- 11 Provide functional and programmable balconies.
 - a. Ensure they are a minimum of 1.5m depth and 3m² in area, to provide sufficient space for chairs and a small table.
 - b. Include weather protection if possible.
- 12 Incorporate recessed balconies, fully or partially, to provide for greater privacy and shelter from wind, reduce the building bulk, and minimize the impact of shadow on other amenity spaces below.
- 13 Incorporate architectural treatments to the undersides of balconies visible from the public realm, such as attractively revealed support beams or complementary materials such as wood.
- 14 Ensure a consistent canopy/awning style throughout the building, and select colours/materials that complement those of the building.



E. UTILITY/SERVICE METERS AND MECHANICAL EQUIPMENT/ROOMS (E)

- 1 Locate utility/service elements/meters away from public frontages and/or screened from public view, interior to the building (internal rooms) and/or integrated unobtrusively into the elevation design.
- 2 Consolidate natural gas and other utility meters wherever possible. Plan for their placement early and integrate them into the building form.
- 3 Locate mechanical equipment/rooms away from public frontages/view, to the center of the rooftop and screened through architectural features/structures or rooftop amenities.
 - a. Use integral architectural features to screen rooftop mechanical elements rather than single-purpose screens. Ensure they are made of materials that complement those used on the building elevations.
 - b. If visible, ensure the mechanical room's exterior structure complements and enhances the design of the building top.
 - c. Where possible, use and design usable spaces (i.e., amenity or living areas) to screen mechanical rooms.
- 4 For District Energy, integrate at-grade elements with site design (i.e., pipes, meters, etc.).
- 5 Clearly identify utility locations and treatment on site plan and elevation drawings.



F. MATERIALS AND LIGHTING (F)

- 1 Select high quality, sustainable and durable materials that complement and enhance the building design, as well as the neighbourhood character and adjacent streetscape. Natural materials such as stone, brick and glass are strongly encouraged.
- 2 Use the highest quality materials at the building base, adjacent to the public realm and pedestrian areas, to create a visually appealing and functional urban environment, while contributing to its durability, safety, and sustainability.
- 3 Avoid the use of materials that imitate another natural/more expensive materials.
- 4 Ensure materials are consistent among elevations.
- 5 Strategically use lighter materials to minimize the building mass, and heavier ones to emphasize important elements of the building design and its articulation.
- 6 Encourage lighter materials and colour palettes on levels above 6 storeys to mitigate the impact of taller mid-rise buildings.
- 7 Enhance vertical breaks and changes in plane through different materials, especially for buildings with long elevations that might otherwise become monotonous.





- 8 Ensure changes in materials are provided at changes in planes.
- 9 Lighting should complement the elevation design and reflect the uses on it. Incorporate high efficiency lighting (LED) wherever possible.
- 10 Meet LEED standards and the City of Brampton Sustainable Community Development Guidelines. Energy efficient measures and materials are strongly encouraged. 🌿



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