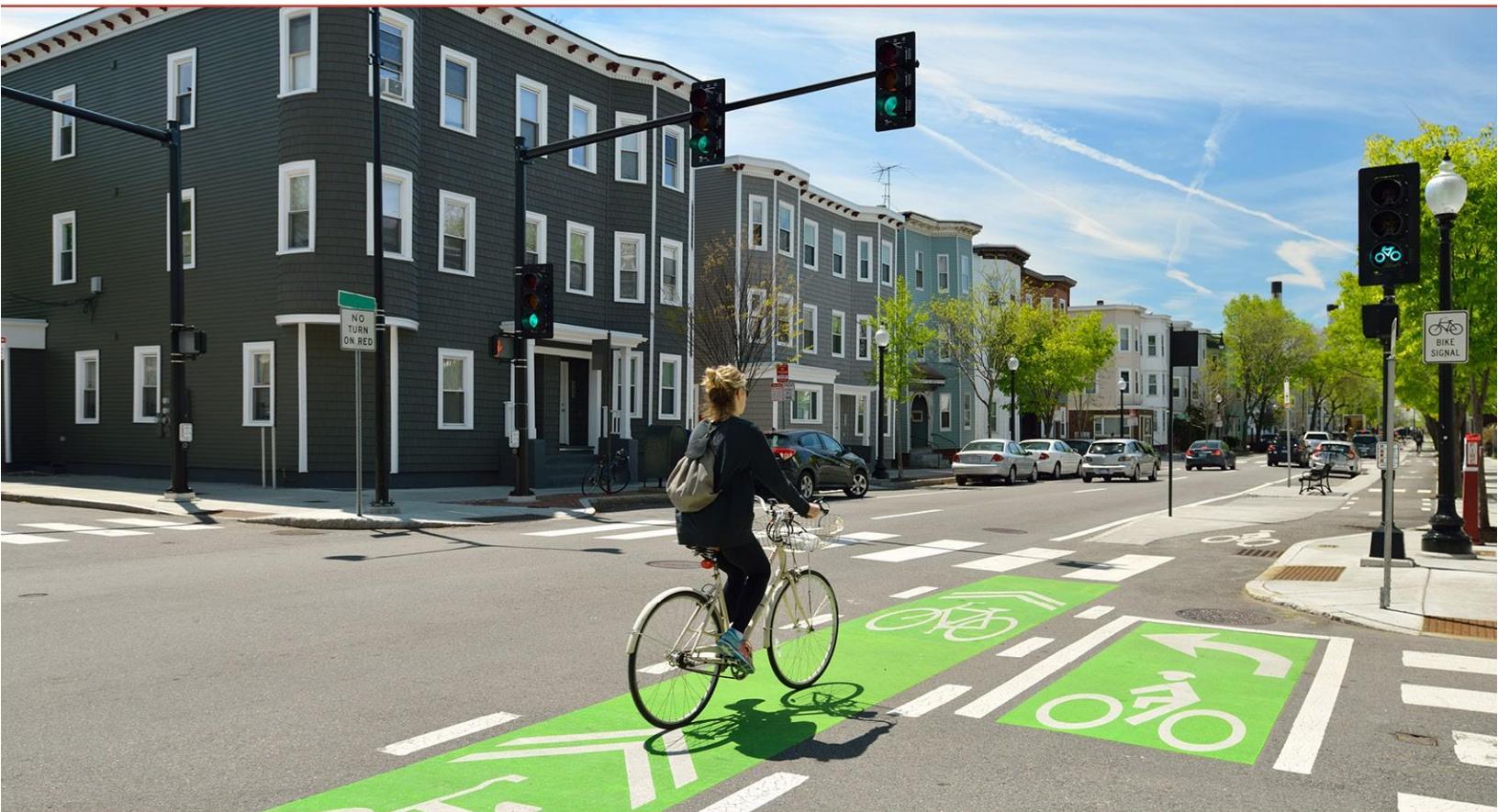


APPENDIX B: ENGAGEMENT SUMMARY

Heritage Heights Transportation Master Plan



February 2026

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- 1.0 INTRODUCTION**
- 2.0 NOTICES**
- 3.0 CITY OF BRAMPTON TECHNICAL ADVISORY ENGAGEMENT**
- 4.0 EXTERNAL TECHNICAL ADVISORY ENGAGEMENT**
- 5.0 ENGAGEMENT WITH INDIGENOUS COMMUNITIES**
- 6.0 PUBLIC CONSULTATION EVENT**
- 7.0 PLANNING AND DEVELOPMENT COMMITTEE MEETING**



1.0 INTRODUCTION

Engagement for the current Heritage Heights TMP has built on an extensive history of consultation and collaboration that has occurred from former iterations of the Secondary Plan and their related Transportation Master Plans, through mediation of the approved Secondary Plan, and as part of the current TMP and related EA process.

Engagement for the current Heritage Heights TMP included various stakeholders, Indigenous communities, and the public, and were undertaken at various phases of the study and through various methods. Community engagement and collaboration is one of the council-endorsed guiding principles that direct the planning of the future Heritage Heights transportation system.

This Appendix provides a summary of the engagement that was undertaken and feedback received over the study period for the Heritage Heights TMP.

2.0 NOTICES

The City of Brampton's communications team lead the public notification campaign. Notices and advertisements through various media and social media were used to inform the public of the study and opportunities to provide input through a public meeting.

Below in **Table 1**, a summary of the public notice and advertisement campaign is provided.

Table 1 Public Notice and Advertisement Campaign Summary

Notice	Media	Date First Published
Notice of Commencement	City of Brampton website Brampton Guardian	May 26, 2025
Notice of Public Information	City of Brampton website City of Brampton social media channels Brampton Guardian articles	August 6, 2025

Copies of the public notices are provided below.

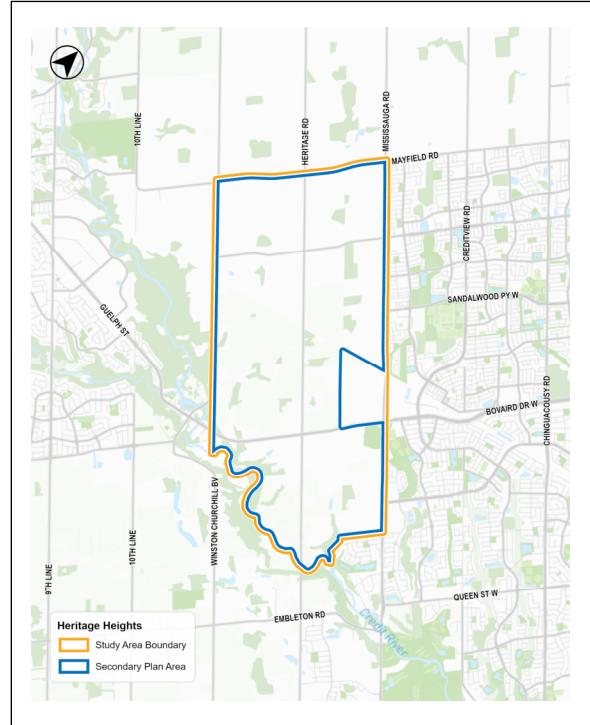


NOTICE OF STUDY COMMENCEMENT

Heritage Heights Transportation Master Plan

The Study

The City of Brampton is experiencing significant growth including in the Heritage Heights Secondary Plan Area. The City of Brampton and the Heritage Heights Landowners Group are working together as co-proponents to undertake a Transportation Master Plan (TMP) to identify the transportation infrastructure required to support the growth envisioned for the Heritage Heights Secondary Plan Area (see figure for study area). The TMP will provide the long-range planning necessary to identify needs, be active transportation (e.g., cycling, trails) friendly and transit supportive, promote mobility, and provide a planning framework that is compatible with new and existing communities and keeps pace with future growth.



The Process

The TMP is being undertaken in accordance with Approach #2 of the Master Planning Process, as outlined in the Municipal Class Environmental Assessment (MCEA) (February 2024). As such, the TMP will address Phases 1 and 2 of the MCEA process for any Schedule B projects and will form the basis for the recommended Schedule C transportation infrastructure projects identified within the TMP report.

We want to hear from you

Public and stakeholder consultation is a key component of the MCEA process. We look for your input to guide the direction of the TMP, gain an understanding of the concerns and needs of stakeholders and help in the selection of the preferred transportation network for the Heritage Heights Secondary Plan Area. Consultation activities will include one (1) Public Information Centre (PIC) during the study which is anticipated to be held in summer 2025. Information on the venue and time will be posted on the City's website (www.brampton.ca/heritageheights) and notices sent to stakeholders to notify them of the event.

For more information about this TMP or to be added to the study mailing list see:

www.brampton.ca/heritageheights or send an email to heritageheights@brampton.ca

Richa Dave, MCIP RPP
Project Manager, Transportation Planning
City of Brampton
T: 905-874-3491

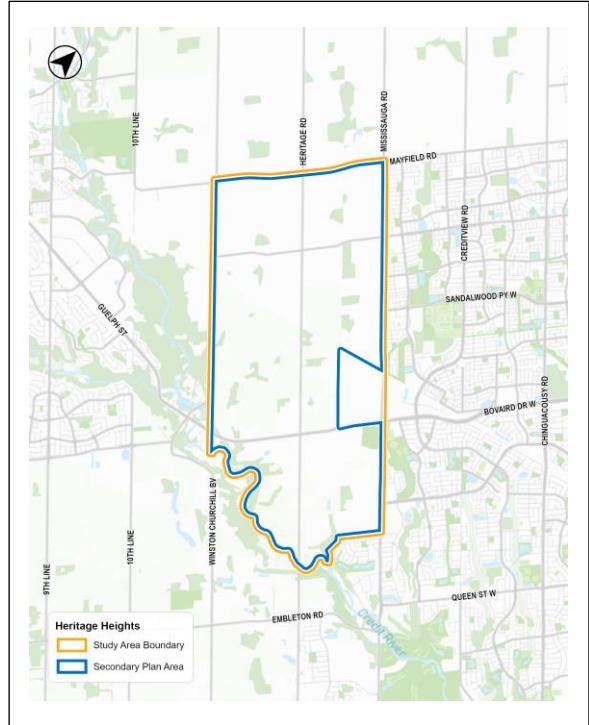
Emily Ecker, P.Eng.
Senior Associate
BA Consulting Group Ltd.
T: 416-961-7110 x138

This notice was first issued on May 26, 2025.

Personal information – such as an individual's name plus address or telephone number – is collected under the authority of the Environmental Assessment Act for the purposes of carrying out a Municipal Class Environmental Assessment in accordance with the Freedom of Information and Protection of Privacy Act. Personal information will become part of a public record that is available to the general public unless you request that your personal information be confidential.

The Study

The City of Brampton is experiencing significant growth including in the Heritage Heights Secondary Plan Area. The City of Brampton and the Heritage Heights Landowners Group are working together as co-proponents to undertake a Transportation Master Plan (TMP) to identify the transportation infrastructure required to support the growth envisioned for the Heritage Heights Secondary Plan Area (see figure for study area). The TMP will provide the long-range planning necessary to identify needs, be active transportation (e.g., cycling, trails) friendly and transit supportive, promote mobility, and provide a planning framework that is compatible with new and existing communities and keeps pace with future growth.



The Process

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Public Information Centre

Public and stakeholder consultation is a key component of the TMP process. As part of the TMP, the Public Information Centre (PIC) will be undertaken virtually and will allow interested members of the public and stakeholders an opportunity to provide comments on the transportation alternatives (including selection of the preliminary preferred transportation network for the Heritage Heights Secondary Plan area), the evaluation process and next steps in the TMP process.

When: August 20, 2025, 7-9pm

Format: Virtual Presentation with Q&A Period

How to Join: See www.brampton.ca/heritageheights

Display boards in a PDF format will be made available to the public and stakeholders on the **City's website** (www.brampton.ca/heritageheights) starting **August 20, 2025**. Recording of the presentation will be posted on the City's website shortly thereafter.

Please submit any additional comments on the PIC material or further project requests by email to heritageheights@brampton.ca by **September 3, 2025**. Any input received by that date will be taken into consideration as part of the TMP report, which will be available for public comment when the TMP is completed.

This notice was first issued on August 6, 2025.

Personal information – such as an individual's name plus address or telephone number – is collected under the authority of the Environmental Assessment Act for the purposes of carrying out a Municipal Class Environmental Assessment in accordance with the Freedom of Information and Protection of Privacy Act. Personal information will become part of a public record that is available to the general public unless you request that your personal information be confidential.

3.0 CITY OF BRAMPTON TECHNICAL ADVISORY ENGAGEMENT

Engagement between representatives of the co-proponents of the Heritage Heights Transportation Master Plan (TMP) (i.e. the Heritage Heights Landowners Group and City of Brampton) occurred throughout the development of the TMP.

Included among this engagement are the following:

- a comprehensive workshop series for development of new collector and local street cross-section standards to be universally applicable across the Heritage Heights Secondary Plan area;
 - workshop dates included:
 - November 20, 2024
 - December 4, 2024
 - December 18, 2024
 - January 8, 2025
 - January 16, 2025
 - January 22, 2025
 - April 9, 2025
 - the workshop series culminated in the design of cross-section standards agreed upon by the co-proponents that were brought before the City of Brampton Standards Committee for approval, which occurred on November 5, 2025.
- coordination with City finance and development departments on capital works projects and implementation measures; and,
- continued collaboration on emerging Precinct planning, Provincial planning objects for Highway 413, and preliminary findings of the TMP to identify opportunities, risks, and to integrate objectives.

Consistent coordination with the following City of Brampton groups and agencies has occurred through the development of the Heritage Heights TMP:

- Recurring coordination with City of Brampton Transportation Departments – Transit, Active Transportation, Engineering, and Planning.
- Recurring coordination with City Interdepartmental Groups – Environment, Standards Committee.



4.0 EXTERNAL TECHNICAL ADVISORY ENGAGEMENT

Technical advisory meetings took place with staff representatives of external stakeholders as listed below in **Table 2**.

Table 2 External Technical Advisory Engagement

External Agency	Meeting Date(s)
Halton Region	May 14, 2025 June 24, 2025
Town of Halton Hills	May 14, 2025 June 24, 2025
Peel Region	May 16, 2025
Town of Caledon	June 9, 2025

Meeting minutes are included in subsequent pages.

In addition to these groups, the following external stakeholders were also invited in advance of public engagement to attend and comment on the Preliminary Preferred Network Alternatives:

- Ministry of Transportation (MTO)
- Credit Valley Conservation Authority (CVCA)
- Metrolinx
- CN Rail.

An External Agency Engagement Session, in the format of a virtual “drop-in” session was held on August 14, 2025. Each of the above groups were invited to the session, to provide opportunity to the external agencies to provide comments on the transportation alternatives including selection of the preliminary preferred transportation network for the Heritage Heights Secondary Plan area, the evaluation process and next steps in the TMP process for the Heritage Heights Transportation Master Plan (HHTMP). Presentation slides are attached below.

Finally, the Heritage Heights TMP project team also attended a workshop and follow-up meetings held by the Highway 413 project team in an effort to coordinate Provincial and City objectives.





HERITAGE HEIGHTS TRANSPORTATION MASTER PLAN

East-West Connection Focus Area
Engagement #1

Prepared For: Halton Hills, Halton Region

May 14, 2025



*Heritage Heights
Landowners Group*



- Introductions
- Heritage Heights Secondary Plan and Policies Related to Halton Peel Boundary
- Transportation Master Plan (TMP) Process Overview
- Overview of Halton Peel Boundary Area Transportation Study (HPBATS) Findings
- Identify Expectations for Next Steps

- City of Brampton
 - Vikram Hardatt
 - Richa Dave
 - Brian Lakeman
 - Henrik Zbogar
- Heritage Heights Landowners Group
 - James Reed
- BA Group
 - Emily Ecker
 - Michael Giallonardo
 - Adrian Lorion
 - Scott Gibbons (regrets)
 - Yahya Deen
- Halton Hills
 - Maureen Van Ravens
 - Jeff Markowiak
 - Ivan Drewnitski
 - Josh Salisbury
- Halton Region
 - Shelley Partridge
 - Walter Scattolon
 - Andrew Morgan
 - Ann Larkin
 - Lina Elmorshedy

Heritage Heights Secondary Plan

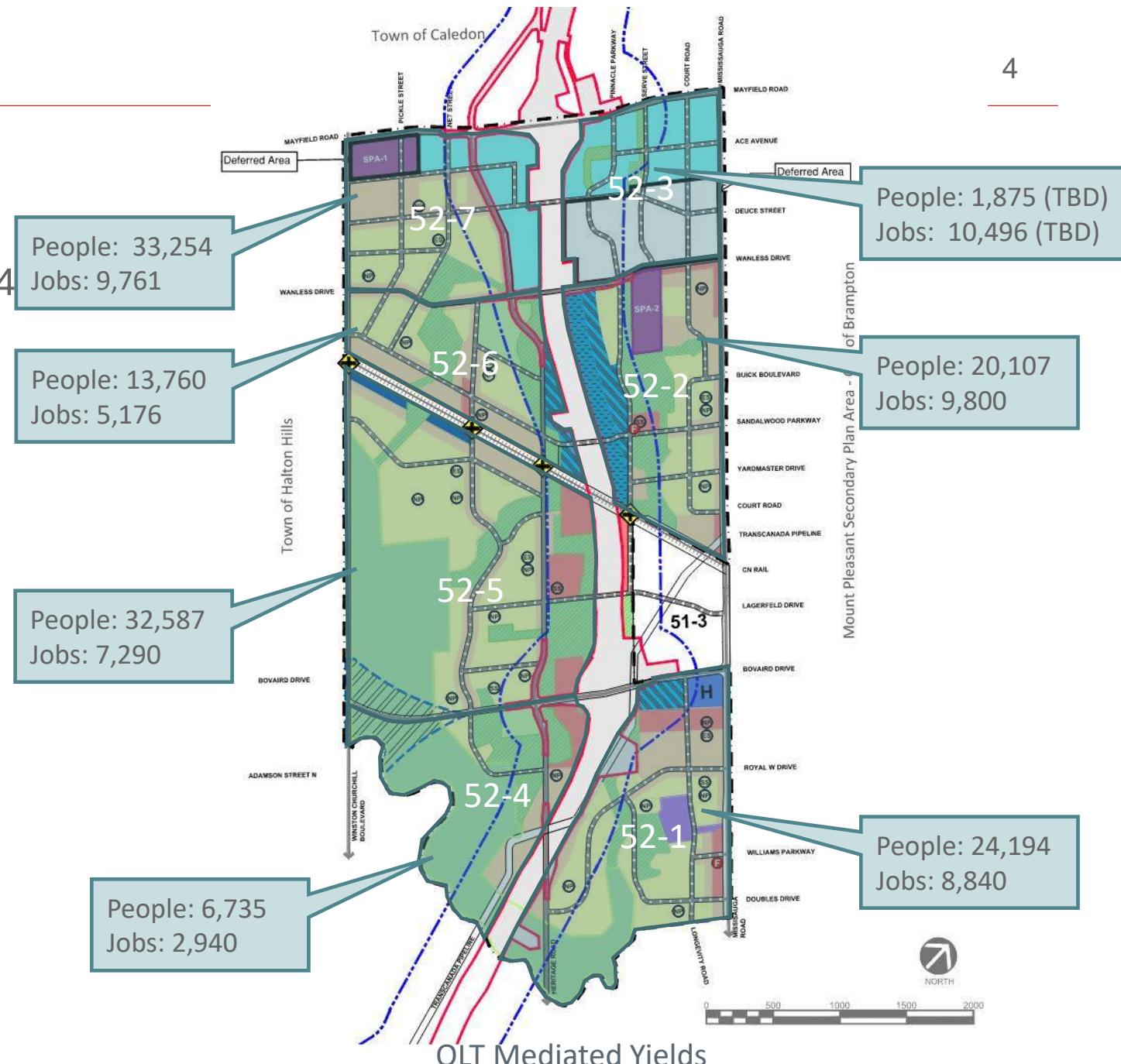
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The Secondary Plan

- Secondary Plan Approved August 21, 2024

Supporting Studies Underway

- Infrastructure Servicing Study (ISS)
- Subwatershed Study (SWS)
- Transportation Master Plan (TMP)

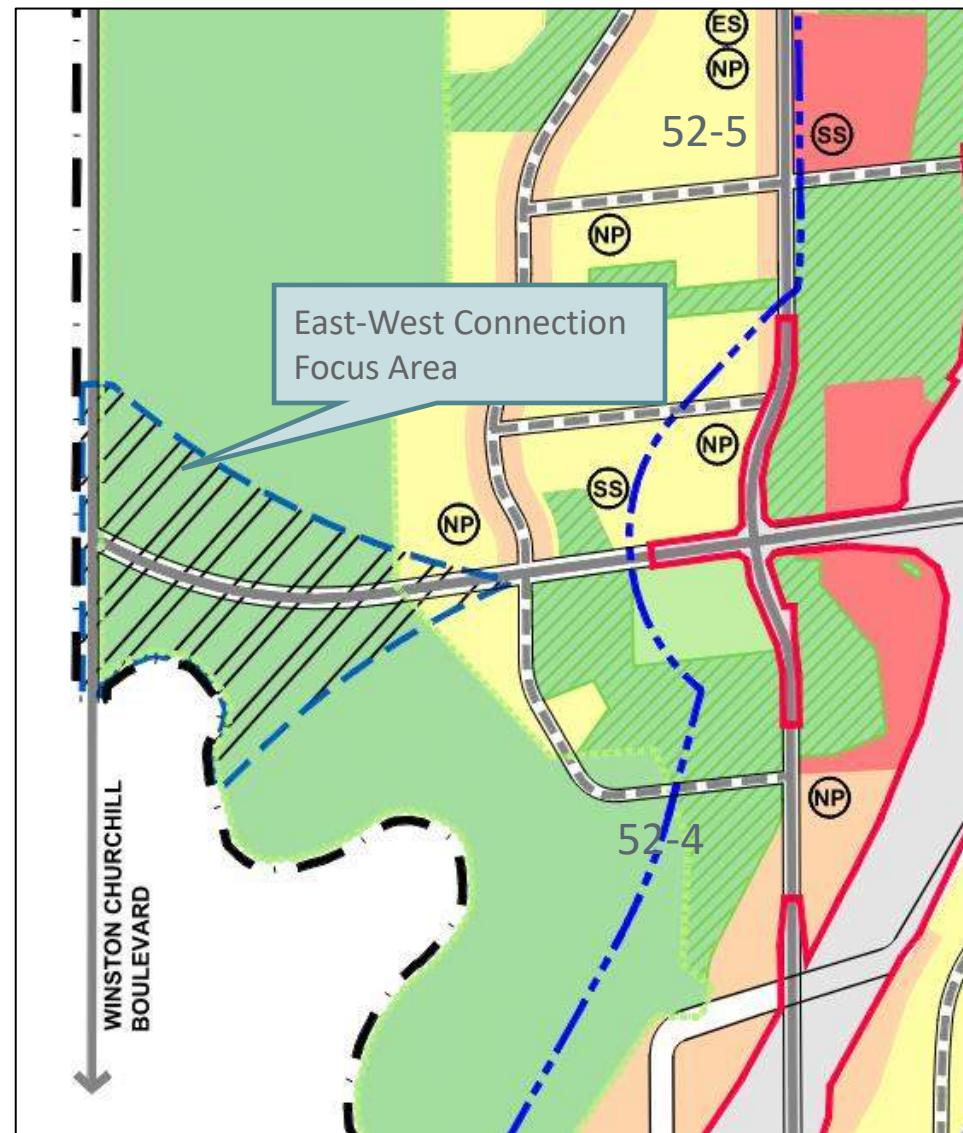


Policies Related to Halton Peel Boundary

13.1.11 b) Within Precincts 52-4 and 52-5 on Schedule 52-2, **future study is required as identified by the Halton Peel Boundary Area Transportation Study Memorandum of Understanding (HPBATS MOU, 2012)** to determine the extent of transportation network improvements that may be required to support **east-west connections across municipal boundaries**. This study will inform Precincts 52-4 and 52-5 and shall occur as part of the Precinct Planning process and may be undertaken by affected landowners. The Study shall be undertaken based on the terms identified in the HPBATS MOU and must be to the satisfaction of the City, in consultation with the Town of Halton Hills and Region of Halton.

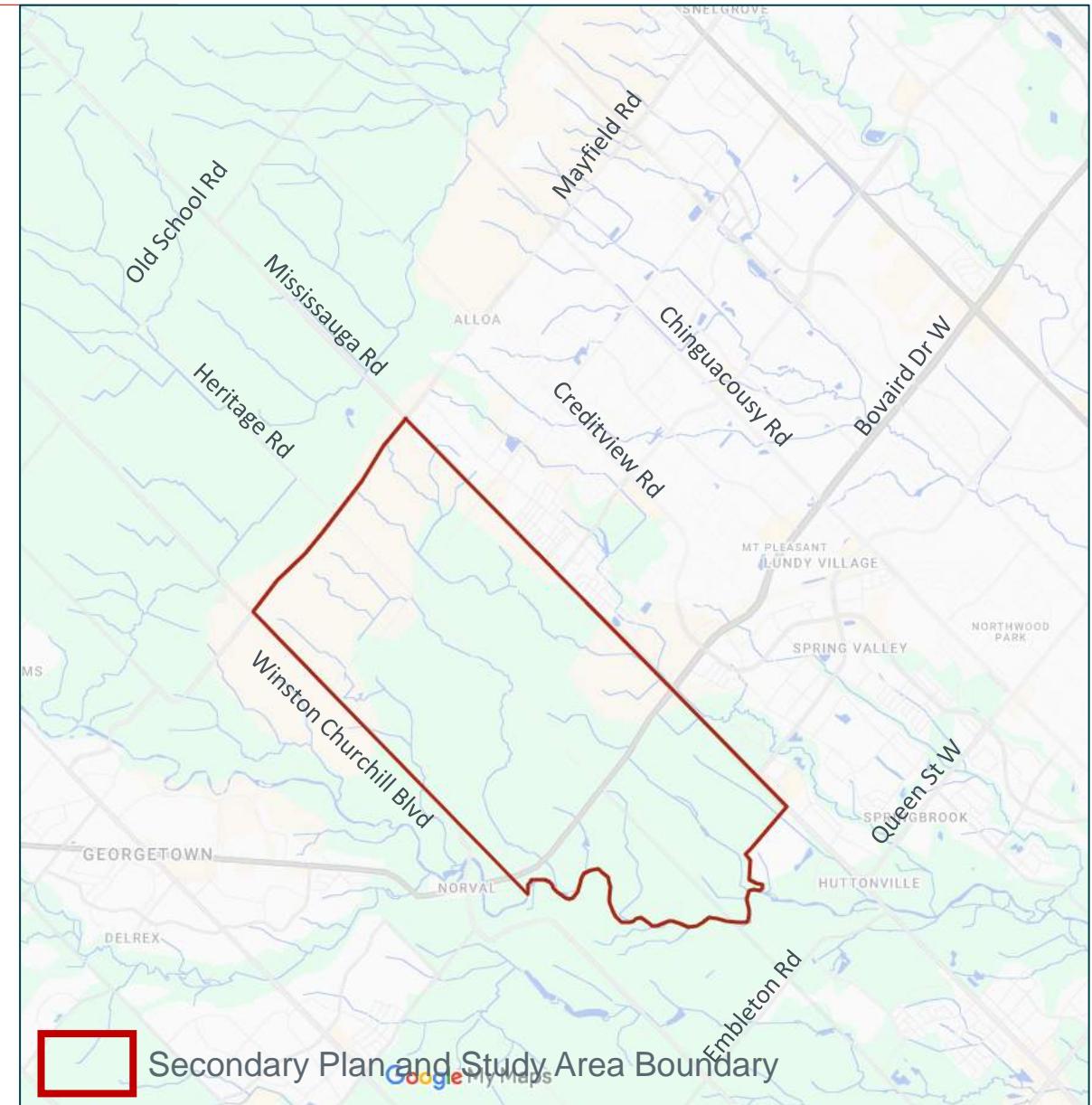
13.1.11 c) Any development proposed within the East-West Connection Focus Area as shown on Schedule 52-6, in advance of adoption of the Precinct Plans for Precincts 52-4 and 52-5 must be reviewed by the City in consultation with the Town of Halton Hills and Halton Region. **No development shall be approved by the City within Precincts 52-4 and 52-5, within the East-West Connection Focus Area as shown on Schedule 52-6, that may preclude the opportunity for the future east-west cross boundary transportation network improvements** identified in the Study identified in Policy 13.1.11 b) above.

To be Addressed at TMP instead of Precincts



The Study

The City of Brampton and the Heritage Heights Landowners Group are working together as co-proponents to undertake a Transportation Master Plan (TMP) to identify the transportation infrastructure required to support the growth envisioned for the Heritage Heights Secondary Plan Area.



MCEA Process

- The TMP is being undertaken in accordance with **Approach #2** of the Master Planning Process, as outlined in the Municipal Class Environmental Assessment (MCEA) (February 2024).
- The TMP will address Phases 1 and 2 of the MCEA process for any Schedule B projects and will form the basis for the recommended Schedule C transportation infrastructure projects identified within the TMP report.

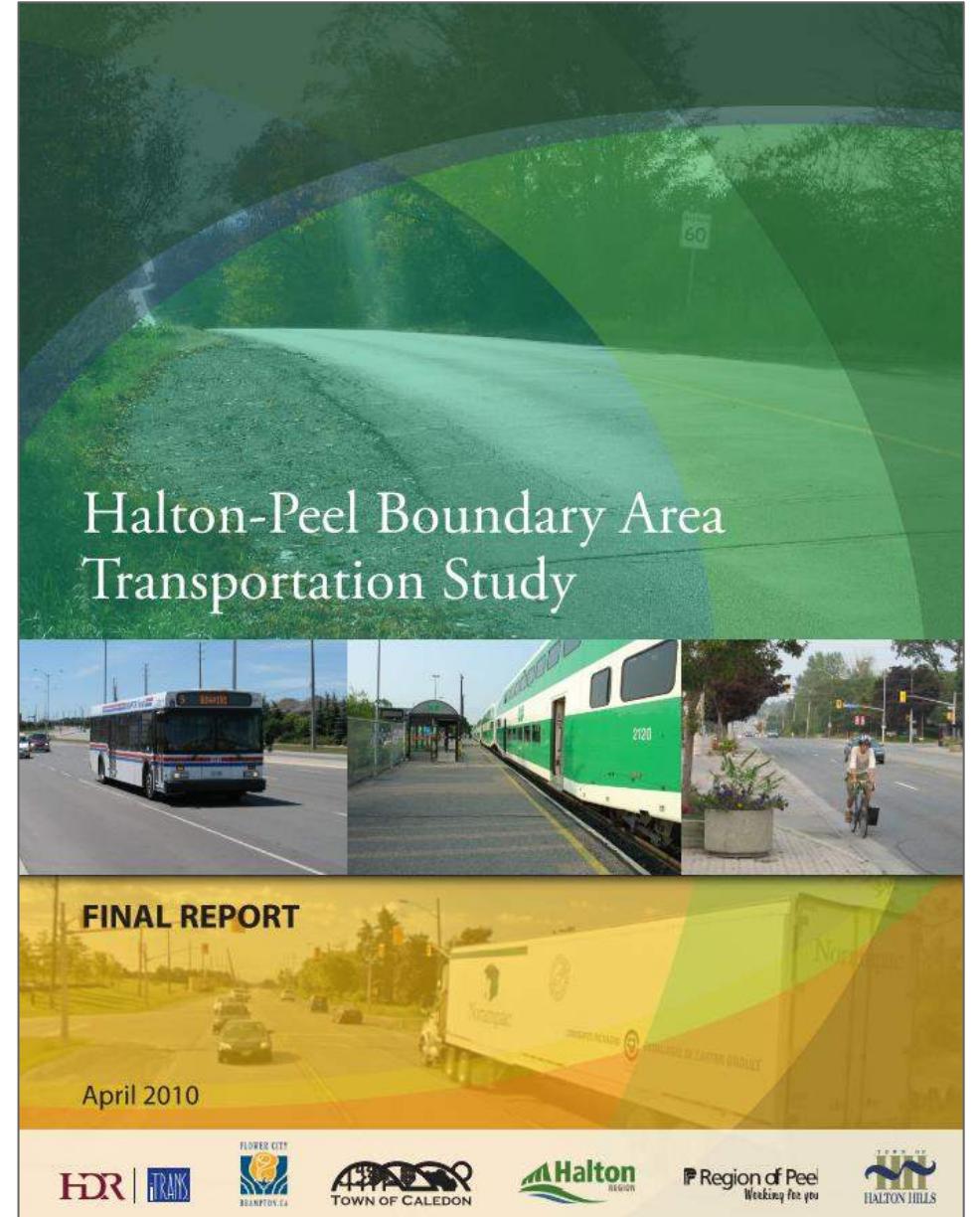
4.5 Master Planning Process

The master planning process is discussed in Section A.2.7. A summary of the various approaches is provided below.

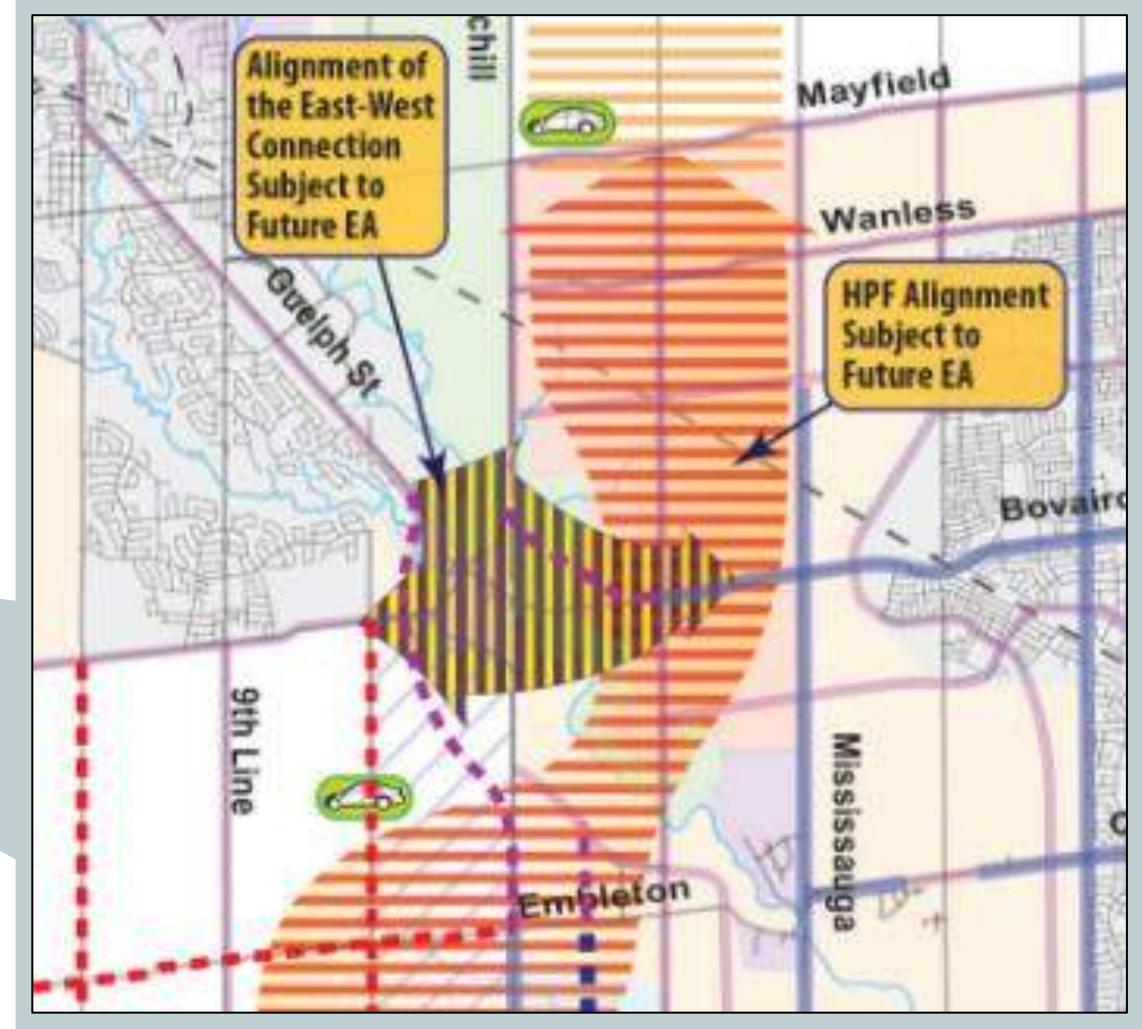
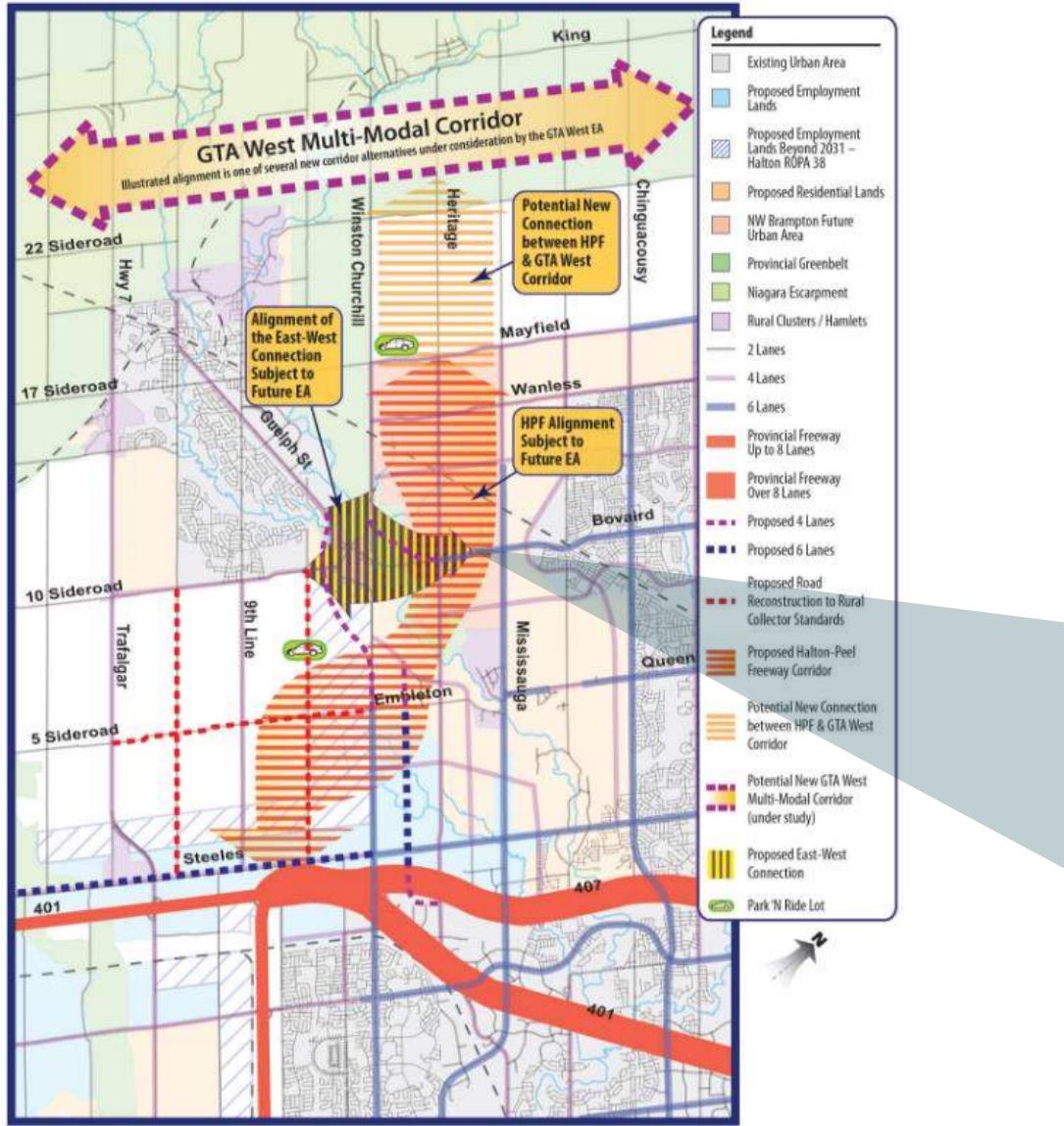
	Approach 1	Approach 2*	Approach 3*
Level of Detail	Broad (project specific information is minimal) Used as support for subsequent Schedule B and C project specific studies	Sufficient to fulfil requirements for Schedule B projects (more detailed project specific information is included) Used as support for subsequent Phase 3 and 4 for Schedule C project specific studies	Sufficient to fulfil requirements for Schedule B and C projects (more detailed project specific information is included)
Process	Preliminary Phase 1 and 2	Phase 1 and 2	Phase 1 to 4
Final Notice	Notice of Master Plan *This should not be called a Notice of Completion	Final notice becomes Notice of Completion for Schedule B projects	Final notice becomes Notice of Completion for Schedule B and C projects
Integration with Planning Act*	*Official Plan and Secondary Plan	*Official Plan and Secondary Plan, Plan of Subdivision, etc.	*Official Plan, Secondary Plan, Plan of Subdivision, etc.

* The various master planning approaches provide proponents with flexibility to customize their master plans to suit their needs. See section A.2.7 for information on modified approaches 2 and 3.

- Halton-Peel Boundary Area Transportation Study (HPBATS, April 2010)
- Conducted by HDR / iTrans on behalf of:
 - City of Brampton
 - Town of Caledon
 - Halton Region
 - Region of Peel
 - Halton Hills
- Memorandum of Understanding (MOU, April 2012) was signed between all municipalities noted above that sets out a framework for implementation of transportation improvements recommended in the HPBATS Study



Overview of HPBAT Study Recommendations - Roads



Subject to future Environmental Assessment studies

Exhibit H-2: HPBATS Recommended Road Network, 2031

Overview of HPBAT Study Recommendations - Transit

10

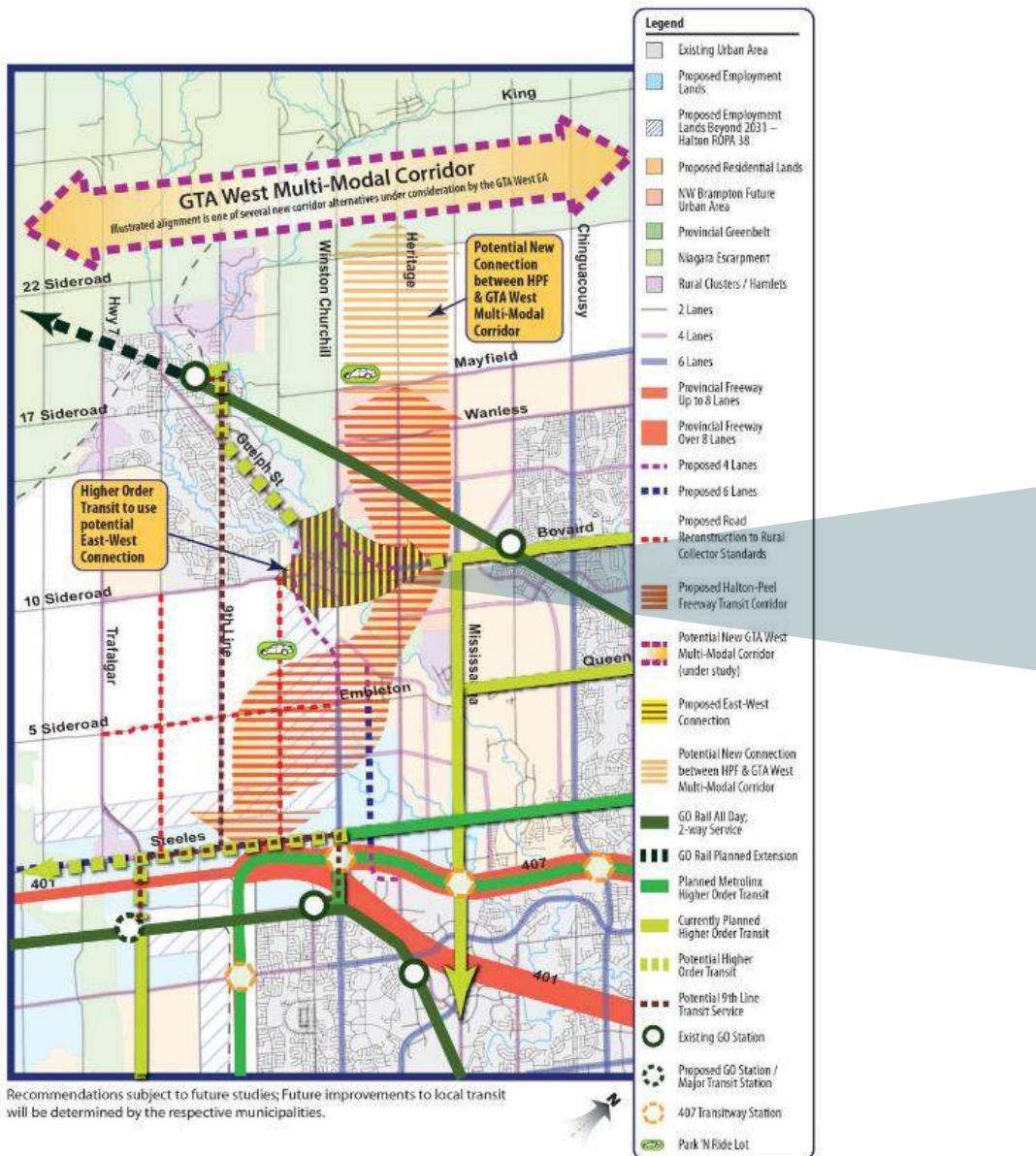
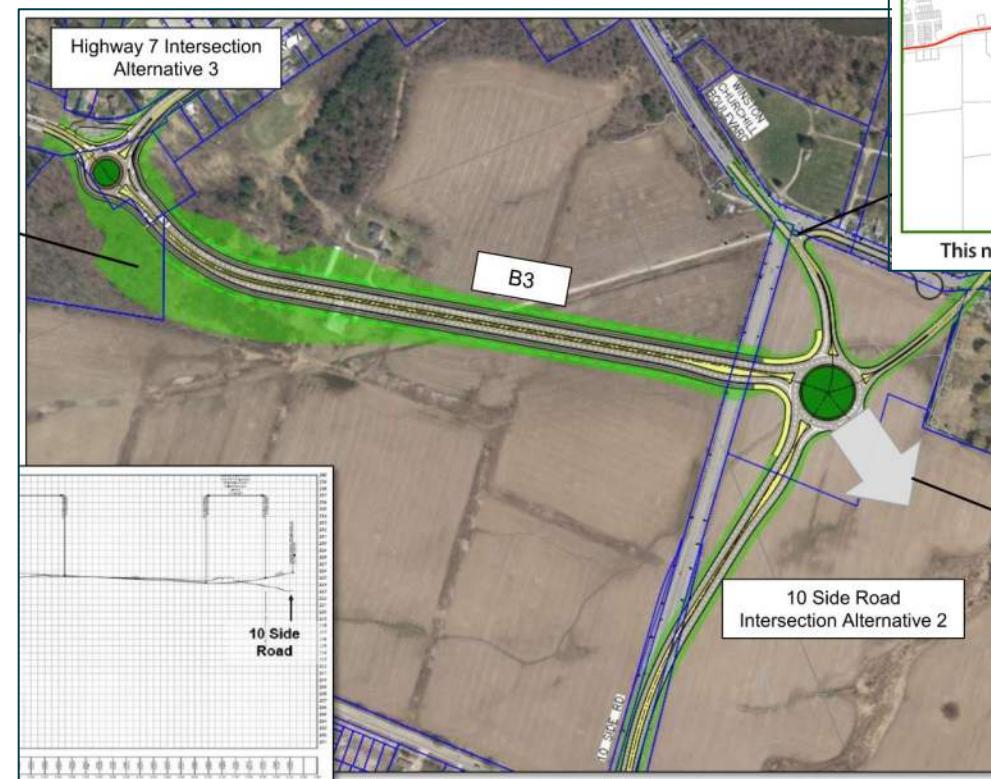


Exhibit H-1: HPBATS Recommended Transit Network, 2031

Norval West Bypass

- MCEA Class C for Norval West Bypass Transportation Corridor Improvements
- Conducted by Stantec on behalf of Halton Region
- Commenced in April 2020
- PIC#2 with Preliminary Preferred Design in February 2024

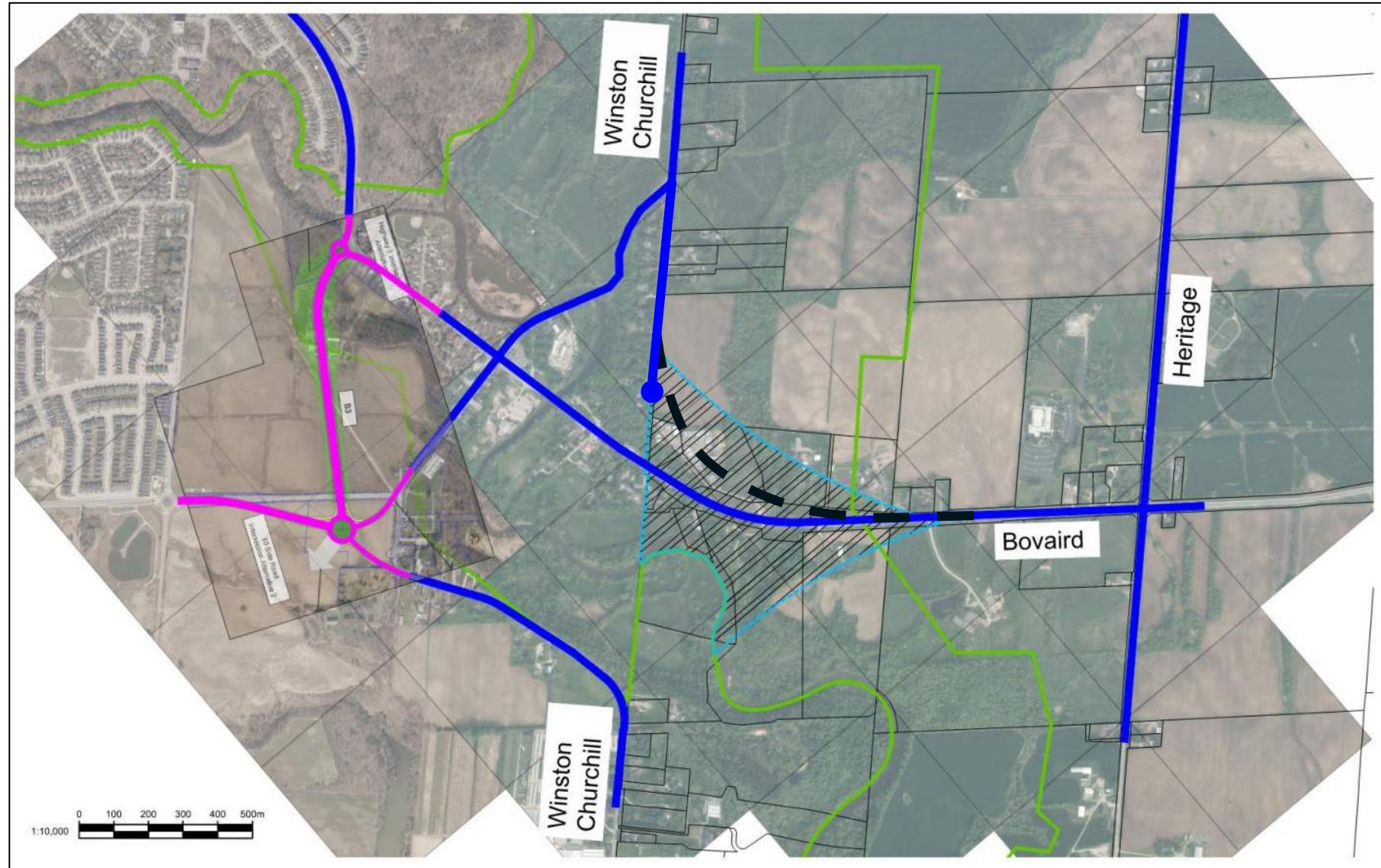


Potential future Winston Churchill Bypass subject to future separate MCEA Study.

Norval West Bypass, PIC #2 Recommended Alternative

12

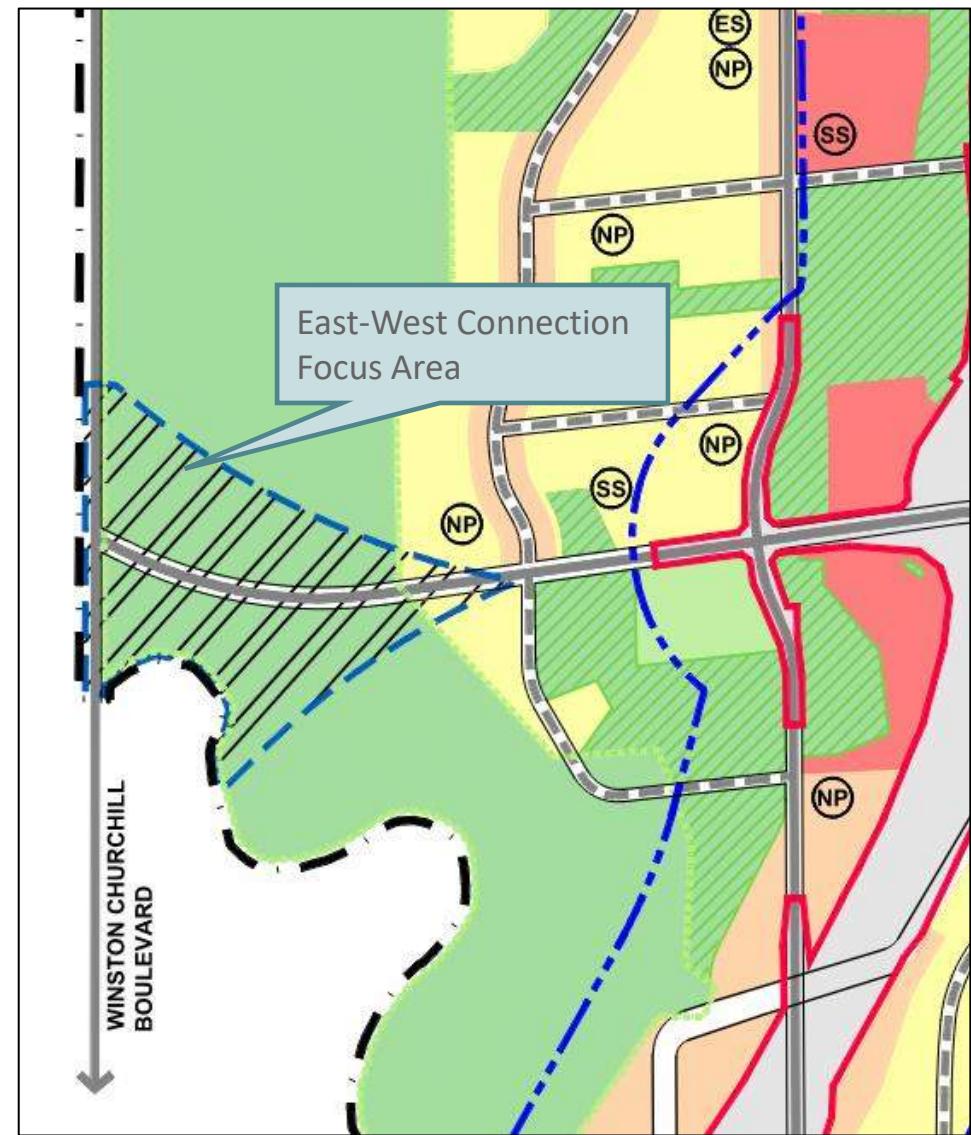
- Existing
- Norval West Bypass
- Adamson Bypass (Illustrative)
- Heritage Heights East-West Connection Focus Area



Next Steps – Heritage Heights TMP

13

- Heritage Heights TMP (instead of Precincts) will:
 - Identify alternative alignments within the Secondary Plan East-West Connection Focus Area that do not preclude and protects for East-West Connectivity
 - Model Capacity Results for Alternatives
 - Consult with Halton Hills and Halton Region on Alternatives and Preliminary Recommendations (Engagement Meeting #2)
 - Identify Process Recommendations (e.g. additional study or EA processes required)



Richa Dave, MCIP RPP

Project Manager, Transportation Planning

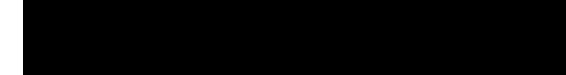
City of Brampton



Emily Ecker, P.Eng.

Senior Associate

BA Consulting Group Ltd.



Project Email: heritageheights@brampton.ca



MEETING MINUTES

TO

Meeting Attendees (May 14, 2025)

LOCATION

Virtual (Zoom)

FROM

BA Consulting Group Ltd.

DATE

May 14, 2025

**RE: HERITAGE HEIGHTS TRANSPORTATION MASTER PLAN – EAST-WEST CONNECTION FOCUS AREA ENGAGEMENT #1 –
MAY 14, 2025 – VIRTUAL DRAFT**

ATTENDEES

City of Brampton Attendees

- Richa Dave
- Vikram Hardatt
- Brian Lakeman
- Henrik Zbogar

Town of Halton Hills Attendees

- Maureen Van Ravens
- Jeff Markowiak
- Ivan Drewnitski
- Josh Salisbury

Halton Region Attendees

- Shelley Partridge
- Walter Scattolon
- Andrew Morgan
- Ann Larkin
- Lina Elmorshedy

Consultant (BA Group)

Attendees

- Emily Ecker
- Adrian Lorion
- Michael Giallonardo
- Yahya Deen

Heritage Heights Landowners**Group Attendees**

- James Reed

REGRETS

Consultant (BA Group) Regrets

- Scott Gibbons

PRESENTATION (EMILY ECKER)

- Emily E. delivered PowerPoint presentation outlining the Heritage Heights Secondary Plan, Heritage Heights Transportation Master Plan (TMP), and all policies/planning relating to the Halton Region / Peel Region boundary, including the 2010 Halton Peel Boundary Area Transportation Study (HPBATS).
- The presentation slides will be shared alongside these meeting minutes.
- Maureen V.R. clarified that Winston Churchill Boulevard does not “dead end” north of Norval but rather continues as Adamson Street through Norval, and continues as Winston Churchill Boulevard south of Norval.
 - North of Norval, alignment of Winston Churchill Boulevard has a short spur continuing south – Old Pine Crest Road – that terminates as a cul-de-sac north of Bovaird Drive West / Guelph Street (Highway 7); it does not currently connect.
- Ann L. highlighted that in the February 2024 PIC #2 boards, southern connection to roundabout proposed as part of Norval West Bypass (near current 10 Side Road and Winston Churchill Boulevard) is illustrated as an arrow. This would be a Winston Churchill realignment alternative.

OPEN DISCUSSION / HALTON HILLS & HALTON REGION FEEDBACK

- Maureen V.R. noted no issues with the presentation and to keep the Town of Halton Hills involved and informed of progress.
- Maureen V.R. advised to continue to illustrate east-west connection in materials.
- Ann L. inquired about whether there is a scope of work and advised to keep Halton Region involved and informed of progress.
 - Emily E. advised that this meeting is a “jump-off point” and we intend to keep the Region and Town involved and informed of progress.
- Walter S. noted he presumed there would be a document to review that would be forthcoming.
 - Vikram H. advised that this document would be the draft Heritage Heights TMP, when it is prepared and ready to be circulated.
- Brian L. noted that HPBATS was from 2010, some time ago now, and inquired whether the east-west connection was still desired/requested/needed by Halton Hills / Halton Region.
- Ann L. inquired about what the City of Brampton and BA Group were modelling.
 - Emily E. advised that the modelling process is agnostic to detailed curvature/alignment of roads and instead considers whether a specific connection exists or not; it is an “on or off” feature. It can define locations, like whether the Adamson Bypass is north or south of Bovaird Drive West / Guelph Street. The model will also consider ‘do nothing’ and road widenings to existing roads as alternatives.
- Maureen V.R. noted that it is for the City of Brampton to decide where is the best place to have the east connection. Halton Hills / Halton Region are doing their part of HPBATS, which is the West Norval Bypass. Halton Hills / Halton Region while Brampton will determine where/if to connect on Brampton side. The TMP model can also reflect on whether the east-west connection is still needed.

- Maureen V.R. and Ann L. clarified that the Adamson Bypass is separate from the east-west connection; the dashed black line on the BA Group drawing should connect from Bovaird Drive West to Winston Churchill Boulevard, north of the transition to Adamson Street (and Old Pine Crest Road).
- Maureen V.R. confirmed that it has been the Halton Region / Halton Hills assumption that the east west-connection would be south of Bovaird Drive West. However, now with Highway 413 plan, is the east-west connections still needed? Is it more needed?
- Brian L. noted that scenarios would evaluate without the Adamson bypass, then add it to determine if there's a need for the east-west connection (south of Bovaird Drive West).
- Maureen V.R. also noted that had the Winston Churchill interchange with Highway 413 been located further north, it would have provided greater east-west relief.
- Brian L. confirmed that City of Brampton modelling has the currently planned Highway 413 interchanges.
- Ivan D. is seeking a Terms of Reference (ToR) for the TMP work that impacts the Halton-Peel boundary. Discussion minuted below informed that the ToR details could take the form of a list of assumptions being made within the model.
- Ivan D. shared page 124 of the HPBATS, illustrating tested routing options from that study.
- Walter S. asked if City of Brampton / BA Group anticipate running a scenario without Highway 413.
 - Brian L. confirmed we are assuming Highway 413 will be built in modelled scenarios.
 - Walter S. advised that maybe including a scenario without Highway 413 provides a litmus test.
 - Brian L. explained that the previous planned north-south corridor planned as part of old Heritage Heights TMP – in absence of Highway 413 – could be the scenario Walter S. requested. This was referred to in the discussion as the “super arterial” road; 4-6 lane road, other features, etc.
 - Henrik clarified that a north-south corridor has been incorporated in some manner along the history of prior TMPs.
- Shelley P. asked if City of Brampton / BA Group required data that Halton Region could share to inform inputs to model. Perhaps planned development in southeast Georgetown.
 - Brian L. confirmed that Brampton did request and receive 2031/2041/2051 land use forecasts and have incorporated these. He will confirm this with City of Brampton modeller who was not present at this meeting (Linda W.).
 - Adrian confirmed this information has informed the model at macro level and is being used to analyze at meso level.
- Ann L. agreed with Ivan D.'s request for a ToR as she finds them very helpful. She advised that it can be very “high level” and would outline assumptions and scenarios.
 - Highway 413 in or out?
 - Land use forecasts?
 - Horizon years?
 - What alternatives are analyzed?

- Adrian L. advised that BA Group will be preparing this material for the draft TMP and can pull out these items to inform Halton Hills / Halton Region of model assumptions/scenarios.
- Adrian L. advised that BA Group has collaborated with City of Brampton for ~6 months to build TMP model for Heritage Heights.
- Vikram H. posited that a takeaway / action item from this meeting would be to continue to engage with this group.
- Maureen V.R. inquired about timing.
 - Vikram said “as soon as possible.”
 - Emily E. advised that draft report is targeted for July of this year, current focus is engagement
- Emily E. recommended to meet again within a month to maintain momentum.
- Maureen V.R. inquired about Region of Peel.
 - They were not present at this meeting but City of Brampton and BA Group meeting with Region of Peel staff on Friday of this week.
- Ann L. confirmed that Peel Region / Halton Region have a road boundary agreement.
- Walton S. requested separate image (i.e. map) of each scenario that is being tested.
- Brian L. described 4 scenarios to test:
 - Base no bypasses
 - Base with north bypasses (e.g. Adamson Bypass)
 - Base with north and south bypasses (i.e. south would be the East-West Connection)
 - Base with only south bypass (i.e. only the East-West Connection).

MEETING OUTCOME AND NEXT STEPS

- BA Group / City of Brampton to prepare list of model assumptions/scenarios regarding TMP work that impacts the Halton-Peel boundary and describes study at high level.
- Presentation materials and Minutes from May 14, 2025 to be provided.
- Heritage Heights TMP to proceed with modelling of 4 scenarios minuted:
 1. Base no bypasses
 2. Base with north bypasses (e.g. Adamson Bypass)
 3. Base with north and south bypasses (i.e. south would be the East-West Connection)
 4. Base with only south bypass (i.e. only the East-West Connection).



HERITAGE HEIGHTS TRANSPORTATION MASTER PLAN

HH TMP Engagement #2

Prepared For: Peel Region, Halton Region, Halton Hills

June 24, 2025



BRAMPTON

*Heritage Heights
Landowners Group*



BA Group

City of Brampton

- Vikram Hardatt
- Richa Dave
- Brian Lakeman
- David Monaghan
- Nelson Cadete
- Henrik Zbogar

Peel Region

- Dana Jenkins

Halton Region

- Shelley Partridge
- Walter Scattolon
- Andrew Morgan
- Ann Larkin
- Lina Elmorshedy

Heritage Heights Landowners Group

- James Reed

Halton Hills

- Maureen Van Ravens
- Jeff Markowiak
- Ivan Drewnitski
- Josh Salisbury
- Bronwyn Parker
- Melissa Ricci

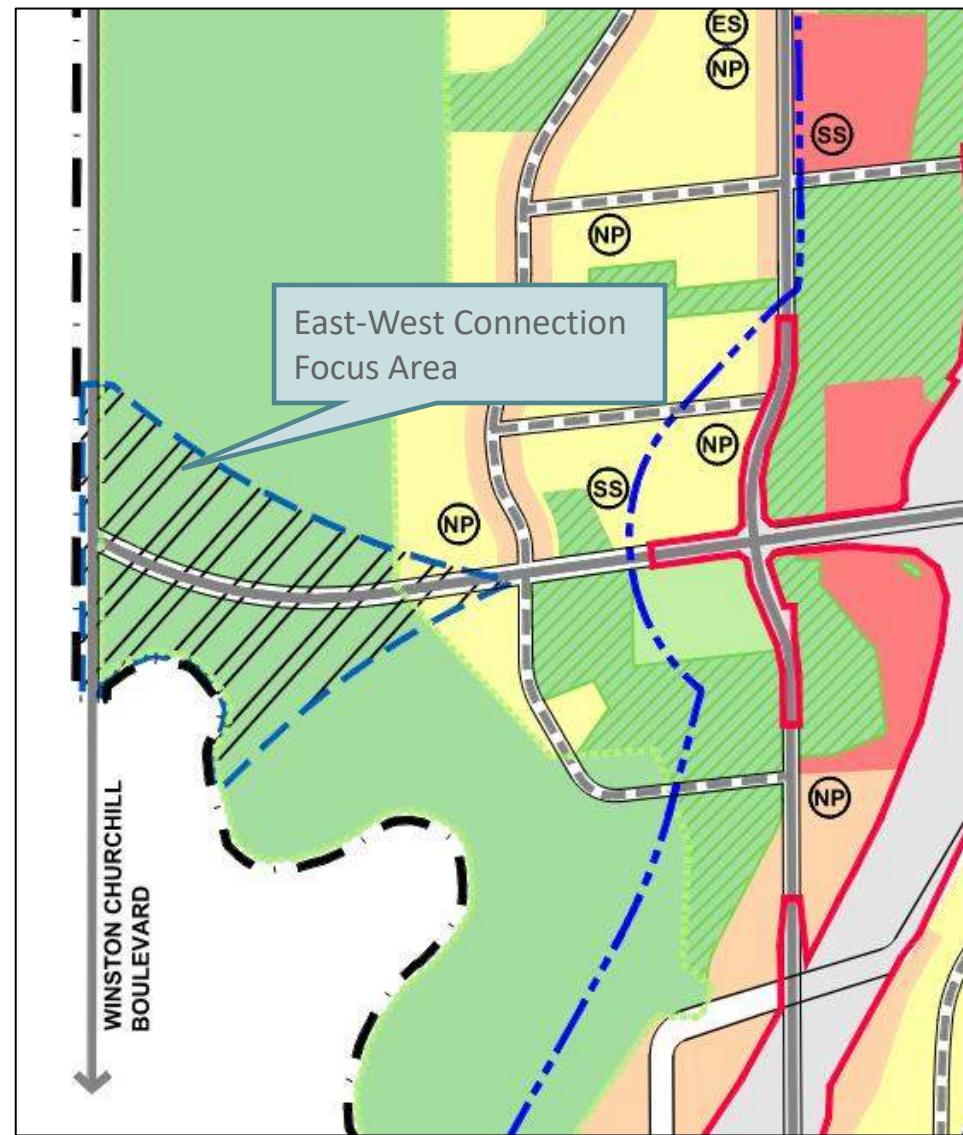
BA Group

- Emily Ecker
- Michael Giallonardo
- Yahya Deen
- Adrian Lorion
- Scott Gibbons
- Cora Freudenberg
- Kevin Xu
- Hansen Rao

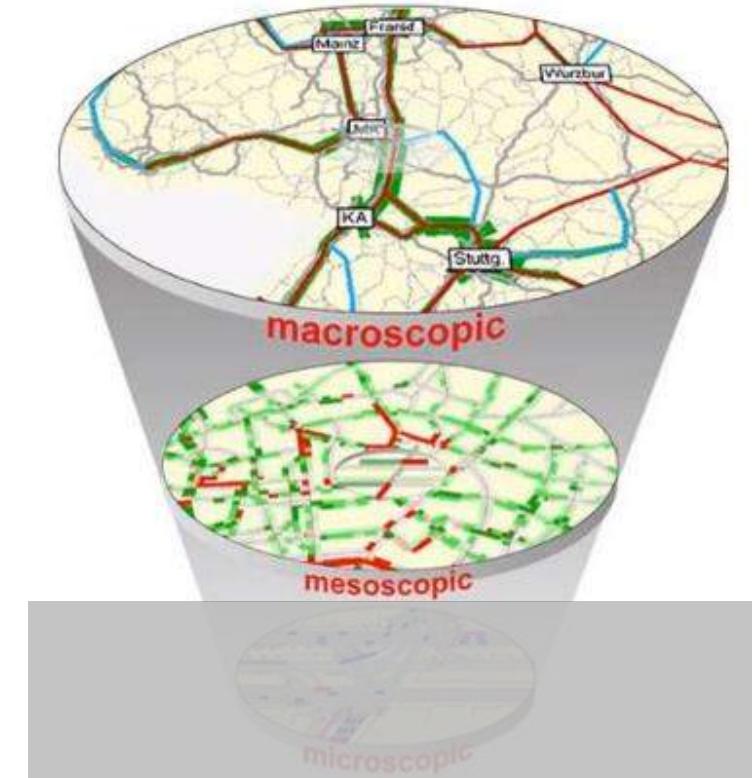
Engagement 1 Recap – Heritage Heights TMP

3

- Heritage Heights TMP (instead of Precincts) will:
 - Identify alternative alignments within the Secondary Plan East-West Connection Focus Area that do not preclude and protects for East-West Connectivity
 - Model Capacity Results for Alternatives
 - Consult with Halton Hills and Halton Region on Alternatives and Preliminary Recommendations (Engagement Meeting #2)
 - Identify Process Recommendations (e.g. additional study or EA processes required)



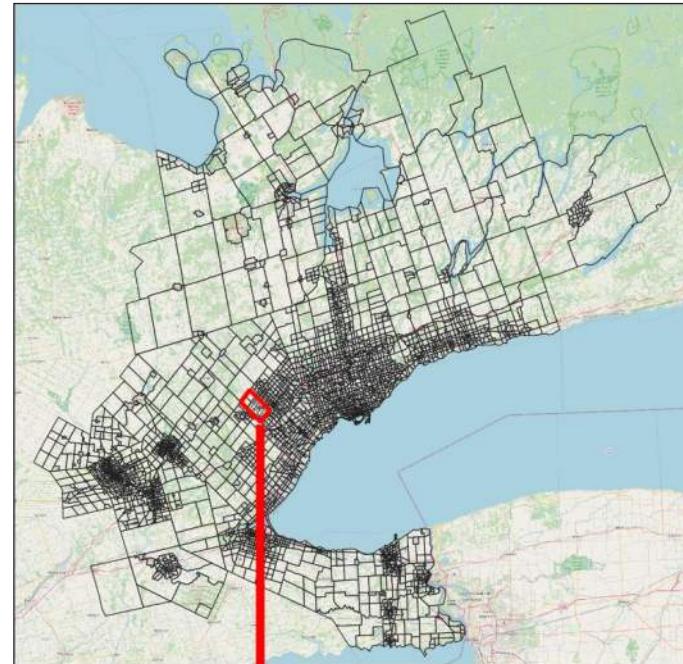
- Modelling framework will support the TMP and respond to the planning needs of the City, Region and the LOG.
- BA Group has adopted a standard multi-resolution and multi-modal modelling approach (macro/meso/micro), which relies heavily on collaboration with City modelling staff.
- BA Group has developed a large scale mesoscopic *Visum* model that encompasses the Heritage Heights Transportation Master Plan area.
- This approach ensures that BA Group's mesoscopic model captures the effects of both large-scale regional planned transportation infrastructure improvements and regional population/employment growth considered in the City's macroscopic travel demand model while producing detailed results at a corridor-level.



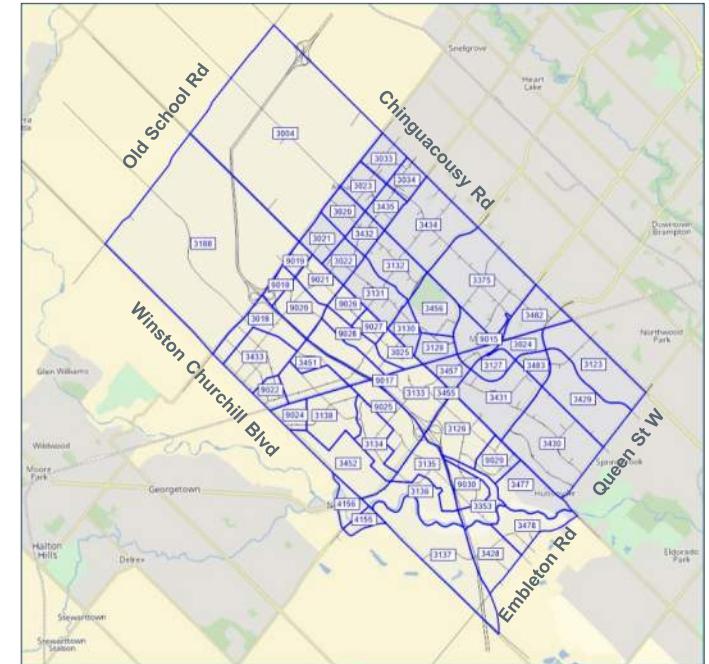
Heritage Heights TMP – Modelling Framework Cont'd

- Subarea Origin-Destination travel demand matrices were extracted from City of Brampton's Regional Demand Model and were used as inputs into the Mesoscopic Model
- BA Group Mesoscopic model study area bounds:
 - Old School Rd to the North
 - Chinguacousy Rd to the East
 - Queen St W/Embleton Rd to the South
 - Winston Churchill Blvd to the West
- Existing conditions mesoscopic model:
 - Developed with **existing conditions (2016) sub-area travel demand matrices** from the City's macroscopic model.
 - **Calibrated with 2024 turning movement count data.**
- Future 2051 conditions mesoscopic models:
 - Developed with **future conditions (2051) sub-area travel demand matrices from the City's BMP macroscopic model.**
 - Used to evaluate road network options (e.g., road locations, number of lanes, network connectivity, etc.)
 - Used to evaluate impacts on traffic operations due to transit network options

Macroscopic Model



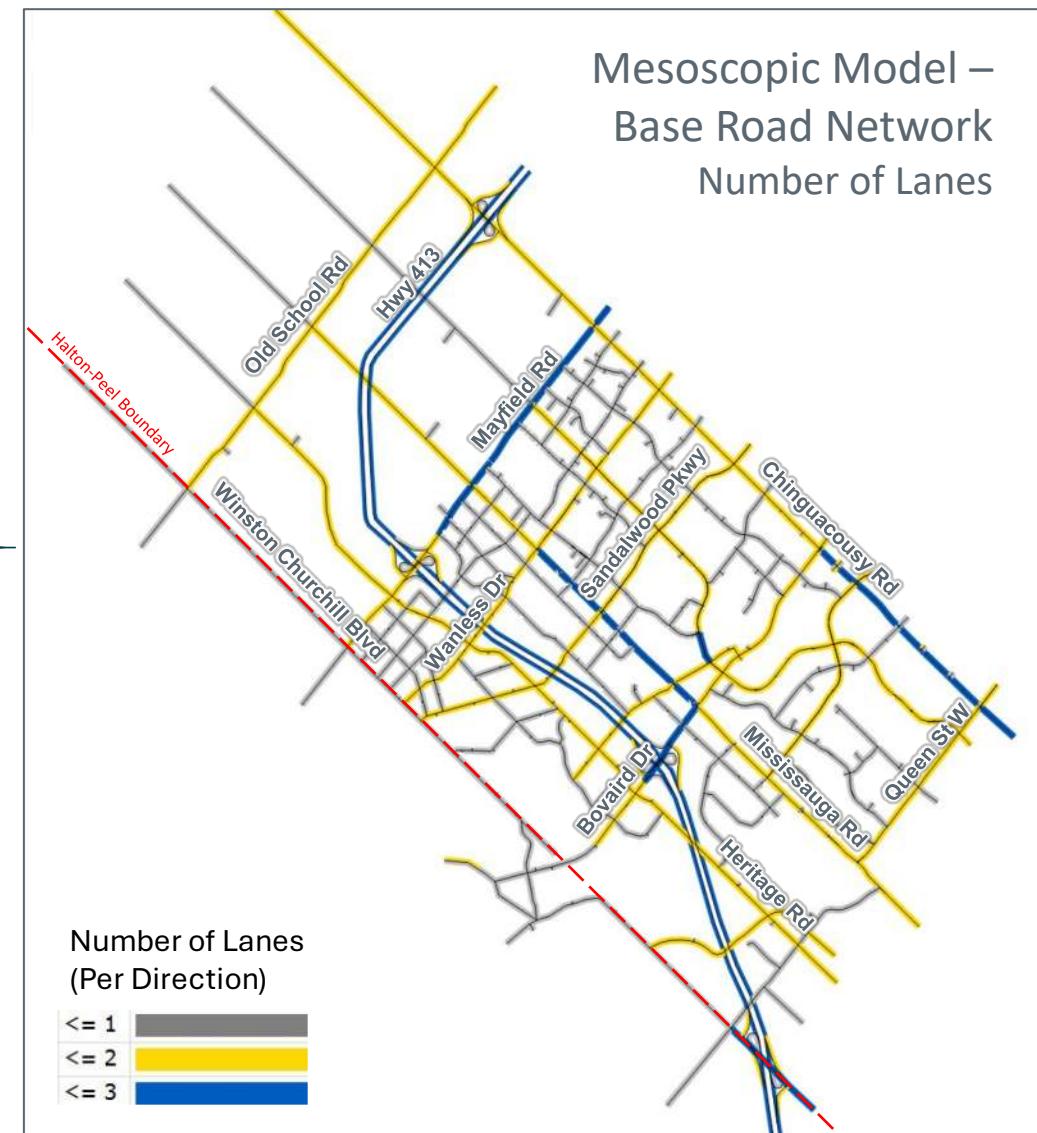
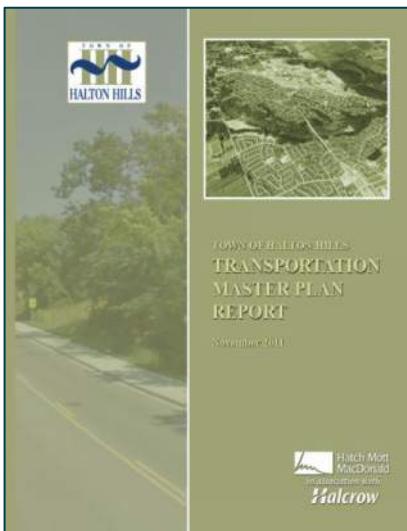
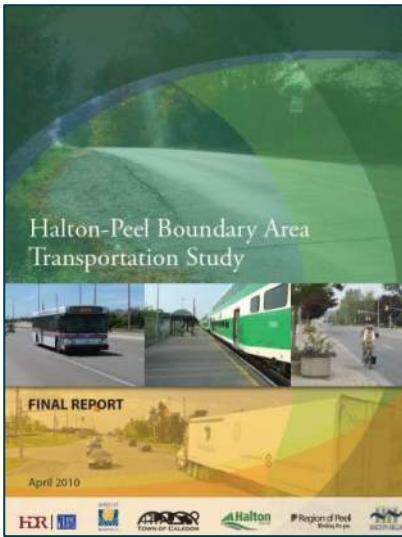
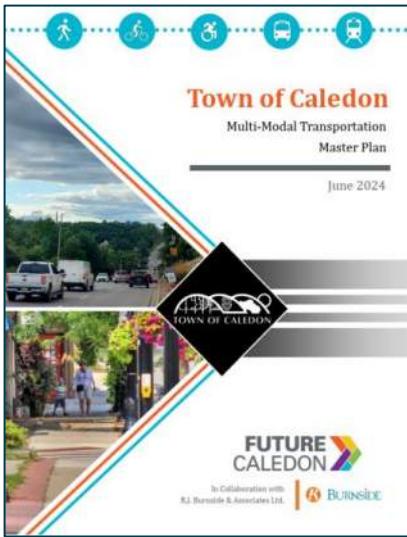
Mesoscopic VISUM Model BA Group's HH TMP Mesoscopic Model



Subnetwork Matrices					
	1	2	...	n	
1	a_{11}	a_{12}	...	a_{1n}	
2	a_{21}	a_{22}	...	a_{2n}	
...	:	:	:
m	a_{m1}	a_{m2}	...	a_{mn}	

2051 Transportation Network

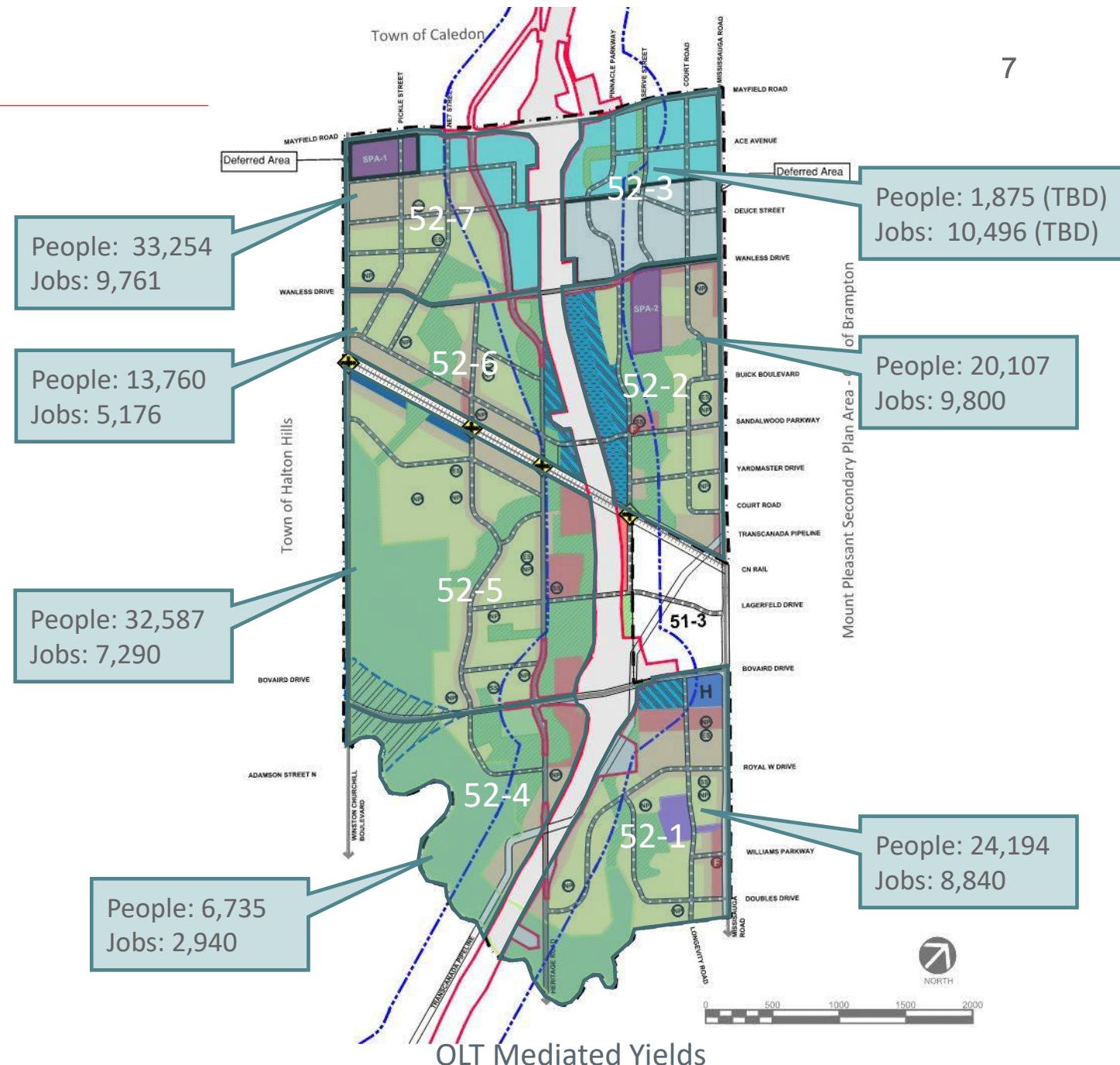
6



2051 Growth Assumptions

7

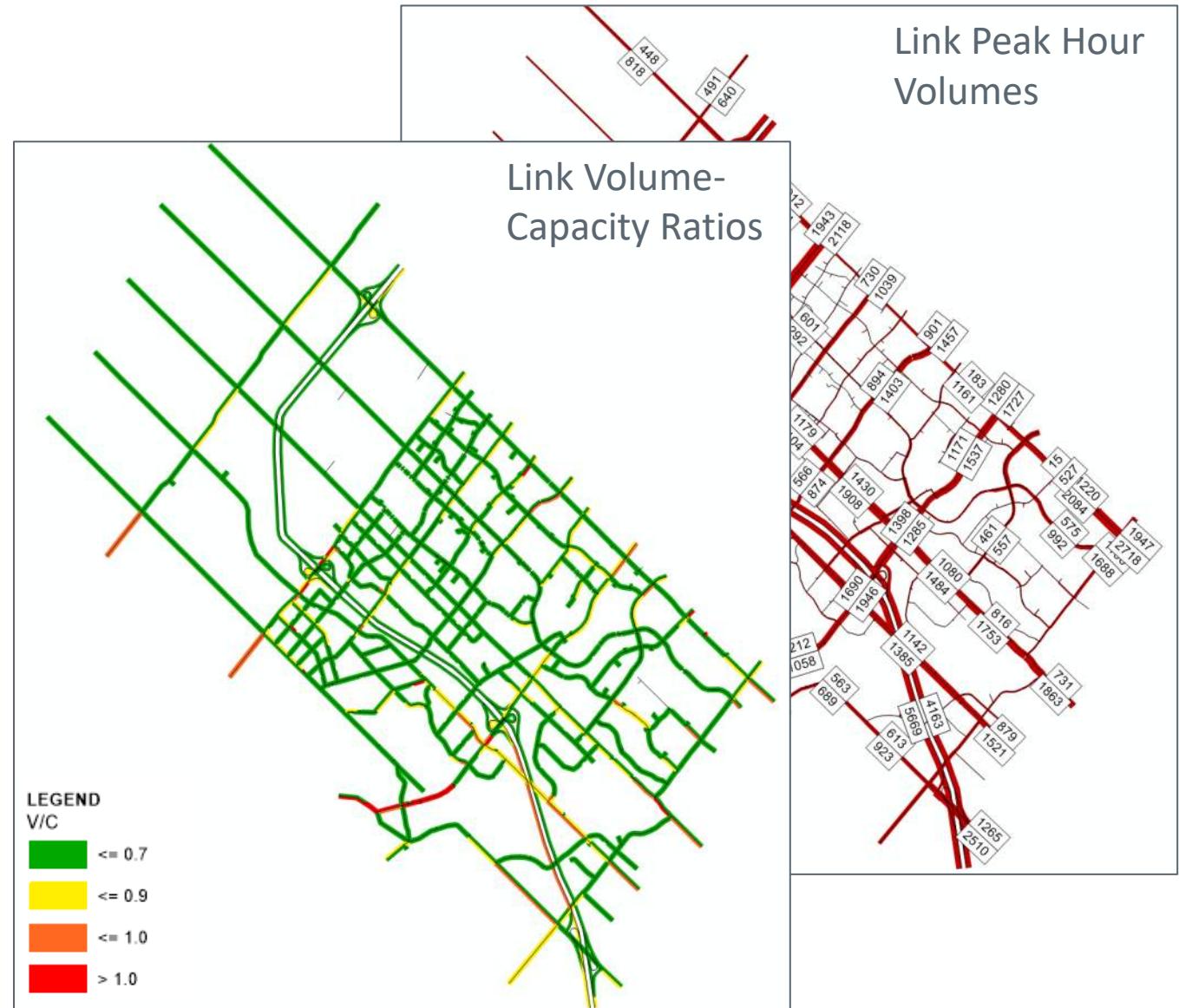
- City of Brampton's 2051 BMP Model was used to extract demand matrices to be used as inputs in 2051 mesoscopic model
 - Uses 2051 Peel Region Scenario 1 growth forecasts
 - Uses Halton growth forecasts obtained from Halton Region staff in December 2023.
- Macroscopic model was adapted for use in the HH TMP modelling exercise
 - Growth forecasts for HH traffic zones were updated to reflect OLT mediated yields
 - Total of 132,188 people and 54,307 jobs



Examples of Model Outputs

8

- Mesoscopic model outputs are being used to assess alternatives in 7 distinct focus areas in the Heritage Heights TMP study area
- Model outputs include link peak hour volumes and link volume-capacity ratios
- Impacts to transportation operations associated with each Norval Bypass options will be evaluated using the mesoscopic model and its outputs



Halton-Peel Boundary Crossing

Overview of HPBAT Study Recommendations - Roads

10

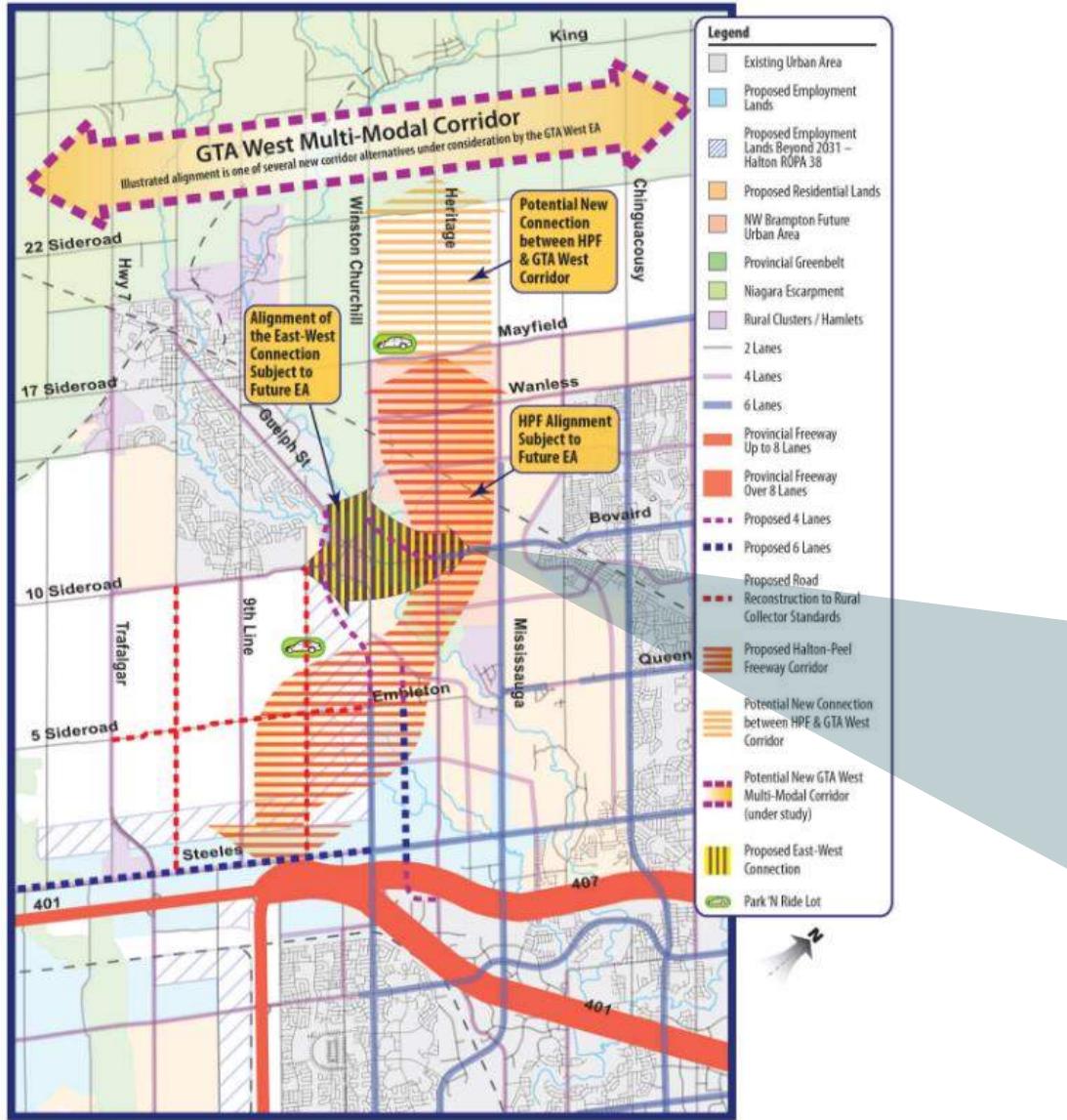
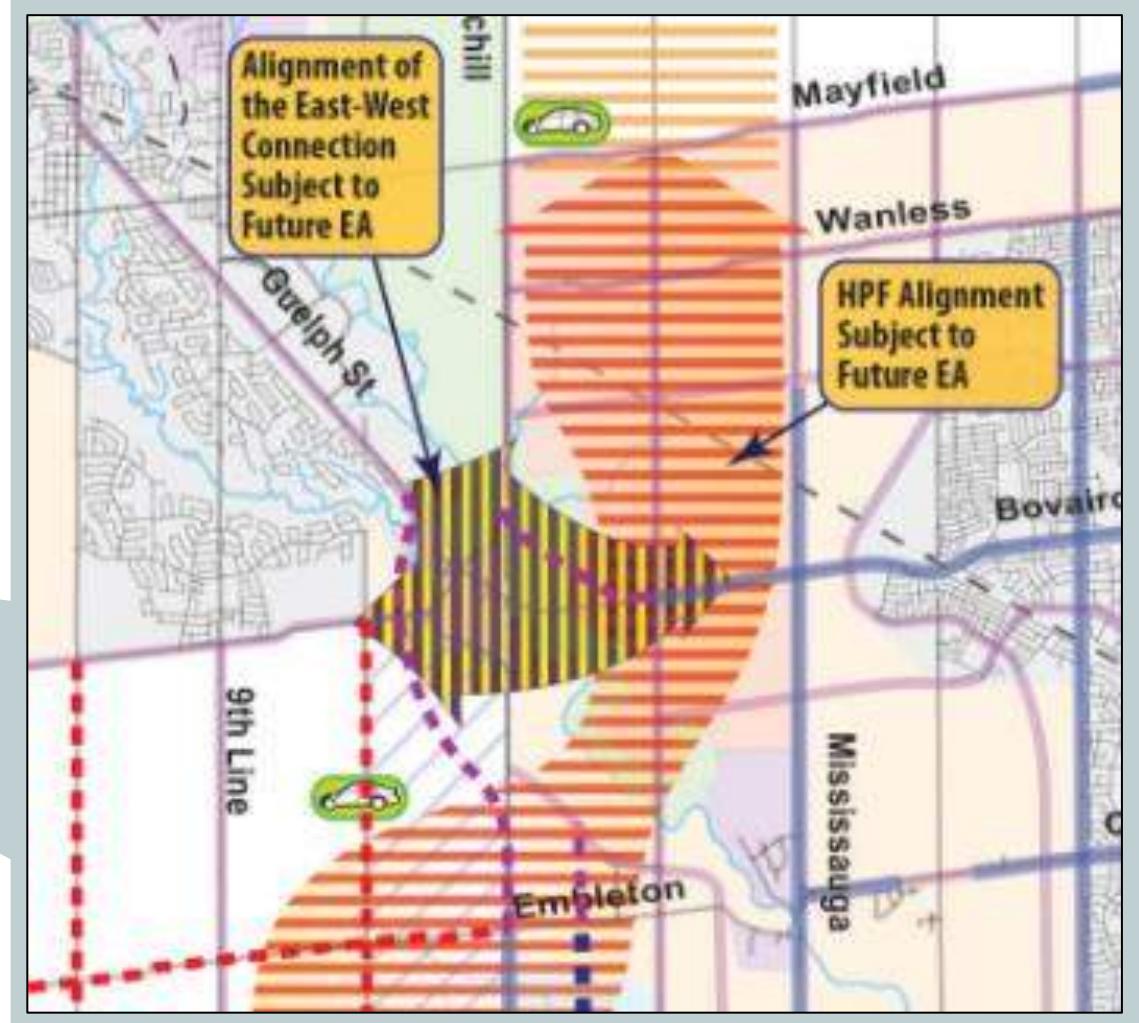


Exhibit H-2: HPBATS Recommended Road Network, 2031



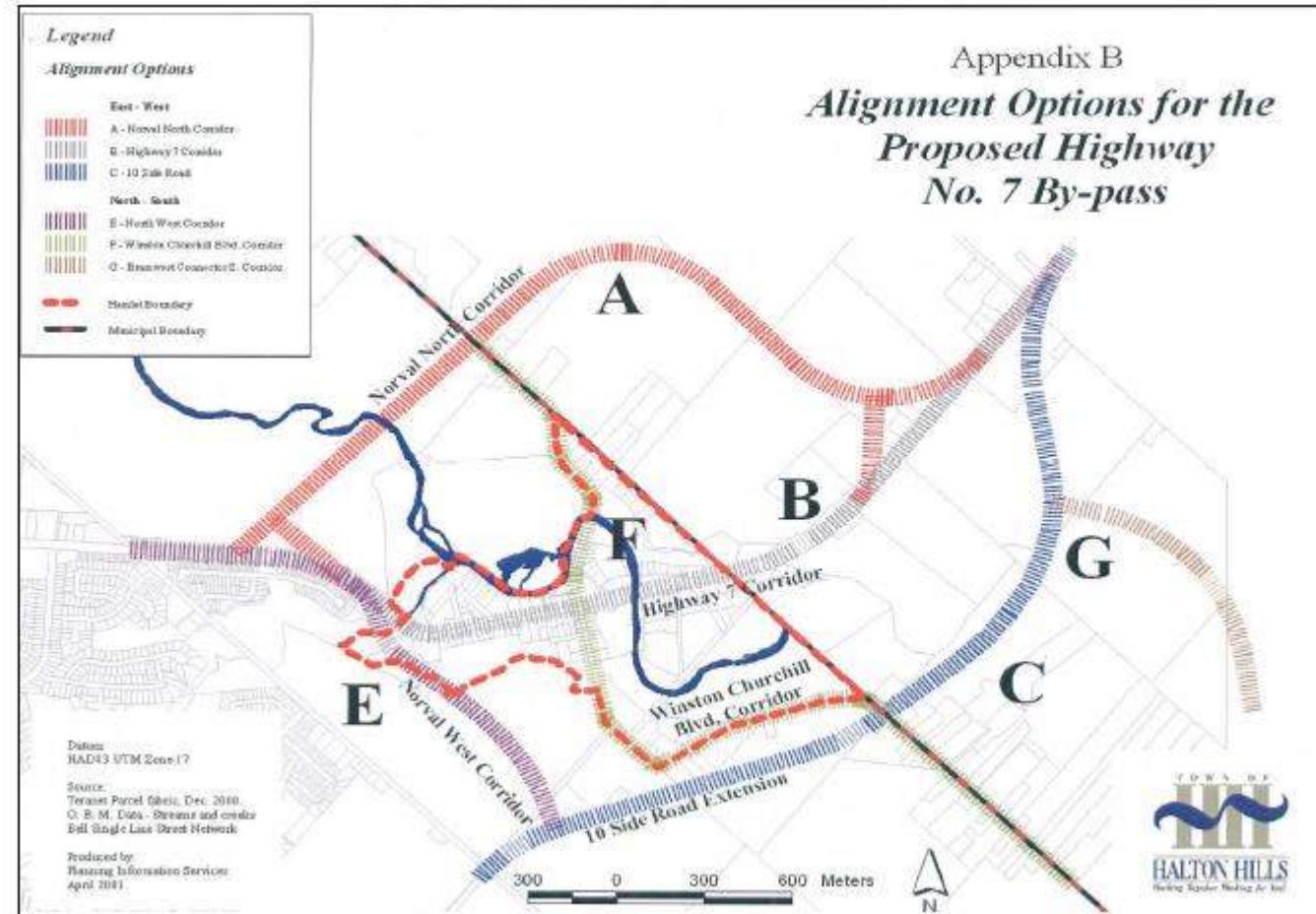
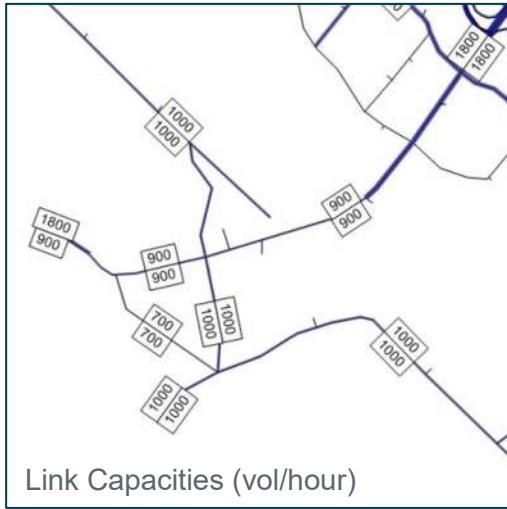
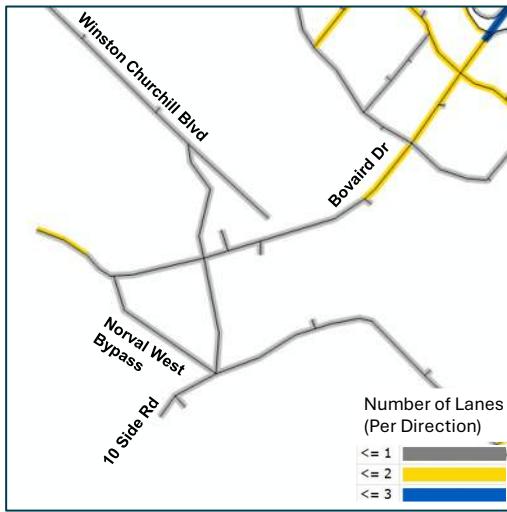


Exhibit 6-8: Early Concepts of the Norval Bypass

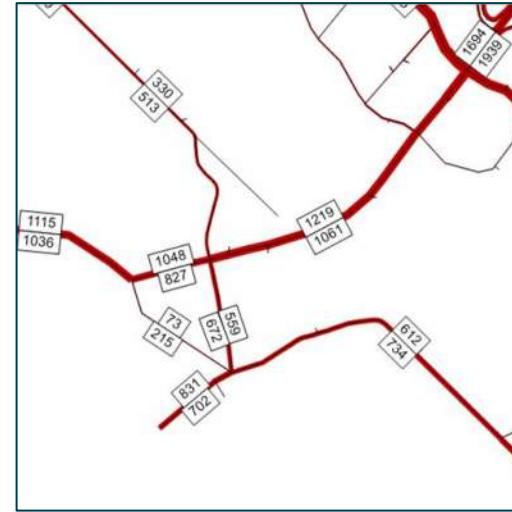
Focus Area 5 – Future 2051 Base Road Network Assumptions and Operations

12

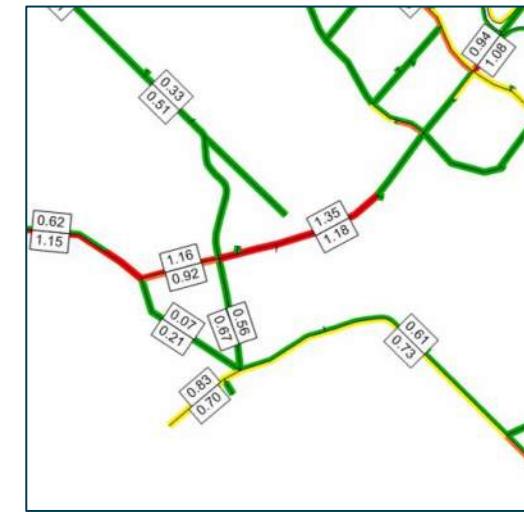
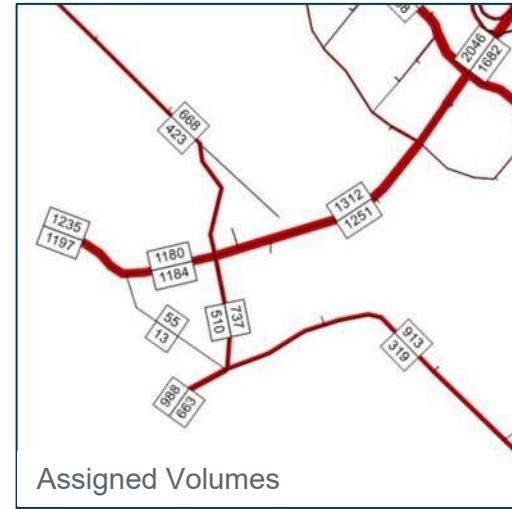


Consistent with UoFT EMME Coding Standards

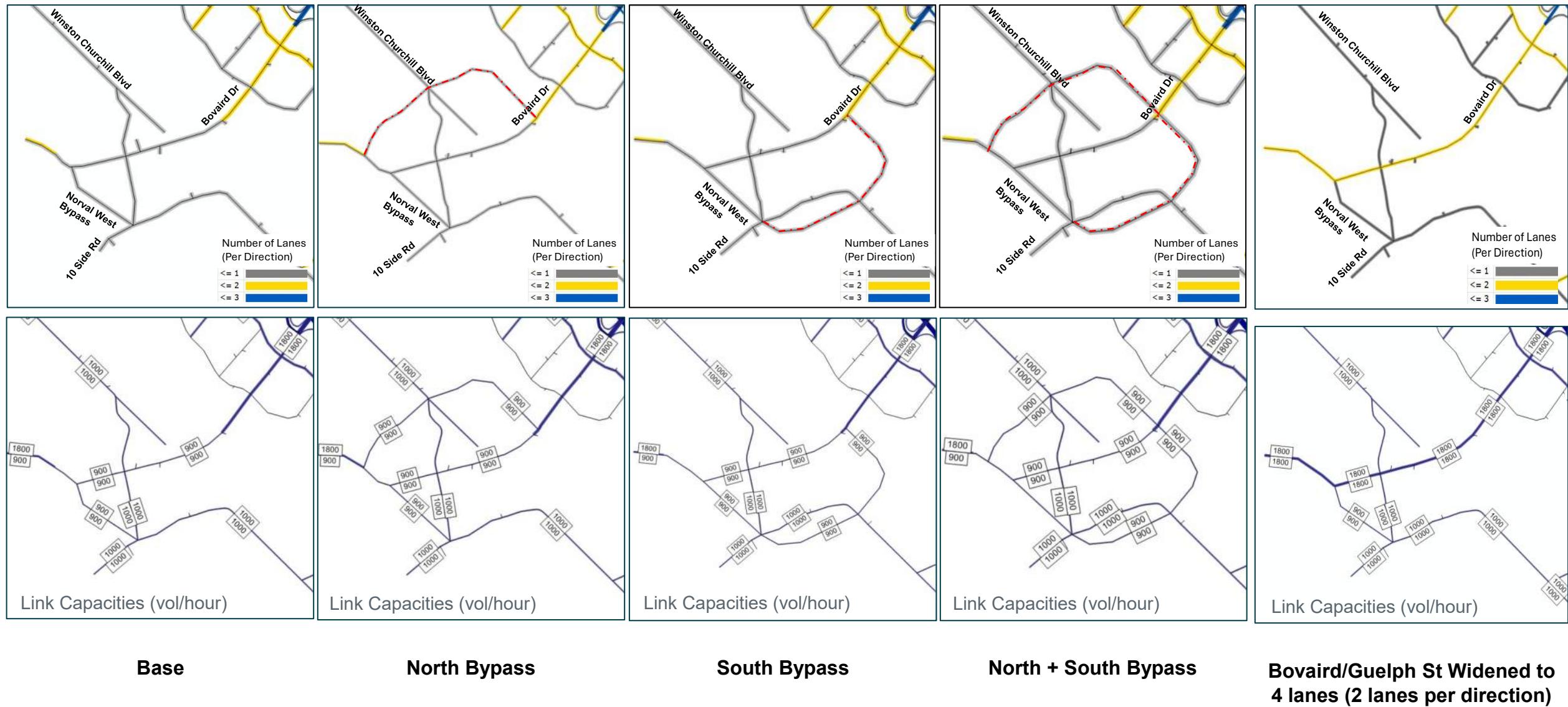
AM Peak Hour



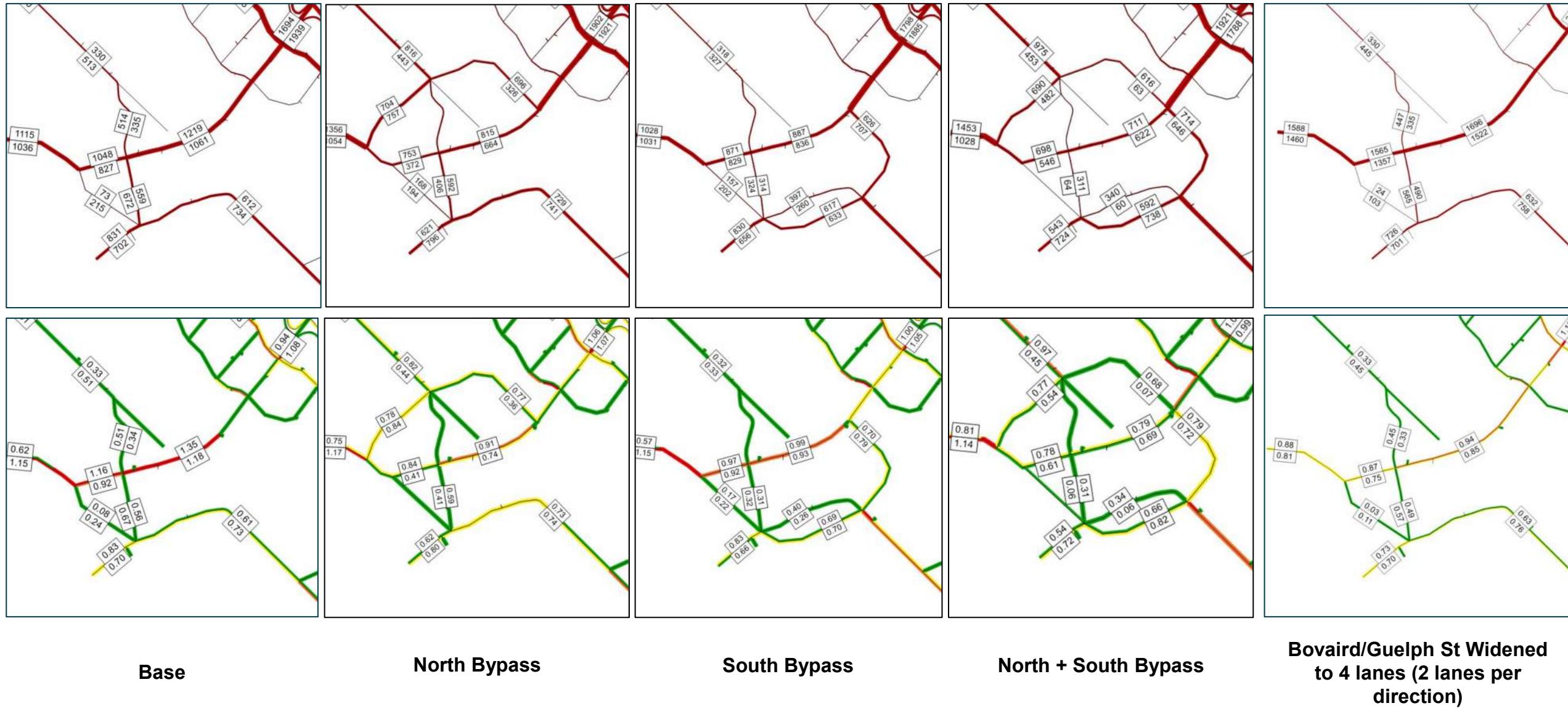
PM Peak Hour



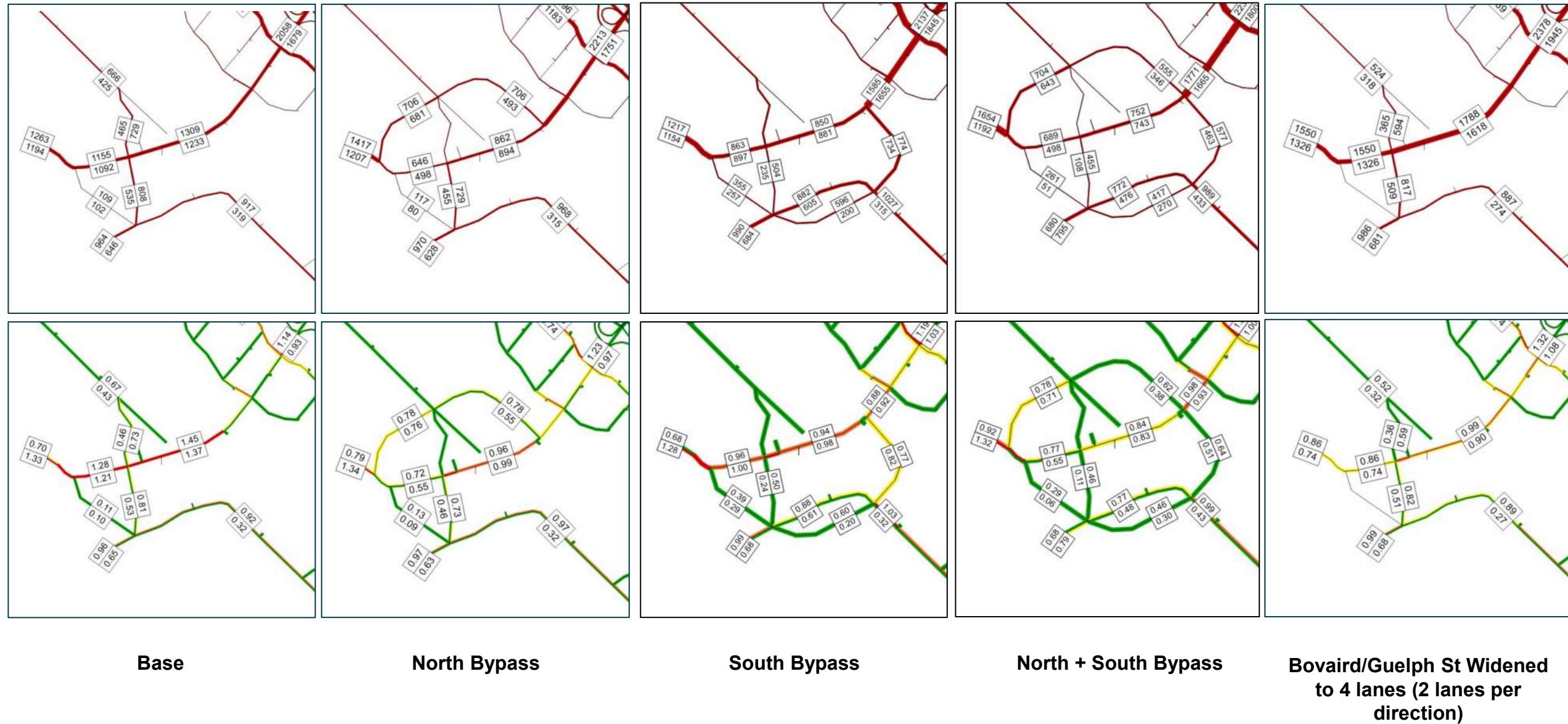
Focus Area 5 – Alternative Scenarios

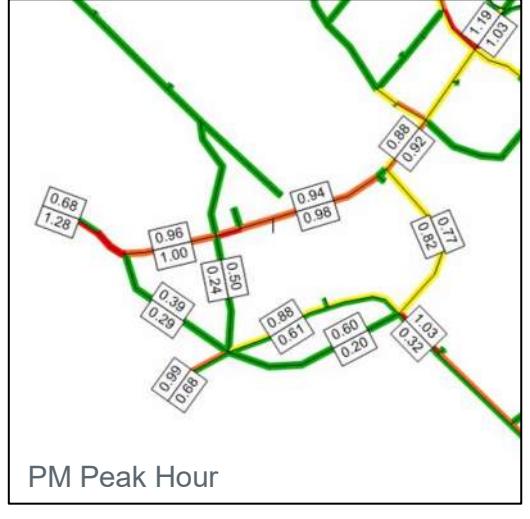
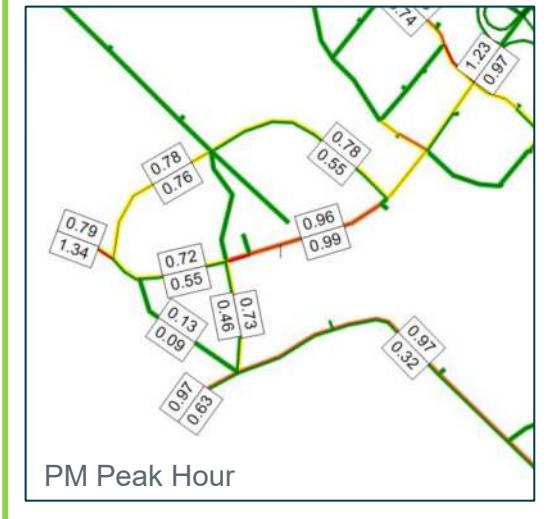
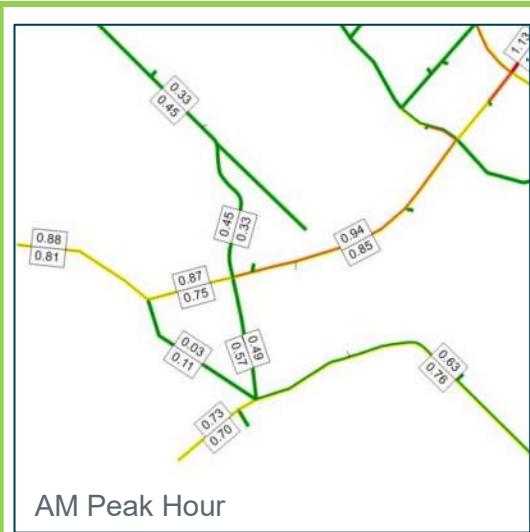
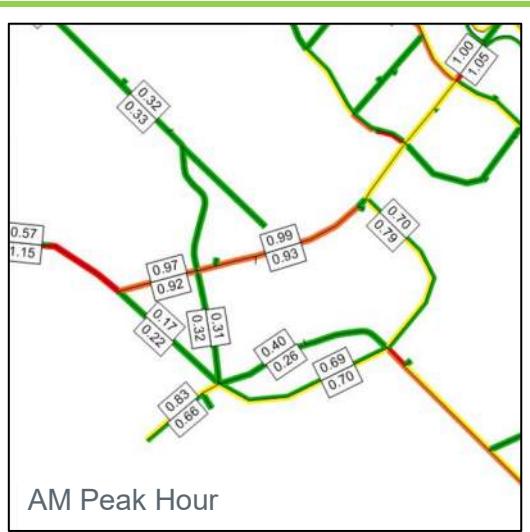
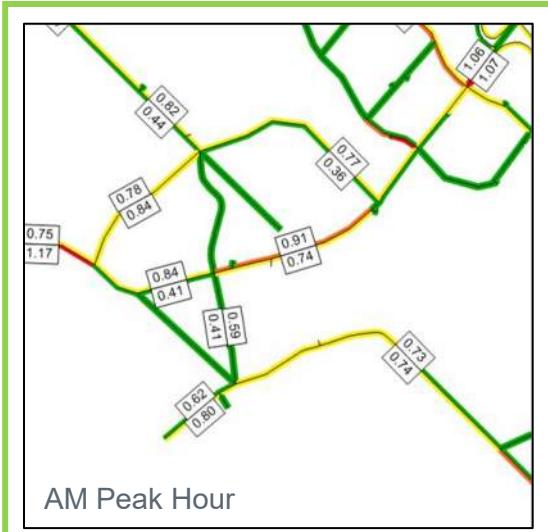


Focus Area 5 – Alternatives Assessment – Future 2051 AM Peak Hour



Focus Area 5 – Alternatives Assessment – Future 2051 PM Peak Hour





North Bypass

South Bypass

Bovaird Dr/Guelph St
Widening

Base Conditions:

- Bovaird over capacity by 300-400 vehicles under base conditions

Overview of Bypass Performance:

- E-W Bypass provides additional 900 vph of capacity per direction assuming same road classification as Bovaird Drive.
- To alleviate traffic congestion on Bovaird, only one bypass is required
- Both the north and south bypass options can accommodate forecasted east-west travel demand. South bypass operates less effectively than North option

Overview of Widening Performance:

- Widening on Bovaird Dr/Guelph St provides additional 900 vph capacity per direction
- Bovaird Dr/Guelph St at 4-lanes can accommodate forecasted east-west travel demand
- Widening results in increased (induced) demand through Norval community

Evaluation Criteria: Norval Focus Area

17

Category	Criteria	E-W Connection Focus Area – Alternatives			
		Do Nothing	North Bypass	South Bypass	Guelph St Widening
Transportation Criteria	Enhance sustainable mobility and multi-modal travel options	Screened Out	●	○	●
	Traffic Impacts		●	○	○
	Enhance connectivity for people and goods		●	○	●
	Integrate transportation and land use planning		●	○	●
	Protect public health and safety		●	○	●
	Leverage flexibility for future planning		●	○	○
Cultural Environment Criteria	Archaeologic	Screened Out	●	●	○
	Cultural and Built Heritage		●	○	●
Natural Environment Criteria	Terrestrial (species)	Screened Out	●	●	●
	Aquatic (watercourses, species at risk)		●	●	●
	Greenbelt & Natural Heritage Systems		●	●	●
	Stormwater & Surface Water (flooding, erosion)		●	●	●
Socio-Economic Criteria	Advance Transportation Equity	Screened Out	●	●	○
	Property Impacts/Requirements		●	●	●
	Nuisance Impacts (Noise, Vibration & Air Quality)		●	●	●
	Ease of Construction (Phasing, Geotechnical)		●	●	●
	Cost (Capital, Operating & Maintenance)		●	●	●

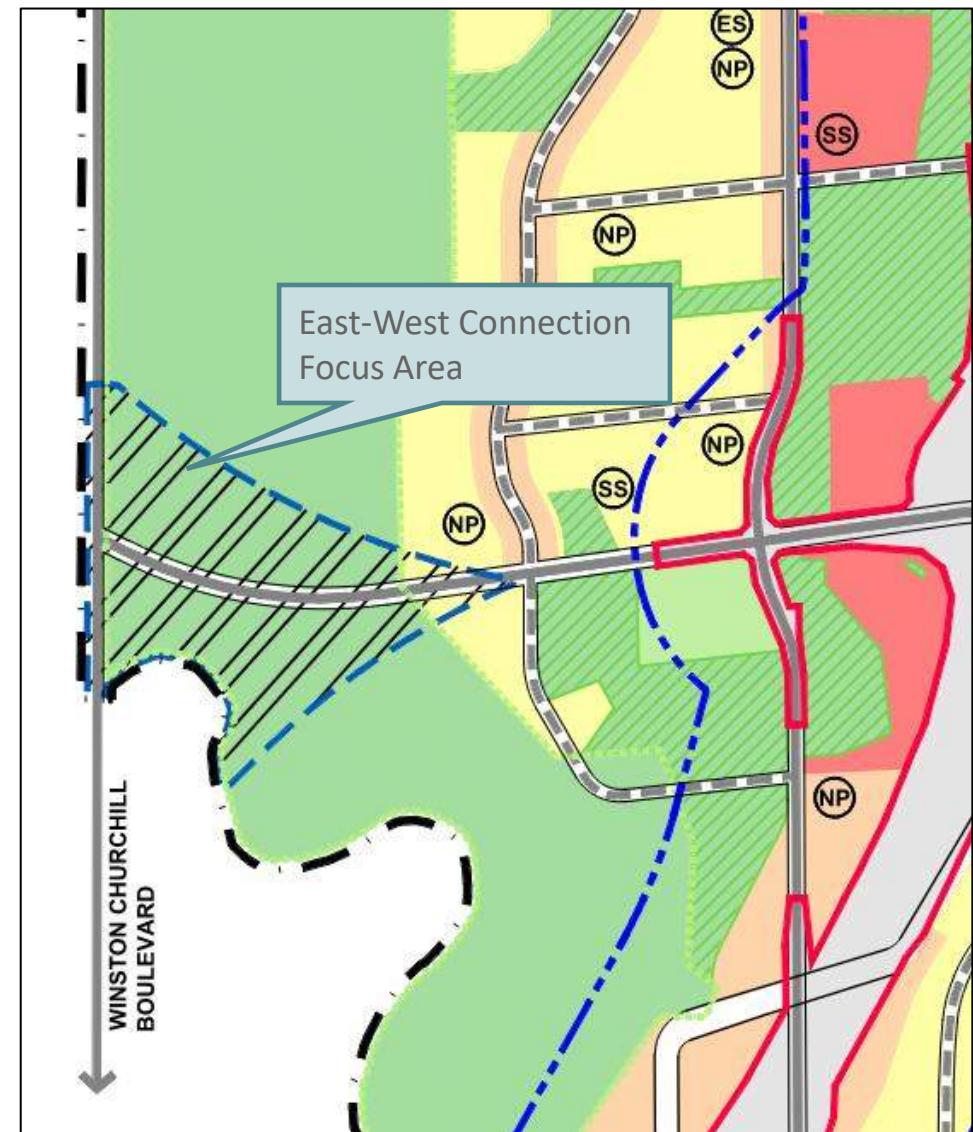
- This is an **illustration** and does not preclude any studied alternative.
- This evaluation criteria is in **draft** format and intended to reflect high level evaluation.
- Mitigation measures not explored at this time.
- **No recommendation made at this time.**

- **Most Preferred**
- **More/Less Preferred**
- **Least Preferred**

Next Steps – Heritage Heights TMP

18

- Heritage Heights TMP (instead of Precincts) will:
 - Document capacity results and illustrative criteria assessment
 - Recommend that the East-West Connection Focus Area be maintained to not preclude solutions for a future East-West Connection EA.
 - PIC for TMP planned for August 2025
 - Final TMP to be published Fall 2025



Modelling

Linda Wu

[REDACTED]
Modelling and Analytics
Transportation Planning
City of Brampton

Adrian Lorion

[REDACTED]
Associate
BA Consulting Group Ltd.

Yahya Deen

[REDACTED]
Lead Analyst
BA Consulting Group Ltd.

TMP Project Management

Richa Dave, MCIP RPP

[REDACTED]
Project Manager, Transportation Planning
City of Brampton

Emily Ecker, P.Eng.

[REDACTED]
Senior Associate
BA Consulting Group Ltd.

Project Email: heritageheights@brampton.ca



MOVEMENT IN URBAN ENVIRONMENTS



MEETING MINUTES

TO

Meeting Attendees (June 24, 2025)

LOCATION

Virtual (Zoom)

FROM

BA Consulting Group Ltd.

DATE

June 24, 2025

RE: HERITAGE HEIGHTS TRANSPORTATION MASTER PLAN – EAST-WEST CONNECTION FOCUS AREA ENGAGEMENT #2 – JUNE 24, 2025 – VIRTUAL DRAFT

ATTENDEES

City of Brampton Attendees

- Richa Dave
- Vikram Hardatt
- Brian Lakeman
- Nelson Cadete
- Henrik Zbogar

Town of Halton Hills Attendees

- Maureen Van Ravens
- Ivan Drawnitski
- Melissa Ricci

Halton Region Attendees

- Shelley Partridge
- Walter Scattolon
- Andrew Morgan
- Ann Larkin
- Lina Elmorshedy

Consultant (BA Group)

Attendees

- Emily Ecker
- Scott Gibbons
- Adrian Lorion
- Michael Giallonardo
- Yahya Deen
- Hansen Rao
- Kevin Xu

Heritage Heights Landowners Group Attendees

- James Reed

Peel Region Attendees

- Dana Jenkins
- Yifan Shen

REGRETS

City of Brampton Regrets

- Dave Monaghan

Town of Halton Hills Regrets

- Jeff Markowiak
- Josh Salisbury
- Bronwyn Parker

Consultant (BA Group) Regrets

- Cora Freudenberg

PRESENTATION (YAHYA DEEN & EMILY ECKER)

- Emily E. introduction and recap of previous meeting.
- Yahya D. delivered PowerPoint presentation outlining the Heritage Heights Transportation Master Plan (TMP) and Modelling Framework.
 - Maureen V. R. asked about turning movement counts from 2016 and then 2024 – did we have them for all intersections? Yahya D. answered that we received data from Brampton and Peel, calibrated in 2021, but based on 2016 TTS (clarified by Brian L.)
- Yahya D. and Emily E. detail HPBATS study maps and the alignment options for Norval Bypass as considered in 2010.
- Base scenario (No Norval Bypass) outlined in 2051 (ultimate). Volumes on Guelph Street exceed capacity by 30-45%. V/C ratios are over 1.00.
 - Maureen V.R. asked if Highway 413 volumes were assumed as part of base assumptions. Yahya answered that yes they were included as part of base scenario. Maureen V.R. recently (last week) received Highway 413 interchange volumes and mentioned 39,000 daily trips.
 - Ann L. mentioned Winston Churchill volumes also play into the consideration.
 - Adrian L. asked if Maureen V.R. knew how the MTO derived their volumes. Maureen V.R., just received them on a sheet.
- BA scenarios:
 - Base
 - North Bypass (would be 2 lanes)
 - South Bypass (would be 2 lanes)
 - North + South Bypass (would both be 2 lanes)
 - Bovaird / Guelph Street Widened to 4 lanes (2 lanes per directions)
- North Bypass and South Bypass both alleviate Guelph Street volumes, North Bypass is more effective as it's a more direct route whereas South Bypass is more circuitous.
- Scenario with North + South Bypasses is overdesigned, scenarios with just one of them would alleviate Guelph Street demand.
- Widening of Guelph Street would bring capacity above demand but there would be induced/increased demand; more volumes than if no widening because there's no bypass.
- Key Takeaways from BA presentation:
 - Both of the Bypass options and the widening of Guelph Street would fix capacity issue in 2051 base condition.
 - Maureen V.R. says Bovaird Drive / Guelph Street is the MTO; it's Highway 7. It's a moot point as they're not at the table. Emily E. said that we have looked at it out of due diligence and due to challenge of the other options. Maureen V.R. advised that if we take that option forward, MTO must be at the table.

- Emily E. walked team through evaluation criteria table. Reiterated that these are not recommendations.
 - Ann L. asked if we don't have recommendations at this time, what are the implementation next steps. Asking about BA bullet point to maintain East-West connection Focus Area to not preclude solutions. Emily explained that the Norval area is located in Halton Hills, not in Brampton. This area (i.e. cone) won't be developable as part of the Heritage Heights Transportation Master Plan (as it is in the Heritage Heights Secondary Plan). Continuation of HPBATS should be it's own Environmental Assessment.
 - Ann L. asked if any alignments will be developed. Emily E. said BA has drafted alignments but we have not gone into species locations, environmental issues. Vikram H. said Brampton is hesitant to get into alignments because drawing a line on a map because it can "snowball down the road."
 - Ann L. said how do we protect East-West Connection Area? Vikram H. said continue to show the cone in City policy and in Precinct Planning and Draft Plan applications.
 - Shelley P. asked to clarify that it is through the Precinct Plans that the East-West Connection Focus Area will be preserved. Vikram confirmed this.
 - Shelley P. asked if we envision if the cone will remain the same size. Vikram H. speculated that it would remain the same size. Henrik Z. advised that this cone (East-West Connection Focus Area) is in the approved Secondary Plan. It will be carried through Precinct Planning emanating from the Secondary Plan.
- Yifan S. noted planned future road widening of Bovaird Drive West between Winston Churchill Road and Heritage Road. Brian L. noted this is factored into the City's macro-model.
- Walter S. said evaluation criteria table is very typical; what's the next level to drill down and detail each evaluation? Could there be rankings of risk, per-metre costs, and then figurative value of things like socio-economic impacts?
 - Emily E. advised a scenario how we can cost the North Bypass, as an example. There are costs to add up and consider but some items may ultimately be "critical failure" which could take an option off the table. Brian L. advised that if this was a full EA, we would dive more deeply into each criteria; today's table is just a first pass.
- Maureen V.R. noted that this work confirms that there will be "a problem" with the Norval area. Halton Hills have asked the MTO to do analysis to understand impacts to the Town's road network; maybe ultimately, they will take ownership of Highway 7 and potentially pay \$ for the ultimate solution.
- Maureen V.R. understands desire to complete Heritage Heights TMP and designate HPBATS as future EA. Therefore, municipalities to continue working on this issue into the future beyond completion of the Heritage Heights TMP. Ultimately, Highway 413 will have traffic impacts.
- Maureen V.R. noted this information is good and should be shared with the MTO.

MEETING OUTCOME AND NEXT STEPS

- There are PICs that are upcoming.
- If municipalities have further comments, please share.
- Municipalities will see the TMP when it is published.

HH TMP Engagement #2 - June 24-2025

Agenda

-

Attendees

- BA, City of Toronto, Peel Region, Halton Region, Halton Hills

Meeting Related Tasks



Notes

- Vikram - intro
- Emily - goes over some of the upcoming milestones
- Yahya - presenting HH modelling material
 - Modelling framework, study area, assumptions, calibration, etc.
- Maureen - asked about turning movement count coverage in calibration, Yahya described the extent of data used, Brian clarified that the City updated their existing model in 2021 utilizing 2016 TTS data
- Yahya - continued discussing model development inputs
 - Growth assumptions, base model, forecasts
- Maureen - asked about 413, recently received interchange volumes from MTO, MTO didn't detail how these were derived
- Yahya - focus area 5 alternatives overview, analysis results
 - modelling takeaways
- Maureen - comment about widening and including MTO in that discussion
- Emily - going over evaluation criteria table
- Ann, Emily, Vikram - discussion of further study, discussed how we protect for east-west connection focus area

- Yifan - planned widening east of winston chuchill, west of heritage rd
- Walter - How would we expand on the evluation criteria
- Brian - highlighted that the criteria would be reviewed in more detail as part of the EA
- Maureen - this work highlights that there is an issue and this needs to be studied more, halton region will be doing their own study on hwy 413 volumes
-



HERITAGE HEIGHTS TRANSPORTATION MASTER PLAN

Peel Region Engagement 1

Prepared For: Peel Region

May 16, 2025



BRAMPTON

*Heritage Heights
Landowners Group*



BA Group

- Introductions
- Heritage Heights Secondary Plan
- Transportation Master Plan (TMP) Process Overview
- Transportation Mesoscopic Model – How it's being Used to Inform Recommendations to Road Network
 - Multi-resolution modelling methodology
 - Forecasted Future 2051 Operations – Base Conditions and Alternatives
- Intersection Spacing Along Mayfield Road Adjacent to Highway 413
- Peel Region Pumping Station Crossing in Precinct 52-1

- City of Brampton
 - Vikram Hardatt
 - Richa Dave
 - Brian Lakeman (regrets)
 - David Monaghan
 - Nelson Cadete
- Heritage Heights Landowners Group
 - James Reed
- BA Group
 - Emily Ecker
 - Michael Giallonardo (regrets)
 - Yahya Deen
 - Adrian Lorion
 - Scott Gibbons (regrets)
 - Cora Freudenberg
 - Kevin Xu
 - Hansen Rao
- Peel Region
 - Dana Jenkins
 - Hashim Hamdani (regrets)
 - John Hardcastle
 - Lindsay Edwards (regrets)
 - Marzuq Shamsi (regrets)
 - Rani Kol
 - Robert Jay
 - Sabrina Khan
 - Sally Rook
 - William Turner (regrets at ~9:30)
 - Catherine Barnes

Heritage Heights Secondary Plan

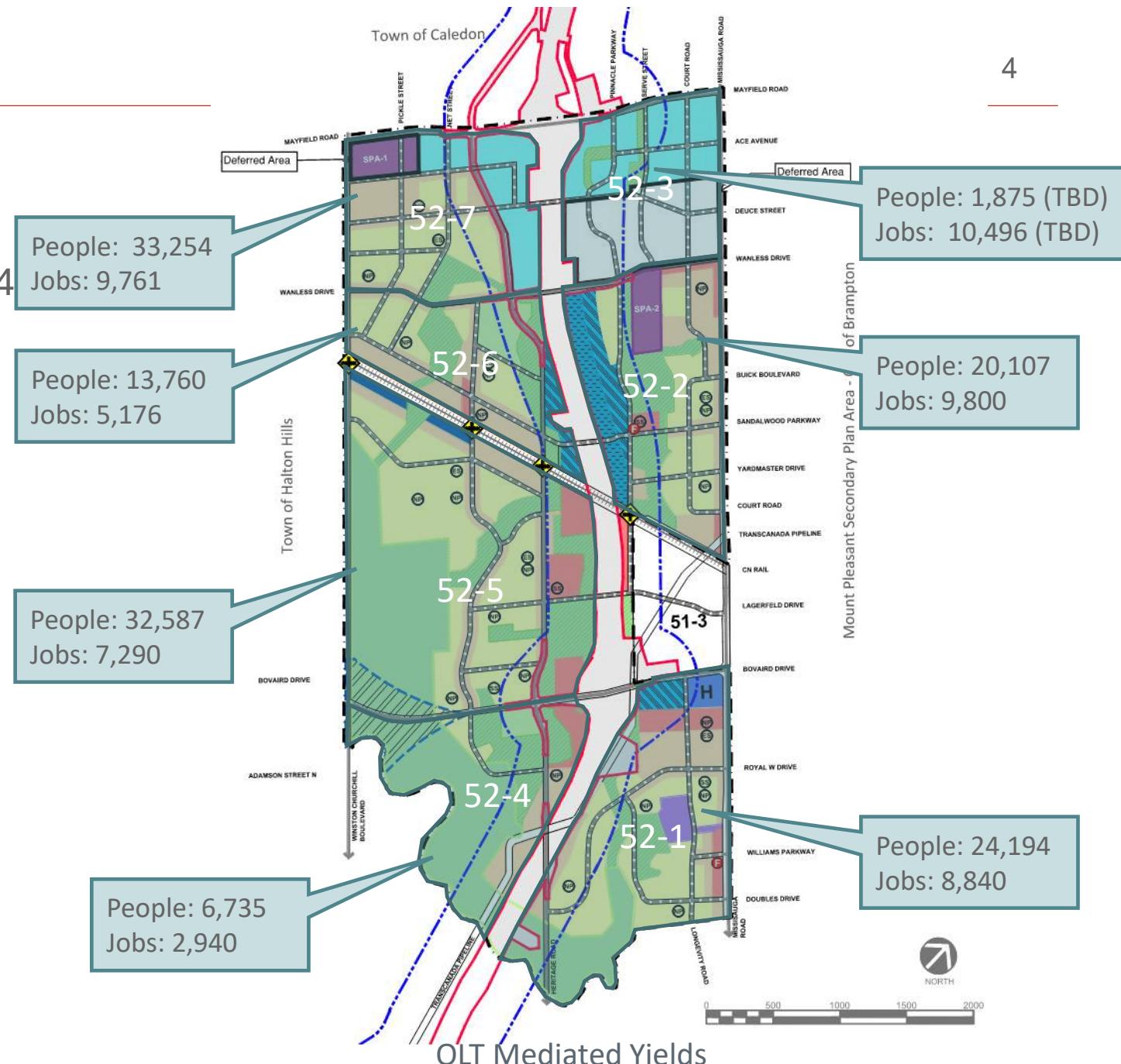
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The Secondary Plan

- Secondary Plan Approved August 21, 2024

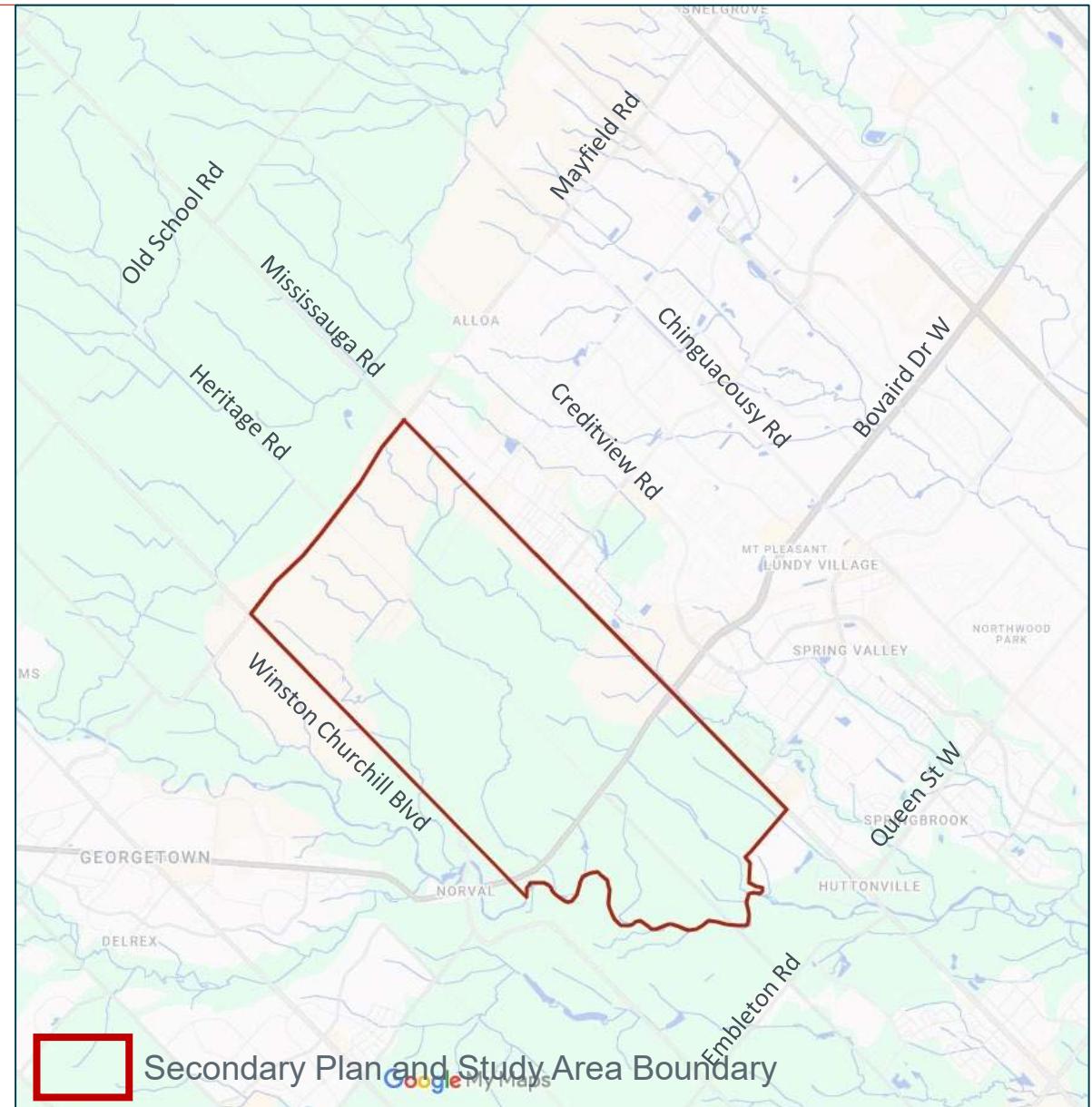
Supporting Studies Underway

- Infrastructure Servicing Study (ISS)
- Subwatershed Study (SWS)
- Transportation Master Plan (TMP)



The Study

The City of Brampton and the Heritage Heights Landowners Group are working together as co-proponents to undertake a Transportation Master Plan (TMP) to identify the transportation infrastructure required to support the growth envisioned for the Heritage Heights Secondary Plan Area.



MCEA Process

- The TMP is being undertaken in accordance with **Approach #2** of the Master Planning Process, as outlined in the Municipal Class Environmental Assessment (MCEA) (February 2024).
- The TMP will address Phases 1 and 2 of the MCEA process for any Schedule B projects and will form the basis for the recommended Schedule C transportation infrastructure projects identified within the TMP report.

4.5 Master Planning Process

The master planning process is discussed in Section A.2.7. A summary of the various approaches is provided below.

	Approach 1	Approach 2*	Approach 3*
Level of Detail	Broad (project specific information is minimal) Used as support for subsequent Schedule B and C project specific studies	Sufficient to fulfil requirements for Schedule B projects (more detailed project specific information is included) Used as support for subsequent Phase 3 and 4 for Schedule C project specific studies	Sufficient to fulfil requirements for Schedule B and C projects (more detailed project specific information is included)
Process	Preliminary Phase 1 and 2	Phase 1 and 2	Phase 1 to 4
Final Notice	Notice of Master Plan *This should not be called a Notice of Completion	Final notice becomes Notice of Completion for Schedule B projects	Final notice becomes Notice of Completion for Schedule B and C projects
Integration with Planning Act*	*Official Plan and Secondary Plan	*Official Plan and Secondary Plan, Plan of Subdivision, etc.	*Official Plan, Secondary Plan, Plan of Subdivision, etc.

* The various master planning approaches provide proponents with flexibility to customize their master plans to suit their needs. See section A.2.7 for information on modified approaches 2 and 3.

Modelling Framework and Results

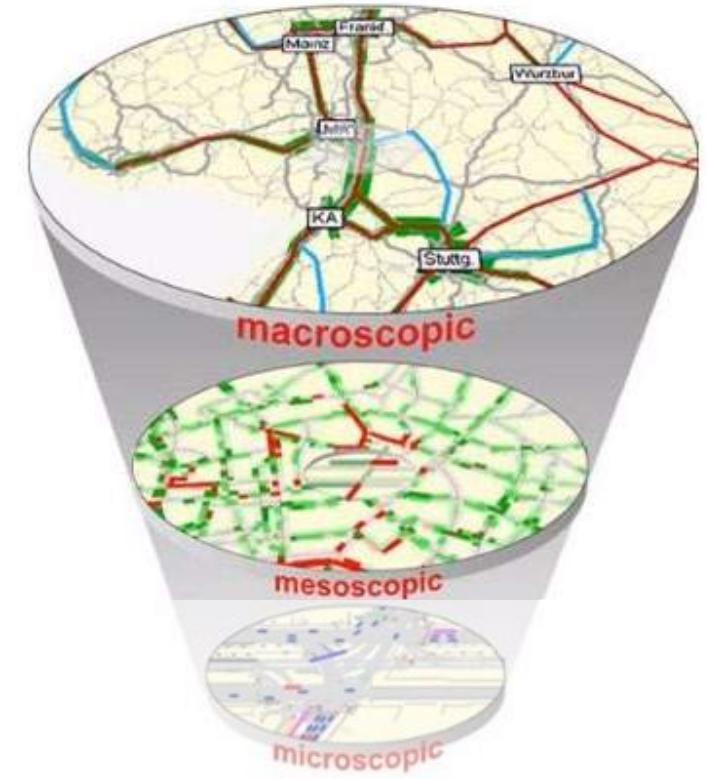
Overview of Demand Patterns Through Meso-Model Study Area

Post-Processing Adjustments to Demand

Base Scenario Results and Focus Areas for Network Improvements

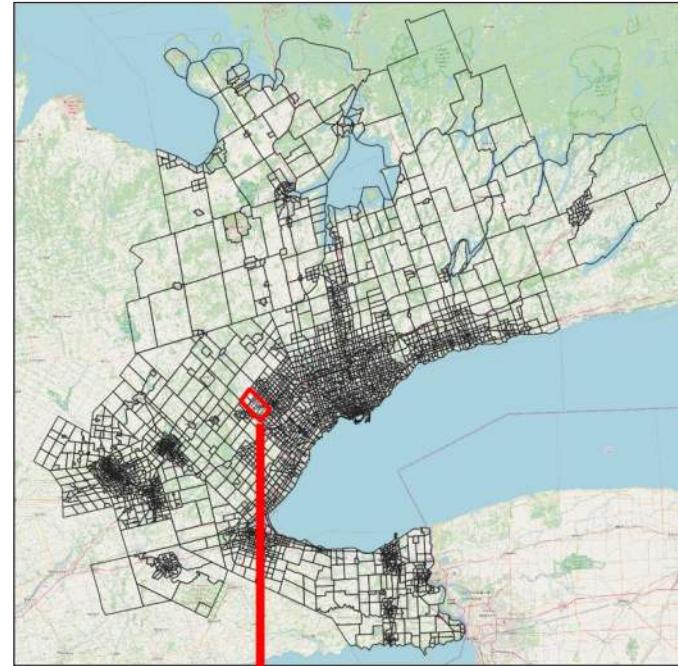
Road Network Alternatives and Associated Results

- The objective is to establish a modelling framework that can support the TMP and respond to the planning needs of the City, Region and the LOG.
- BA Group has adopted a standard multi-resolution and multi-modal modelling approach (macro/meso), which relies heavily on collaboration with City modelling staff.
- BA Group has developed a large scale mesoscopic *Visum* model that encompasses the Heritage Heights Transportation Master Plan area.
- This approach ensures that BA Group's mesoscopic model captures the effects of both large-scale regional planned transportation infrastructure improvements and regional population/employment growth considered in the City's macroscopic travel demand model while producing detailed results at a corridor-level.



- BA Group Mesoscopic model study area bounds:
 - Old School Rd to the North
 - Chinguacousy Rd to the East
 - Queen St W/Embleton Rd to the South
 - Winston Churchill Blvd to the West
- Subarea Origin-Destination travel demand matrices were extracted from City of Brampton's Regional Demand Model and were used as inputs into the Mesoscopic Model
- Existing conditions mesoscopic model:
 - Developed with existing conditions (2016) sub-area travel demand matrices from the City's macroscopic model.
 - Calibrated with 2024 field collected turning movement count data.
- Future conditions mesoscopic models:
 - Developed with future conditions (2051) sub-area travel demand matrices from the City's BMP macroscopic model.
 - Used to evaluate road network options (e.g., road locations, number of lanes, network connectivity, etc.)
 - Used to evaluate impacts on traffic operations due to transit network options

Macroscopic Model
City of Brampton's Macroscopic Model



Mesoscopic VISUM Model
BA Group's HH TMP Mesoscopic Model

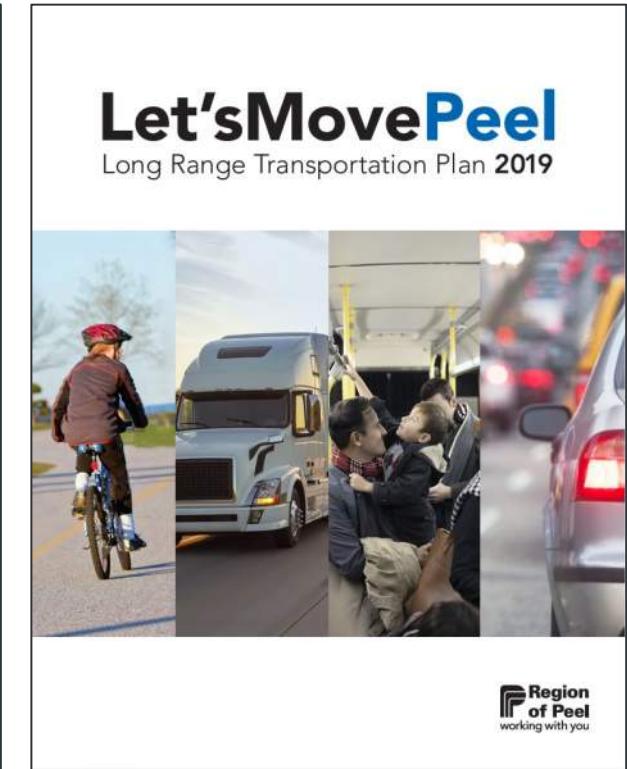
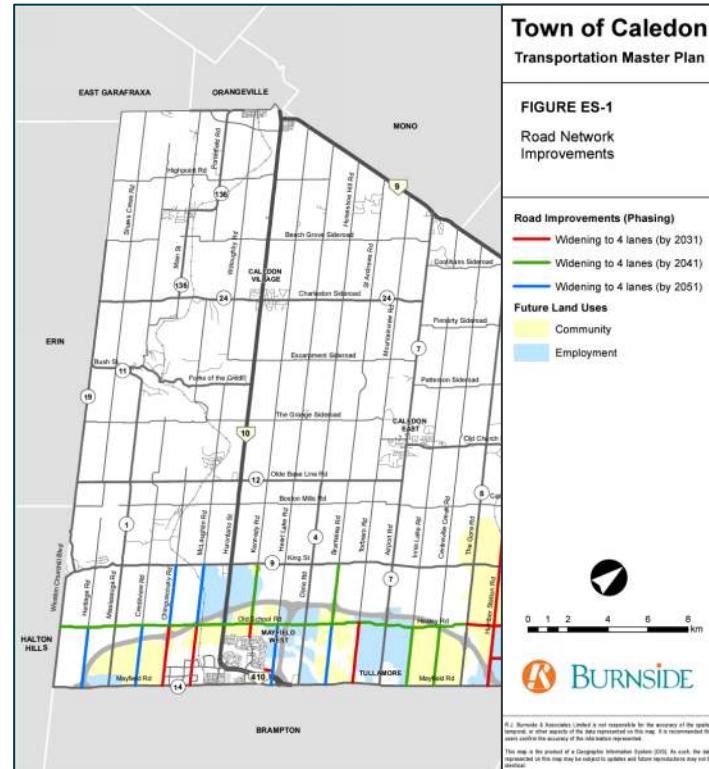
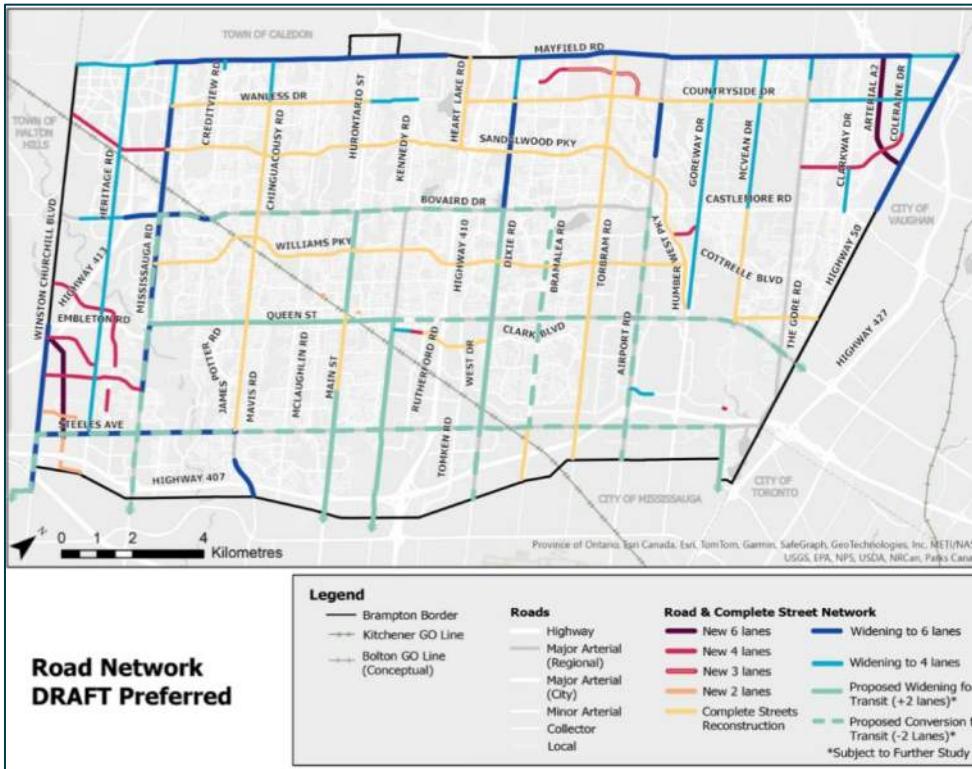


Subnetwork Matrices

	1	2	...	n
1	a_{11}	a_{12}	...	a_{1n}
2	a_{21}	a_{22}	...	a_{2n}
...	⋮	⋮	⋮	⋮
m	a_{m1}	a_{m2}	...	a_{mn}

2051 Future Road Network Improvements – BMP, Town of Caledon MMTMP, Peel LRTP

10

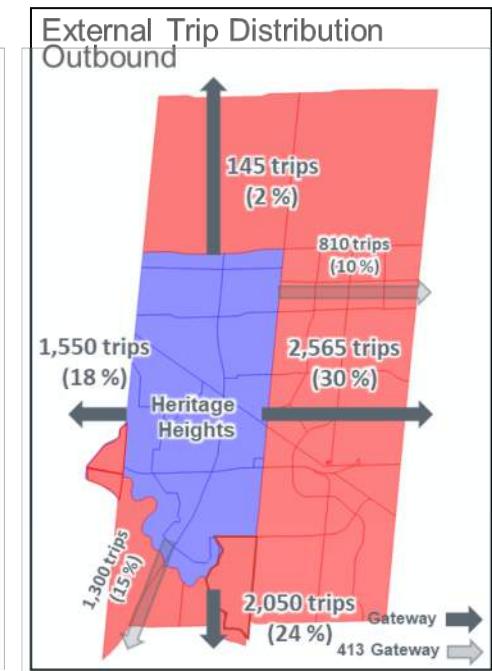
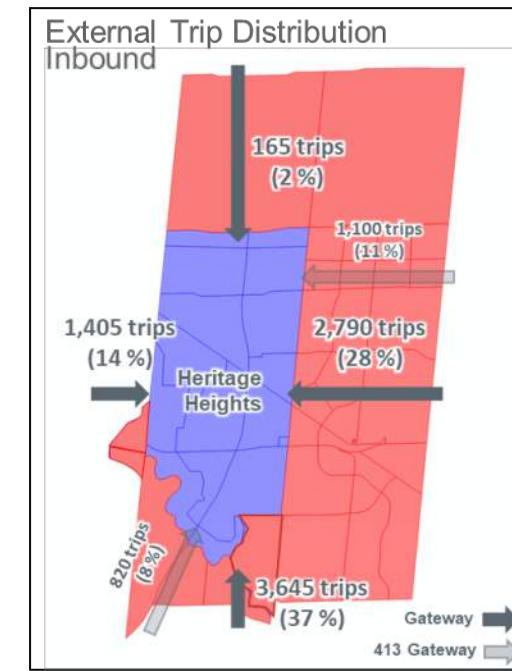
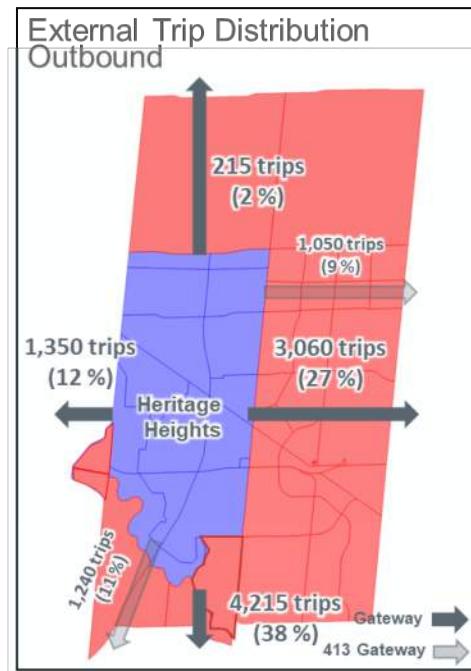
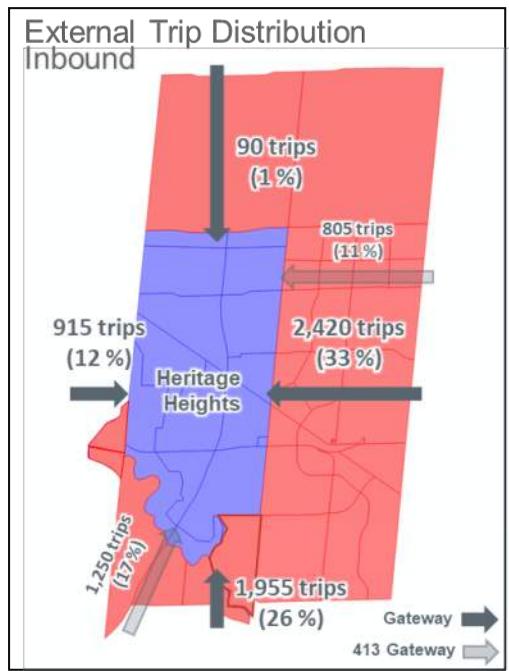


- BMP, alongside with 2024 Town of Caledon MMTMP and Peel Region's LRTP will inform the road infrastructure improvements to be included in the base 2051 mesoscopic model
- Halton Hills TMP, Halton Region TMP, and Halton-Peel Boundary Area Transportation Study may also be used as part of this study

Future Conditions Macroscopic Model

Heritage Heights Secondary Plan, Trip Generation and Distribution

11



Heritage Heights Auto Trips (AM Peak Hour)

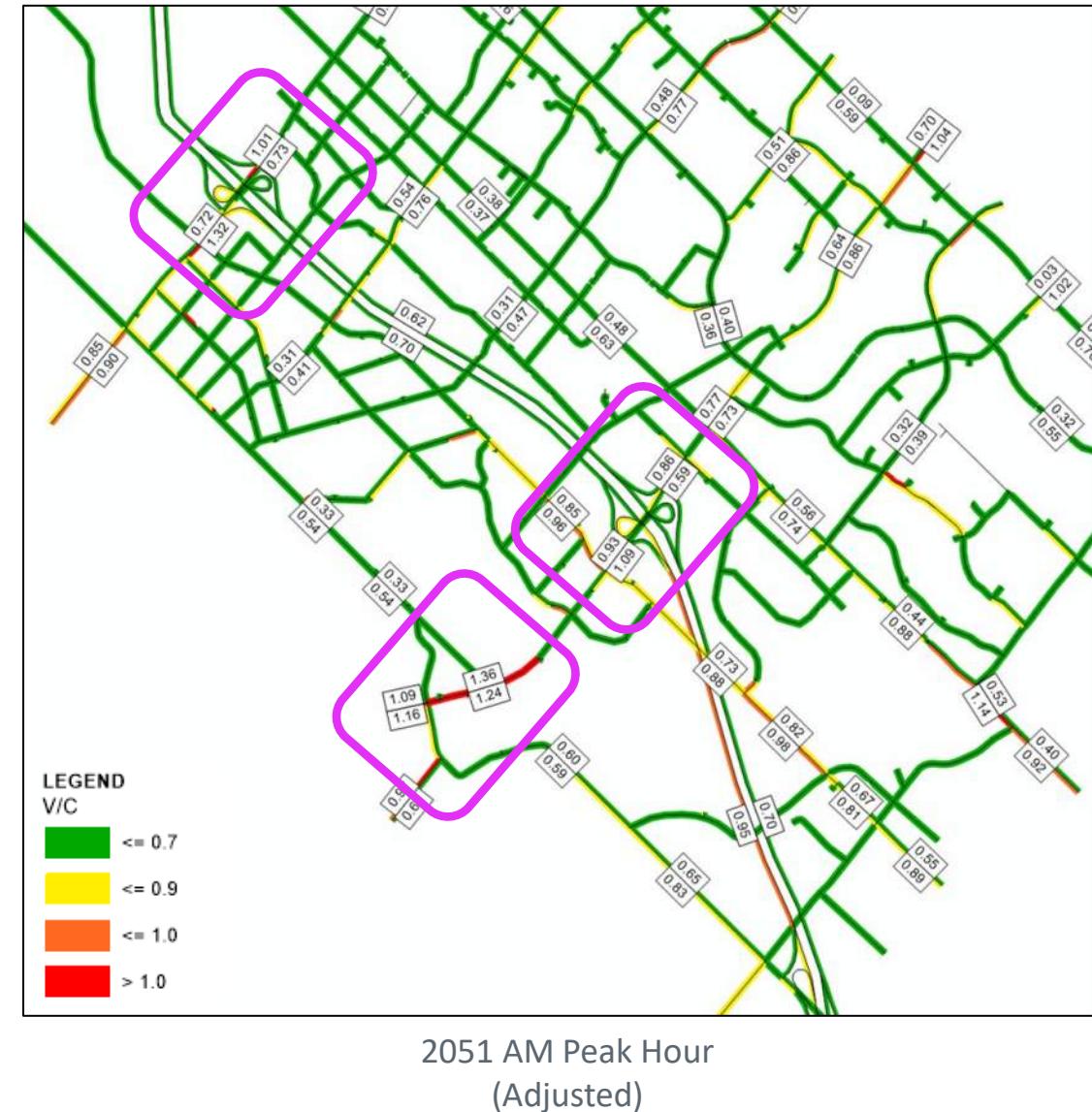
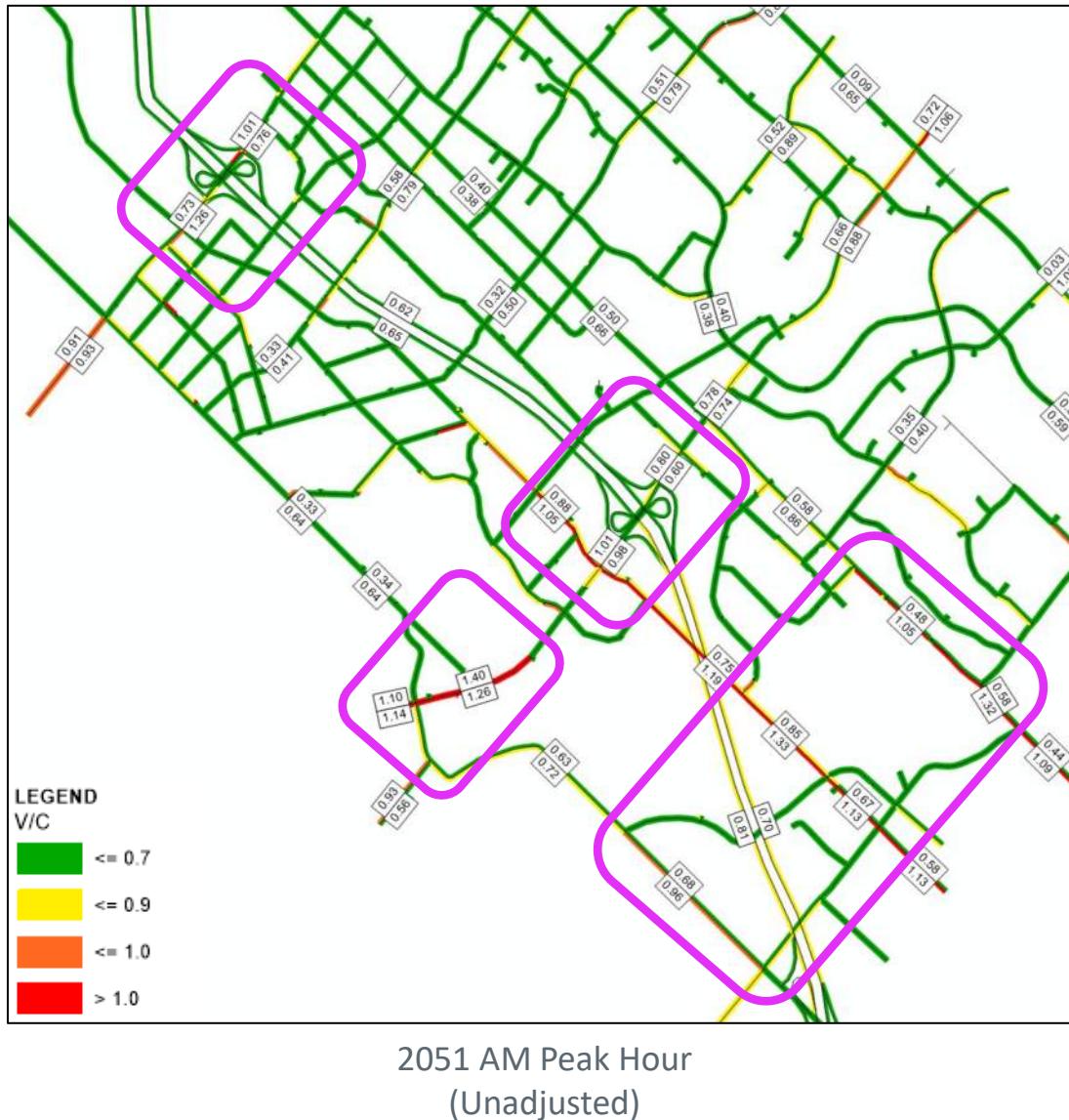
Outbound Trips	Inbound Trips	Internal (Heritage Heights) Trips
14,560	11,755	1,720 (12%)

Heritage Heights Auto Trips (PM Peak Hour)

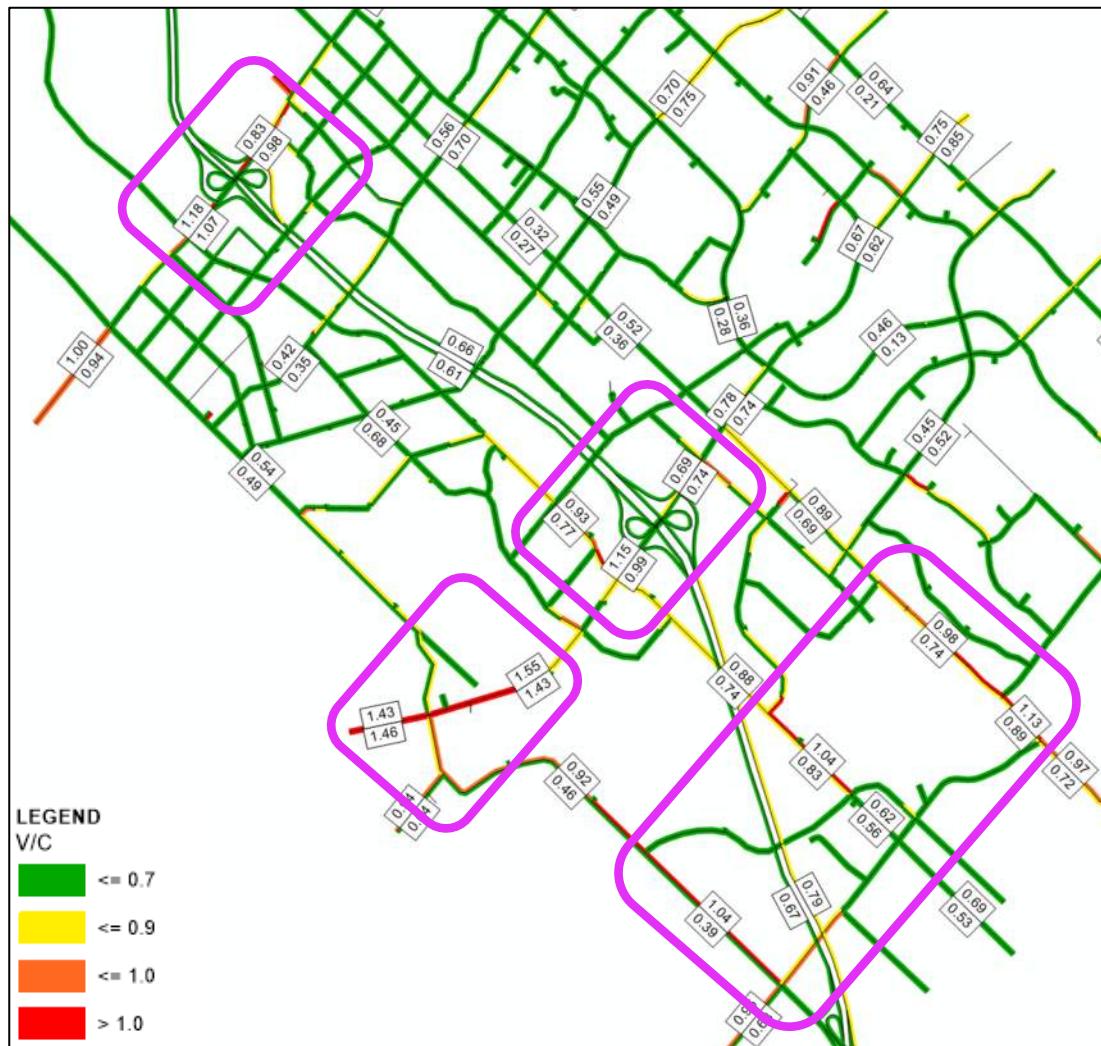
Outbound Trips	Inbound Trips	Internal (Heritage Heights) Trips
15,021	15,802	2,920 (19%)

- BMP targets a 35% non-auto mode share (65% auto)
- In consultation with the City of Brampton, post-processing adjustments to sub-area matrices were made to account for increased non-auto mode share** – this will ensure modelling tool is suitable to provide recommendations that fit within vision for Heritage Heights
- Post processing adjustments will aim to replicate higher transit usage for trips to/from HH, a conservative reduction of 5% was applied to the auto mode share. Reduction was targeted at trips between HH and the East and South gateways of the mesoscopic model

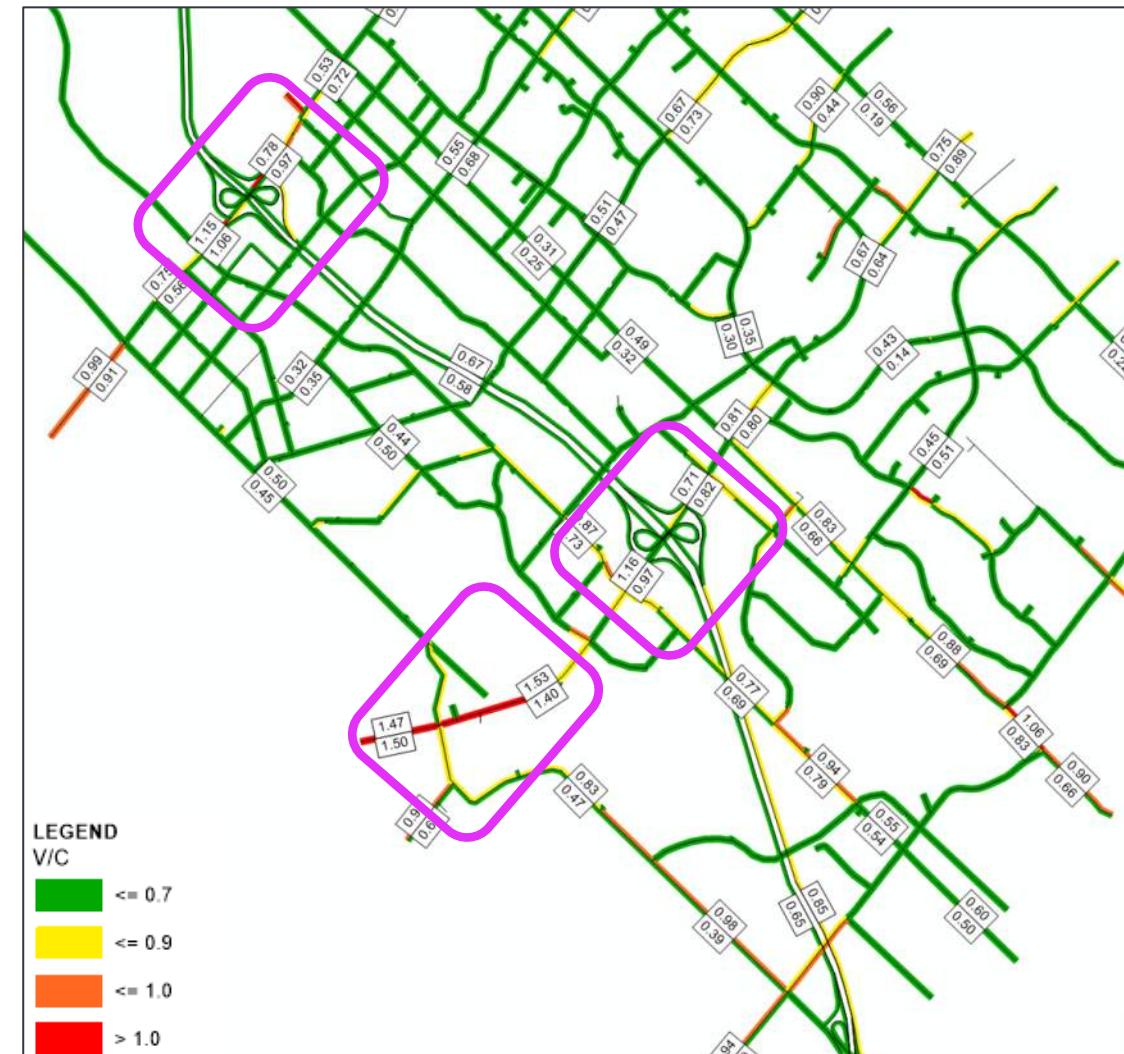
Mesoscopic Model Results – Volume-Capacity Ratios – AM Peak Hour



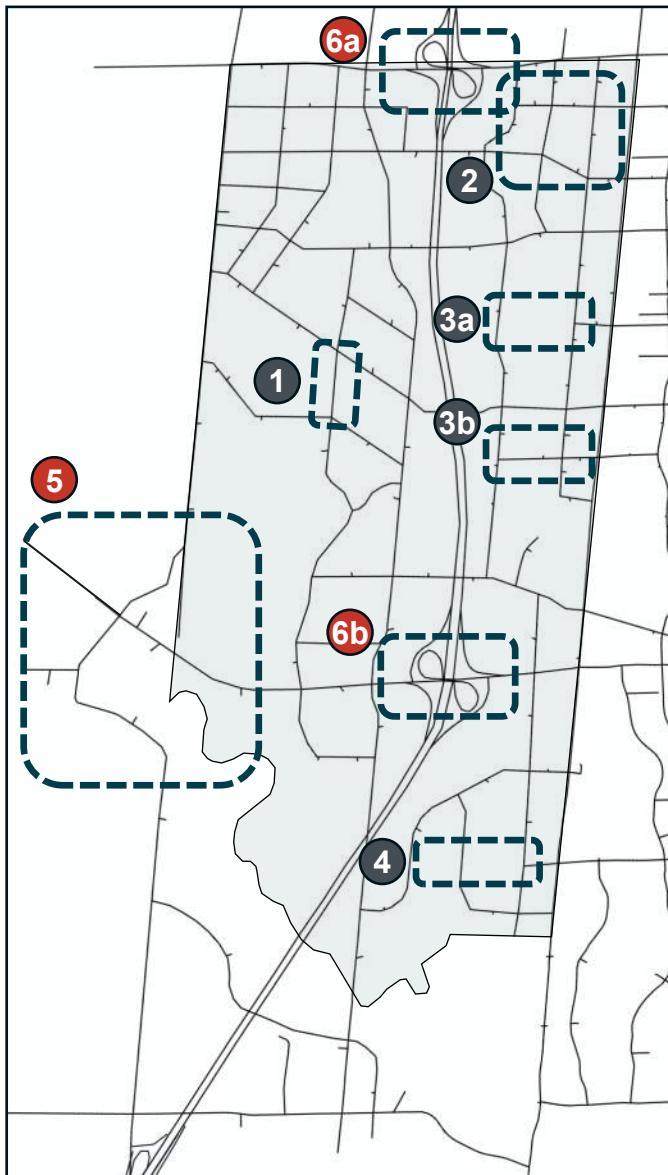
Mesoscopic Model Results – Volume-Capacity Ratios – PM Peak Hour



2051 PM Peak Hour (Unadjusted)



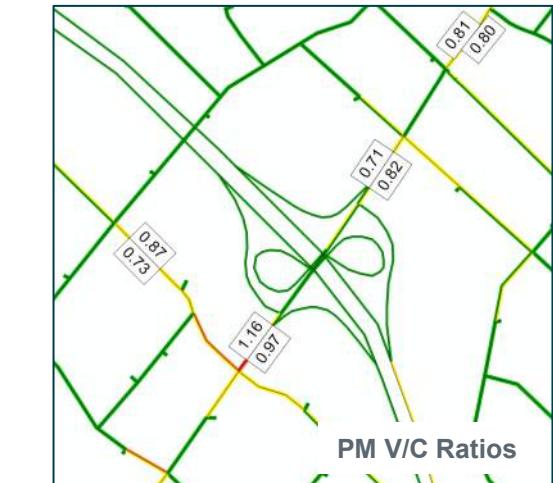
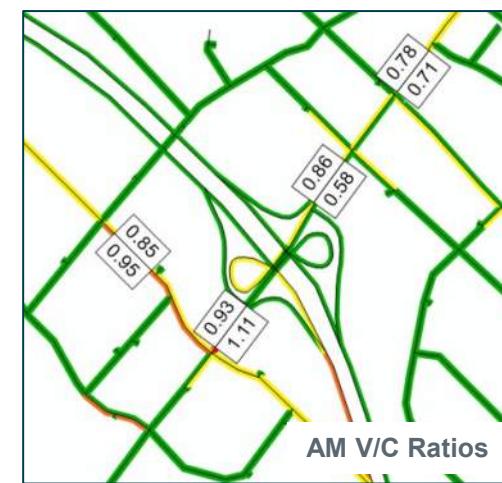
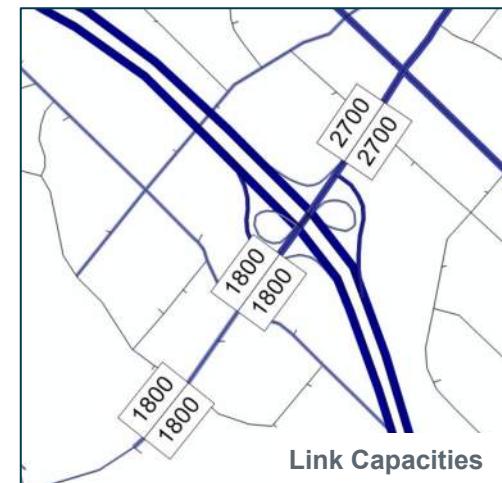
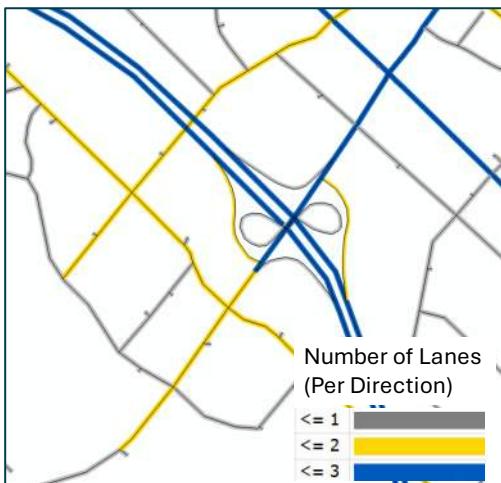
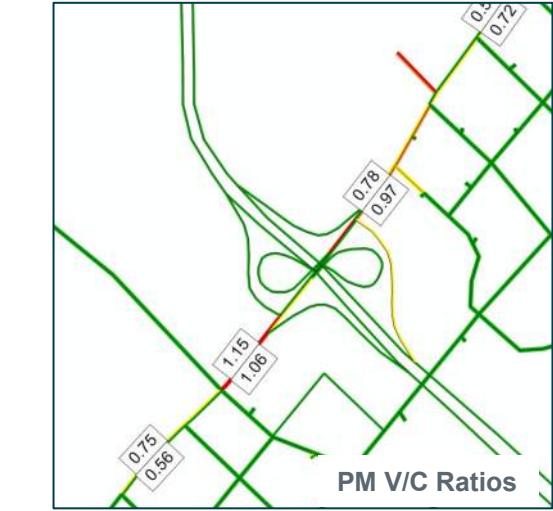
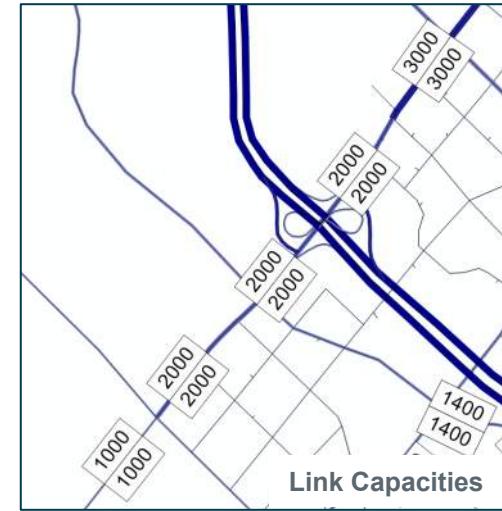
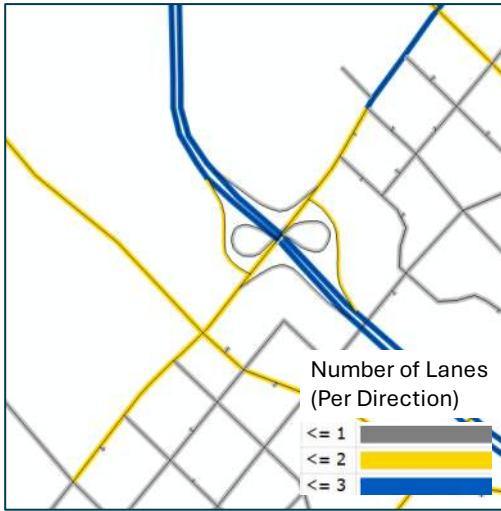
2051 PM Peak Hour
(Adjusted)



Network Alternatives:

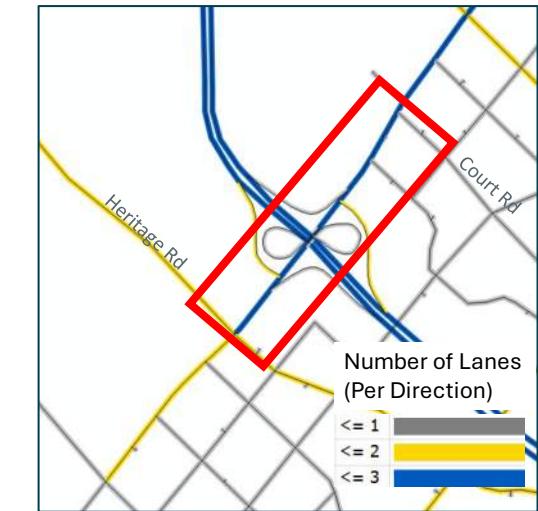
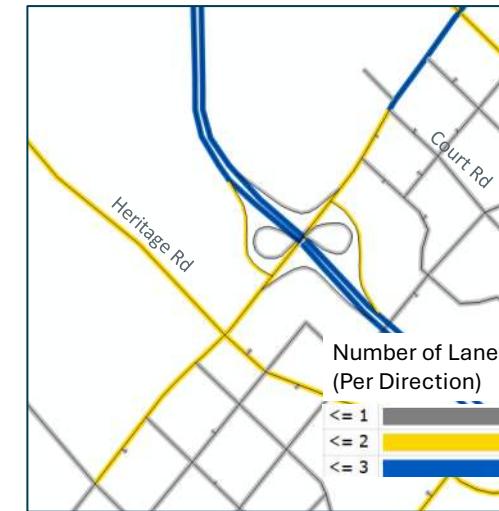
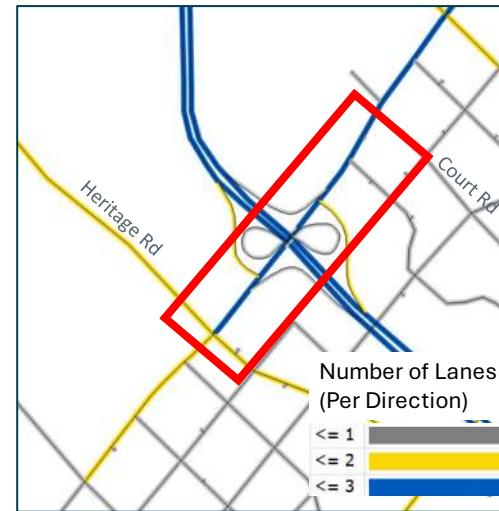
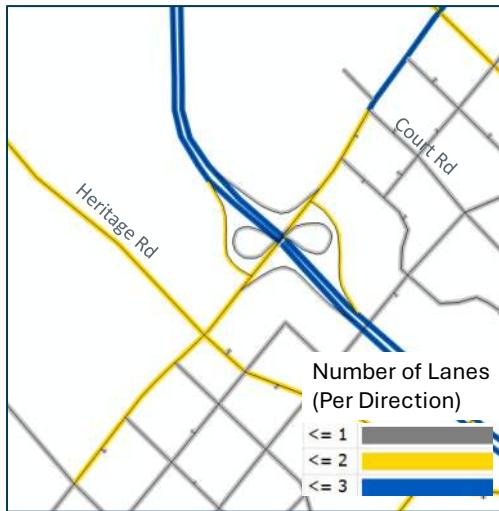
1. Testing the need for rail north-south crossing
2. Precinct 52-3 road network alternatives
3. Assessment of E-W crossings (Buick and Yardmaster) in 52-1
4. Williams Parkway Extension
5. Norval bypass/HPBATS E-W Connection
6. Refinements to number of lanes on Bovaird and/or Mayfield Rd to support traffic to/from 413

Focus Area 6 – Base Road Network Assumptions and Operations

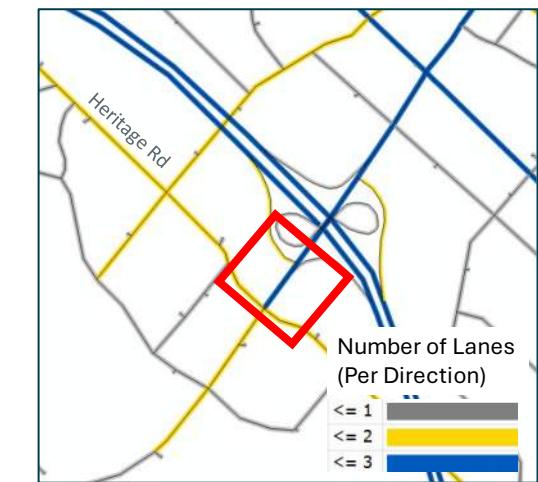
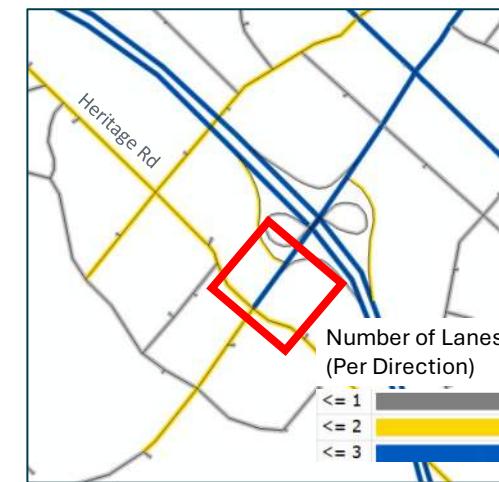
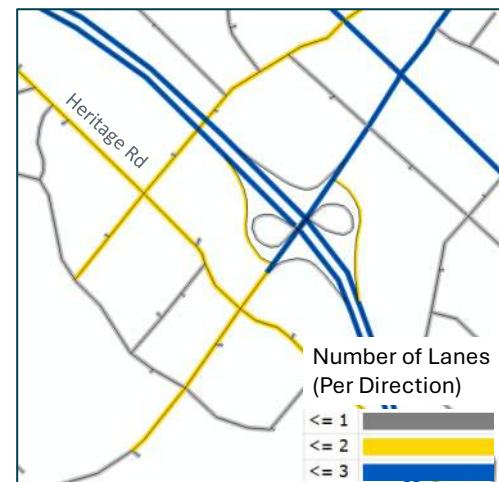
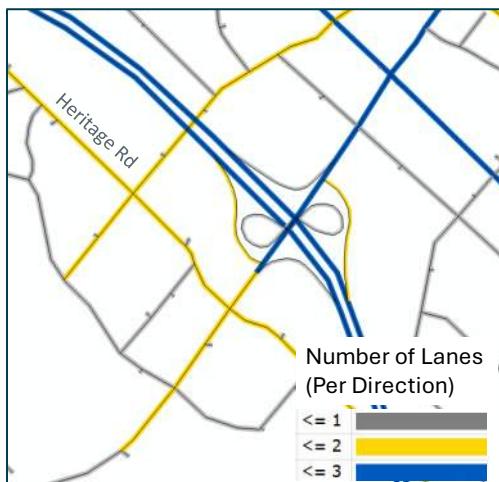


Focus Area 6 – Alternatives

Mayfield Rd



Bovaird Dr



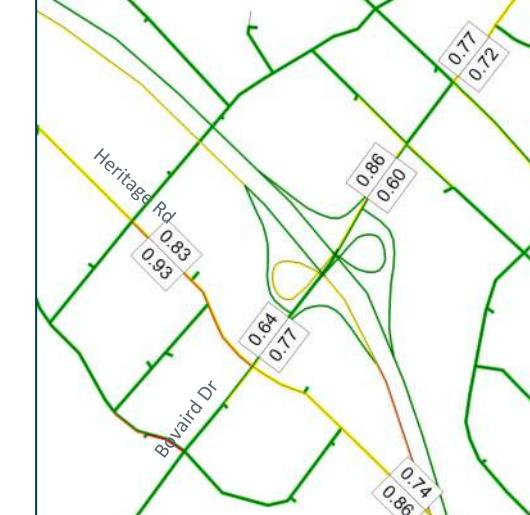
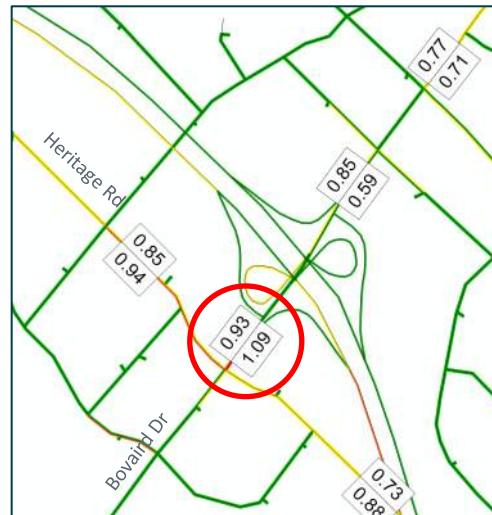
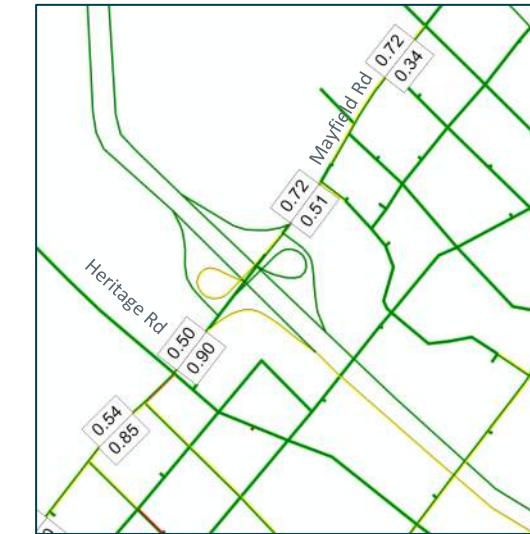
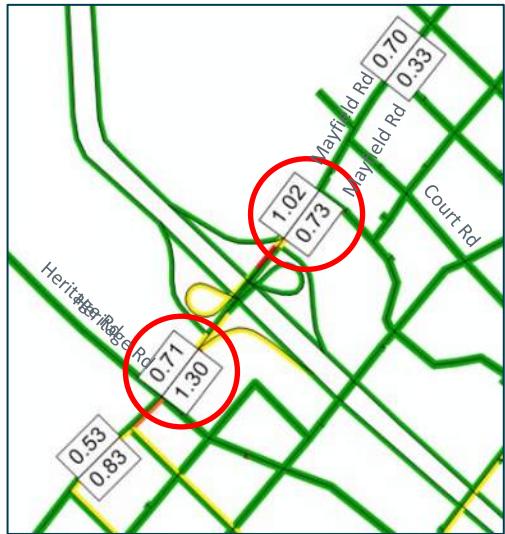
Base

Alt 1
Mayfield 6 Lanes

Alt 2
Bovaird 6 Lanes

Alt 3
Both 6 Lanes

Focus Area 6 – Alternatives Assessment – V/Cs – AM Peak Hour



Base

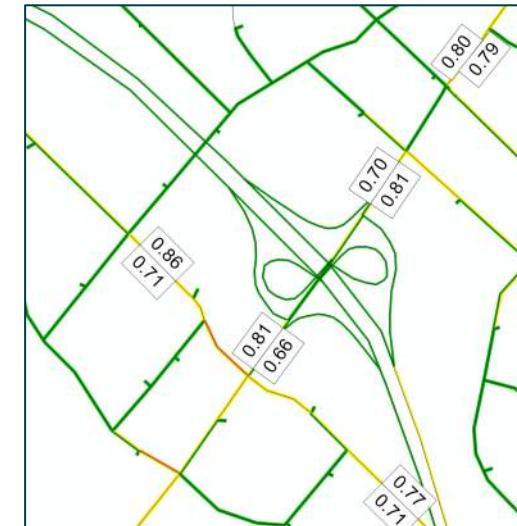
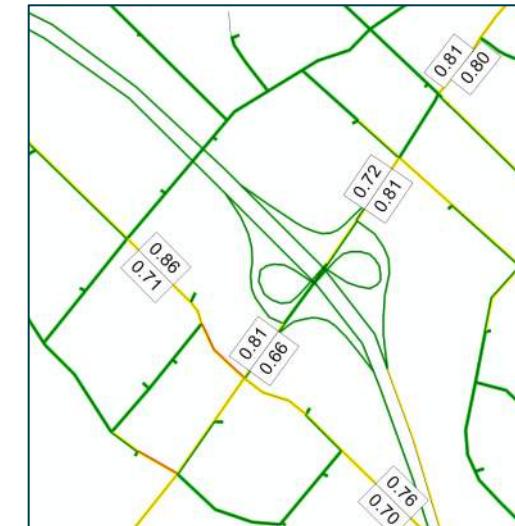
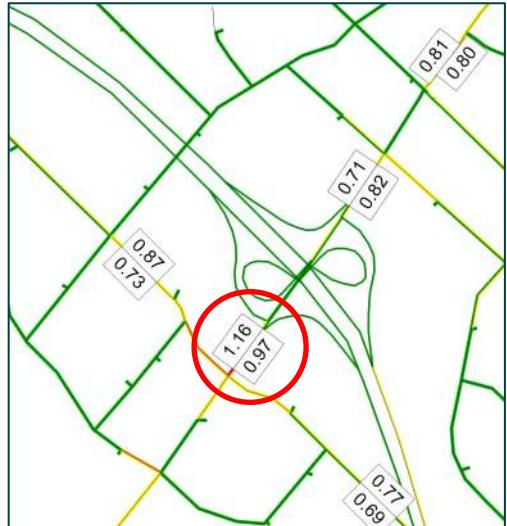
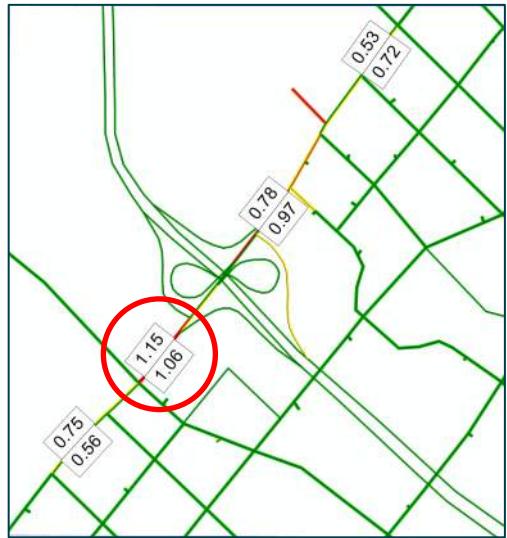
Mayfield 6 Lanes

Bovaird 6 Lanes

Both 6 Lanes



Focus Area 6 – Alternatives Assessment – V/Cs – PM Peak Hour



Base

Mayfield 6 Lanes

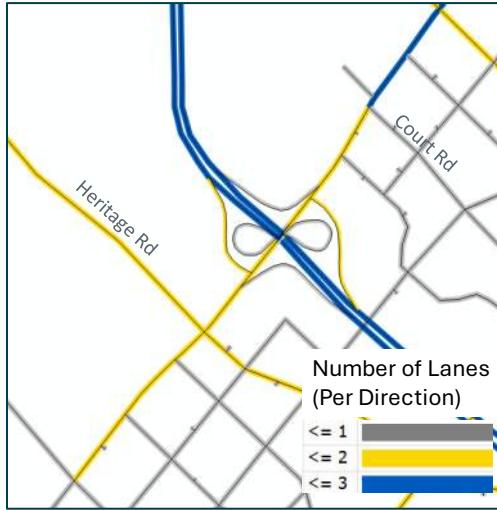
Bovaird 6 Lanes

Both 6 Lanes

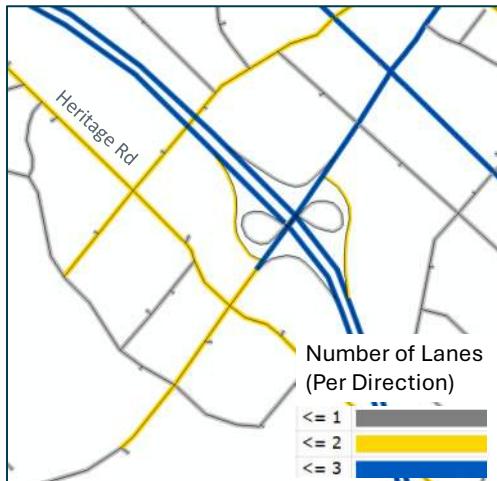


Focus Area 6 – Recommendations

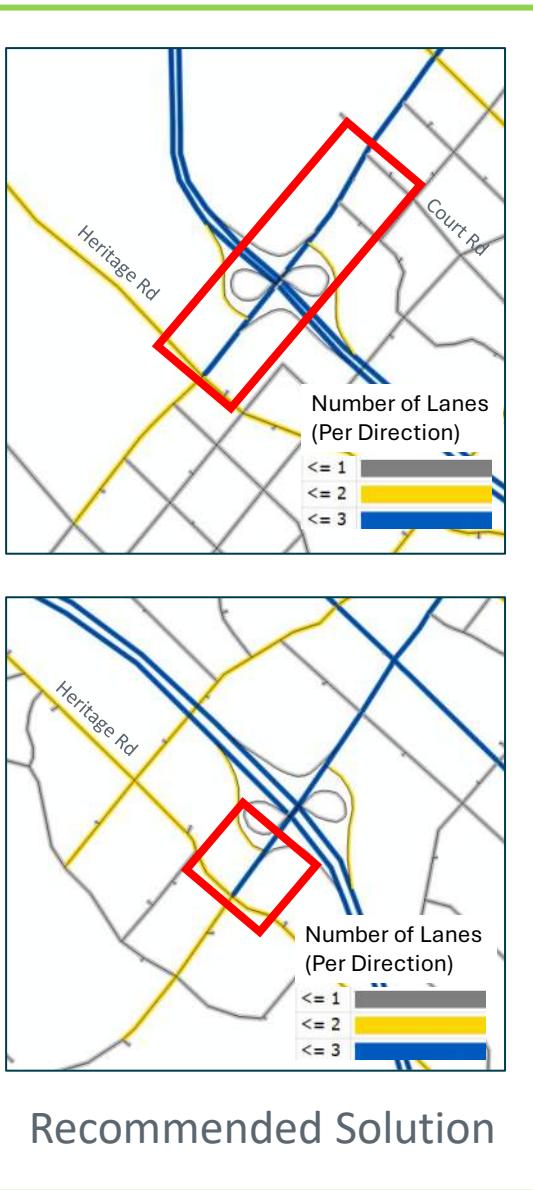
Mayfield Rd



Bovaird Dr



Base

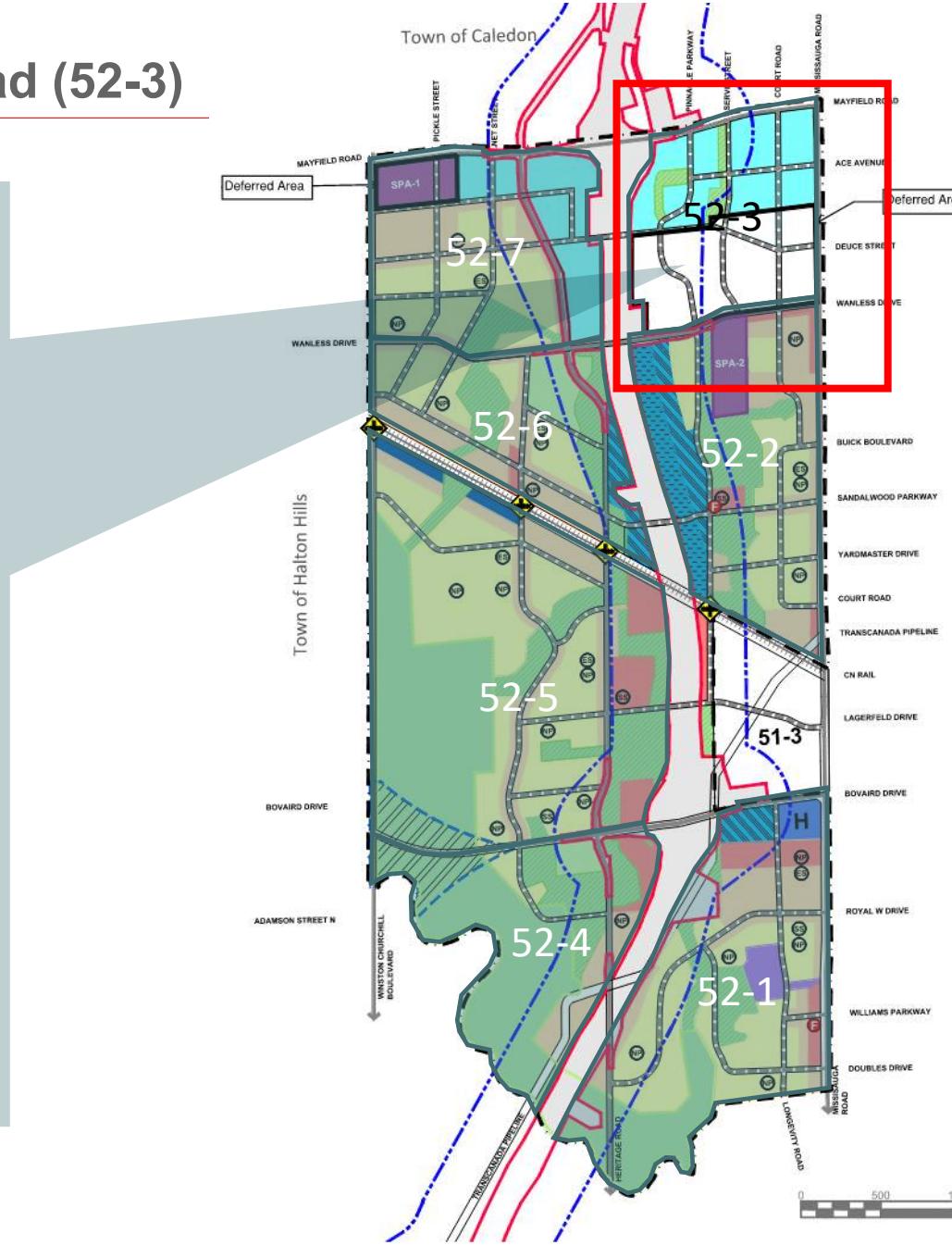
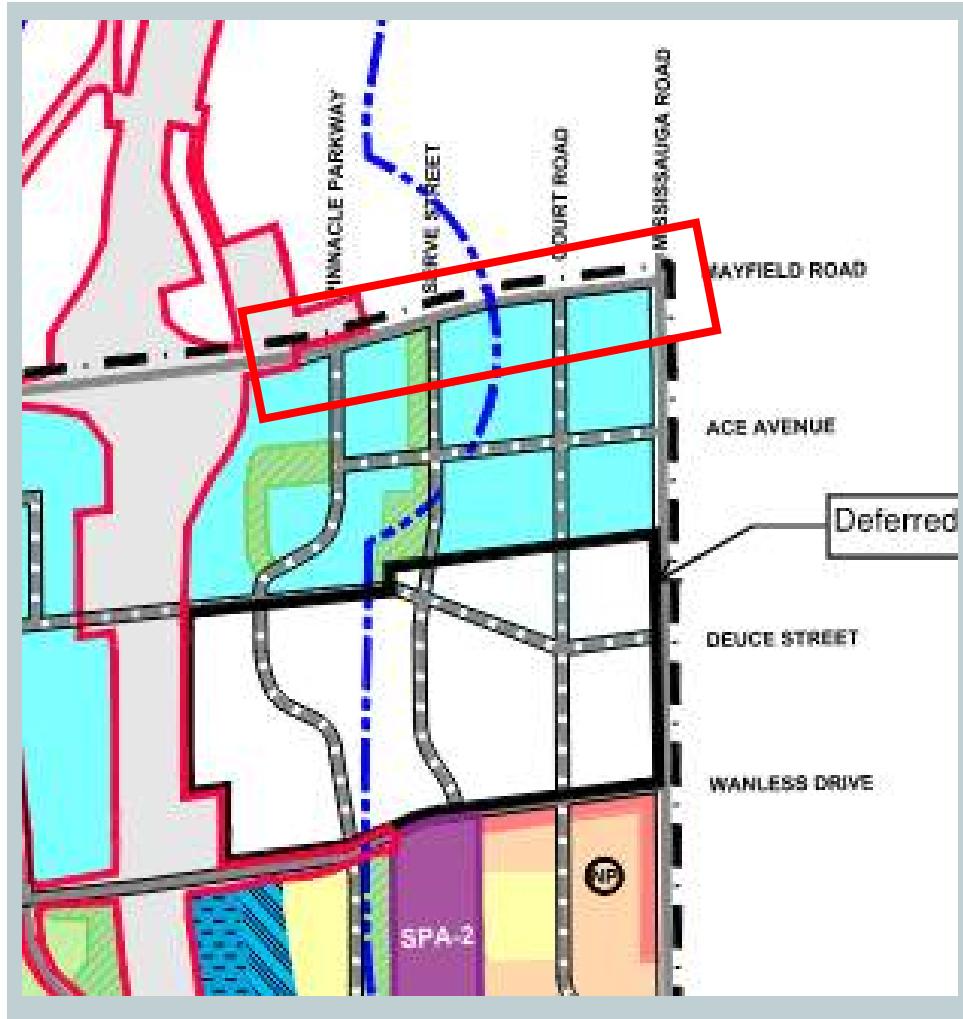


- In the base scenario, Mayfield Road is planned to decrease from 6 to 4 lanes west of Court Road, and Bovaird Drive is proposed to decrease from 6 to 4 lanes west of Highway 413
- In the base scenario, the 4-lane segments of Mayfield Road and Bovaird Drive are at or above capacity (specifically between Heritage Road and Highway 413)
- Heritage Road acts as main N-S connection that HH trips use to access 413 via Mayfield/Bovaird. HH trips to/from 413 plus E-W trips to/from west screenline results in high E-W volumes between Heritage and 413.
- **RECOMMENDATION:** Extend widening to 6-lanes westwards to Heritage Road on both Mayfield Road and Bovaird Drive

Intersection Spacing Along Mayfield Road Adjacent to Highway 413

Intersection Spacing Along Mayfield Road (52-3)

21

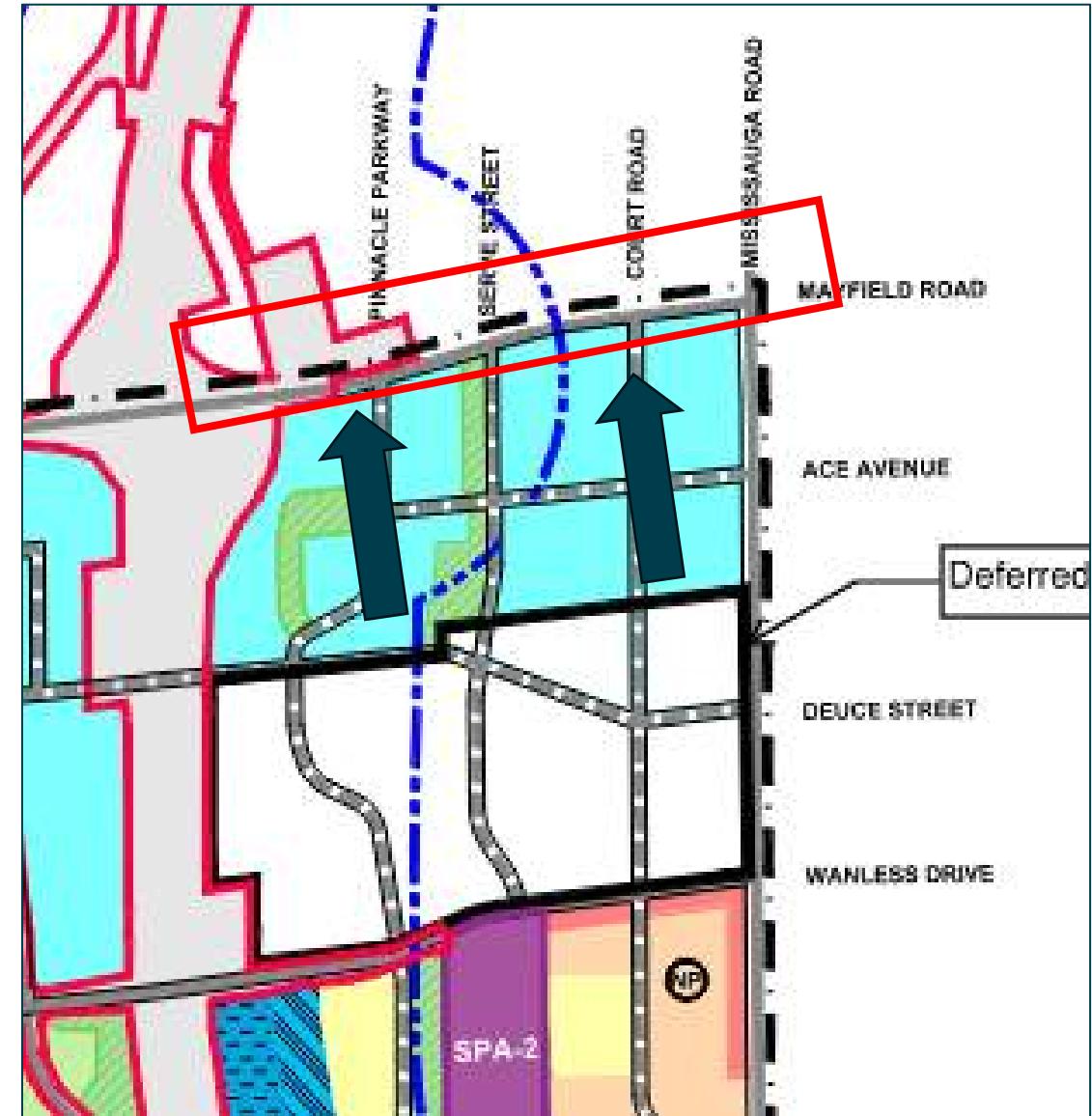


Intersection Spacing Along Mayfield Road (52-3)

22

- From the Peel Road Characterization Study:
 - Mayfield Road Classification: Industrial Connector
 - Full-to-Full Intersection Spacing: 450m
- Mississauga Road Classification: Suburban-Commercial Connector
- Full-to-Full Intersection Spacing: 300m

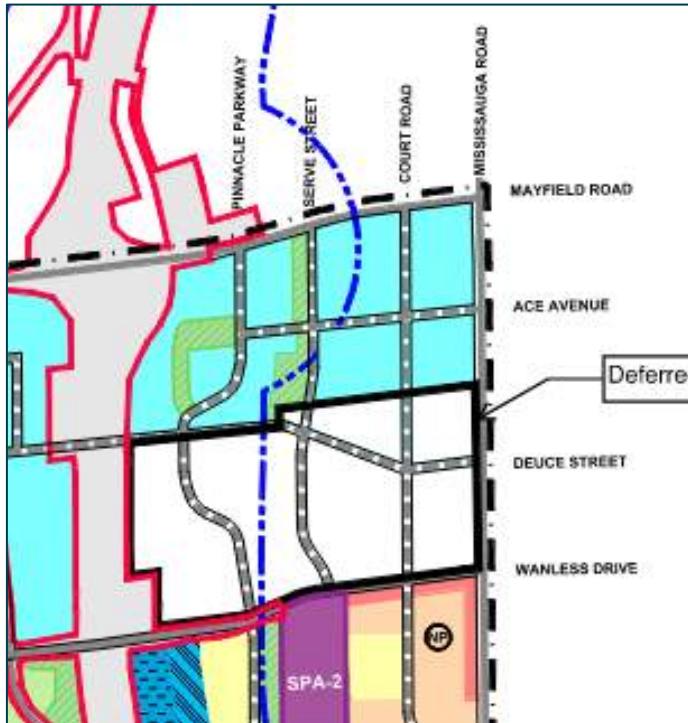
- From MTO Highway Corridor Management Manual:
 - MTO Intersection Offset Spacing: 400m min; 800m desirable



Intersection Spacing Along Mayfield Road (52-3)

23

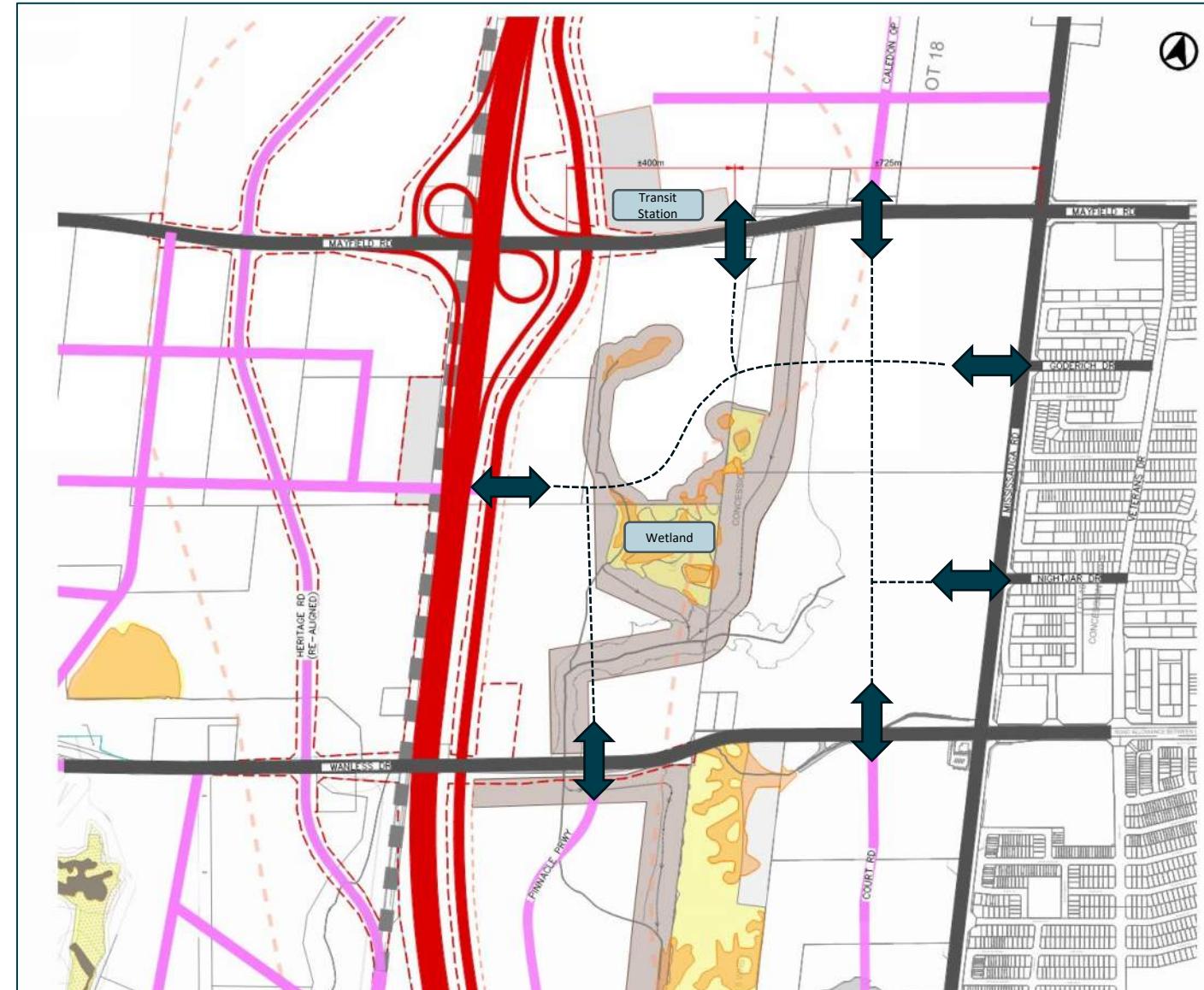
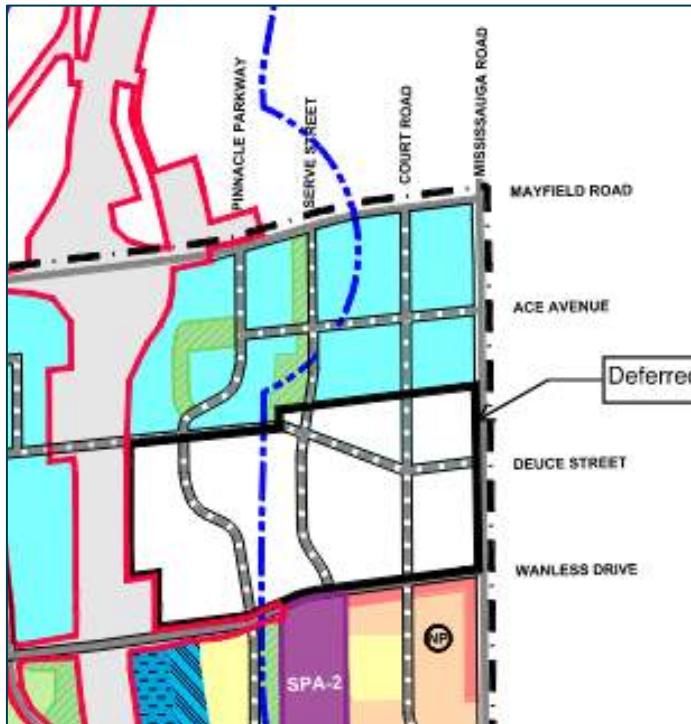
- Surrounding Context:
 - Highway 413
 - Existing Mount Pleasant
 - Caledon
 - Precinct 52-7
 - Environmental Features



Intersection Spacing Along Mayfield Road (52-3)

24

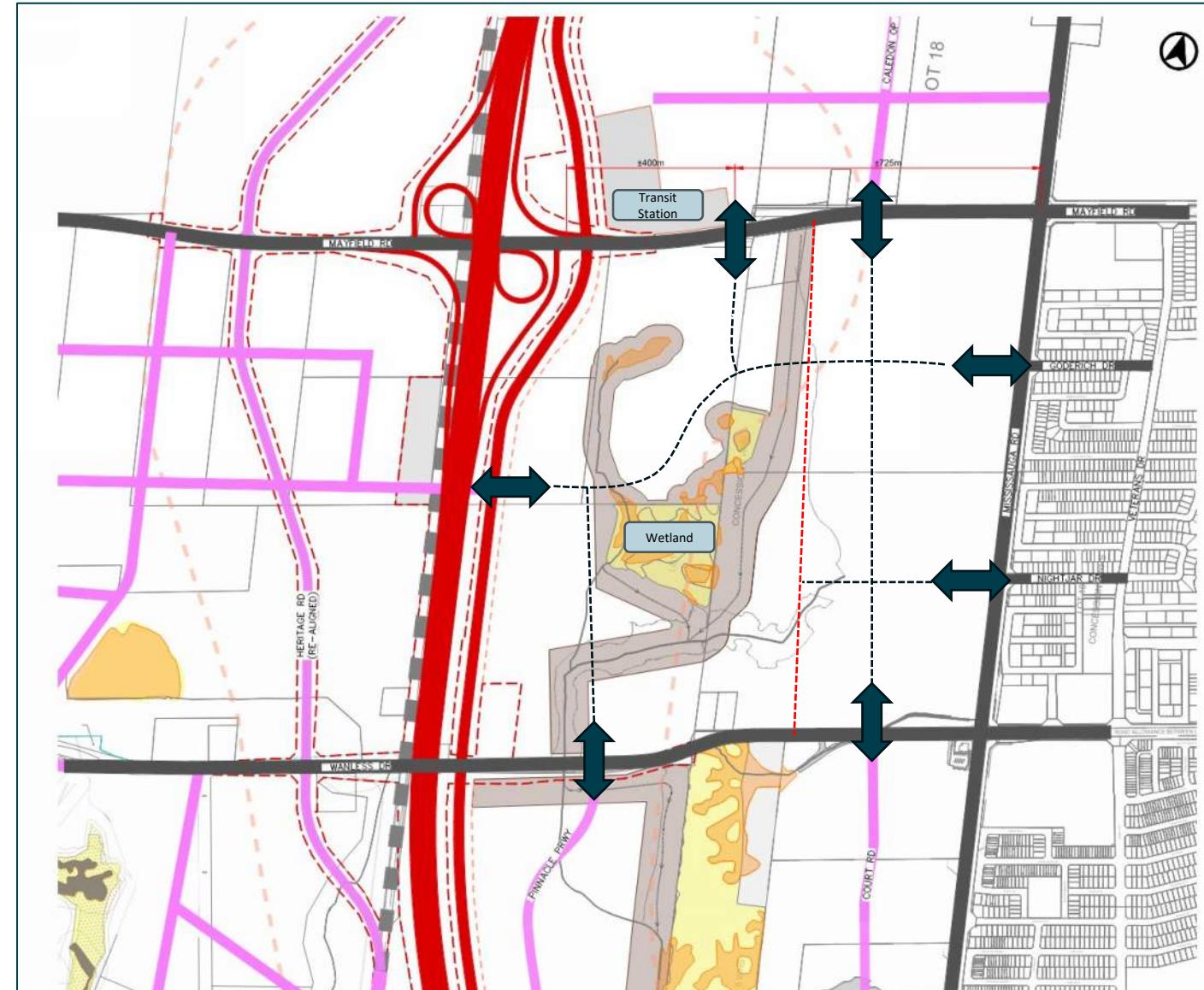
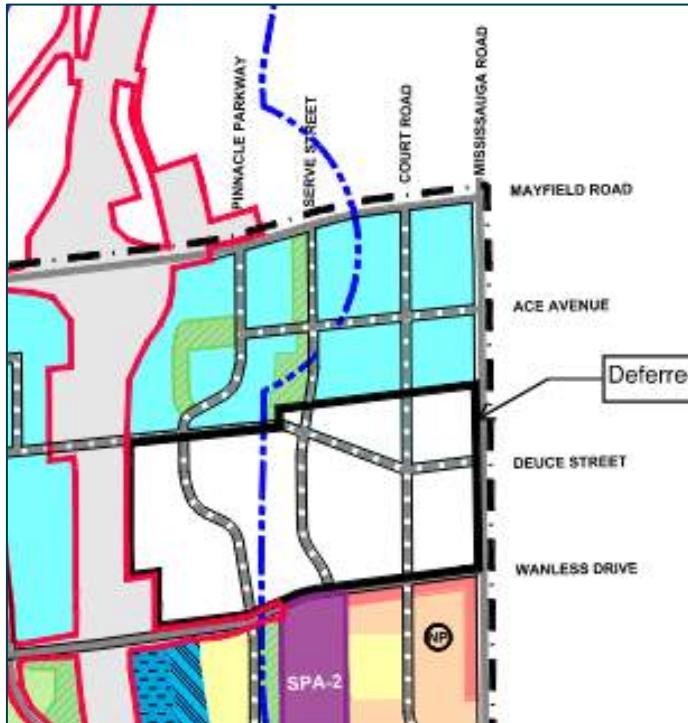
- Surrounding Context:
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 - Existing Mount Pleasant
 - Caledon
 - Precinct 52-7
 - Environmental Features



Intersection Spacing Along Mayfield Road (52-3)

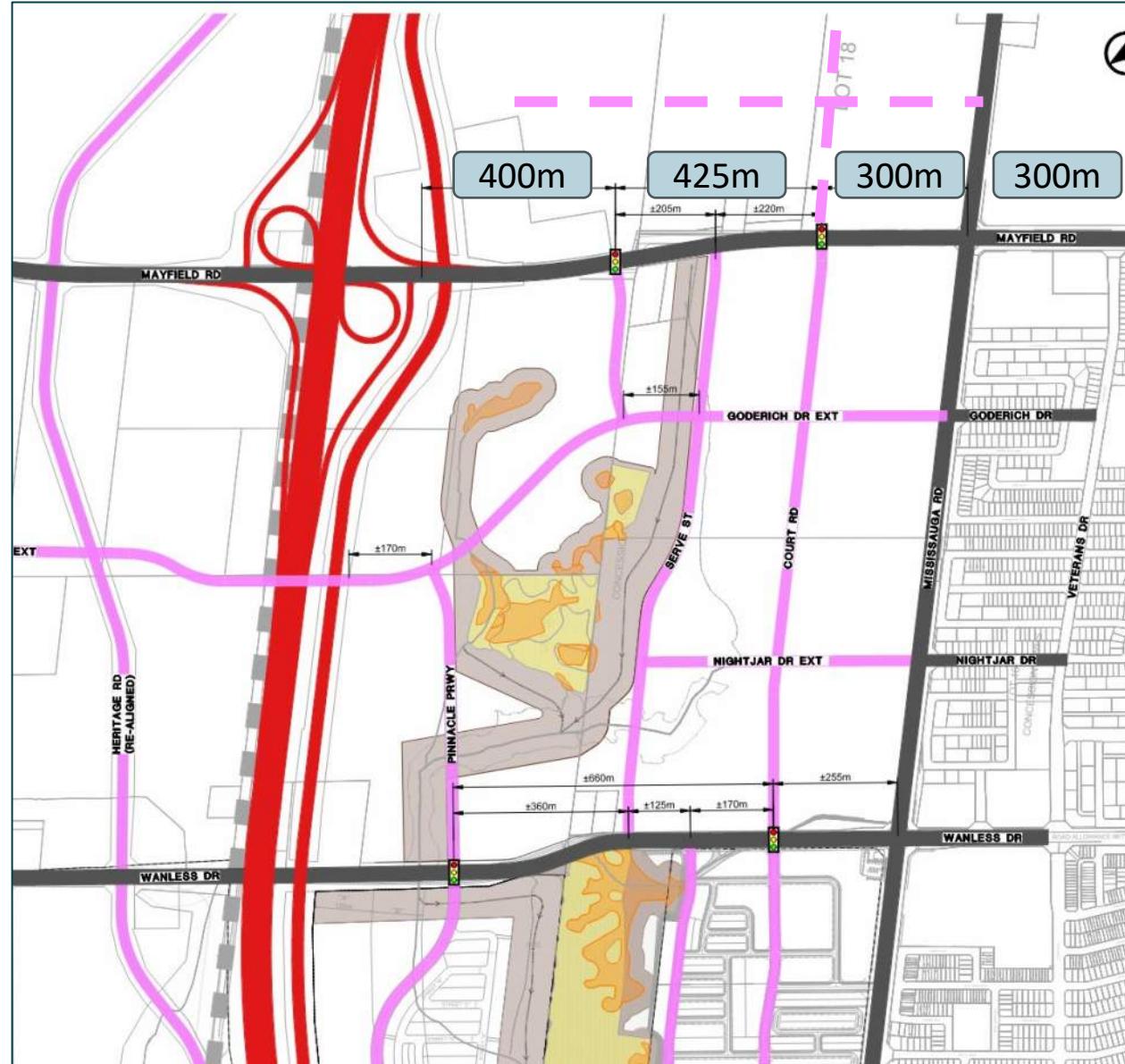
25

- Surrounding Context:
 - Highway 413
 - Existing Mount Pleasant
 - Caledon
 - Precinct 52-7
 - Environmental Features



Intersection Spacing Along Mayfield Road (52-3) – Proposed Concept

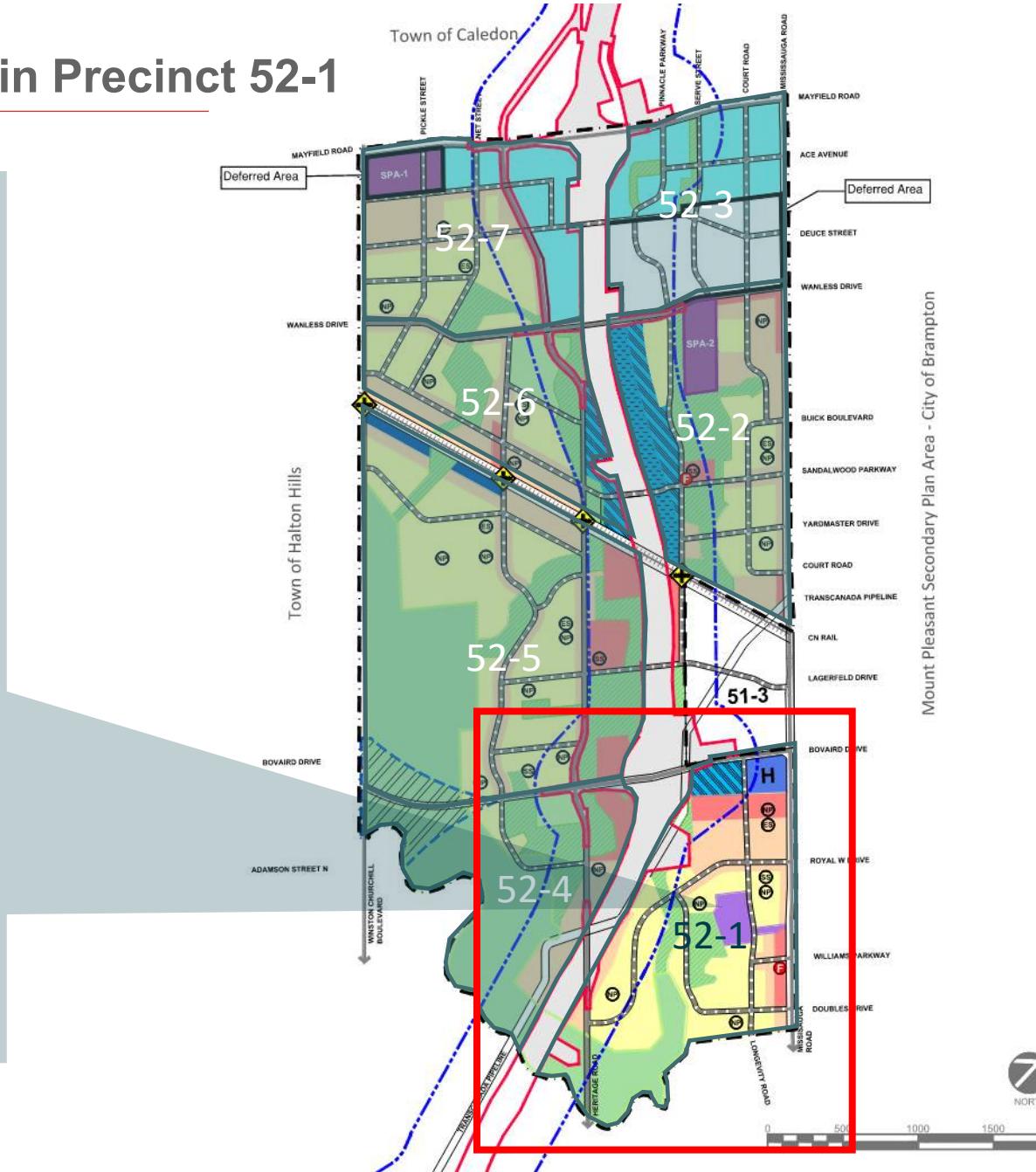
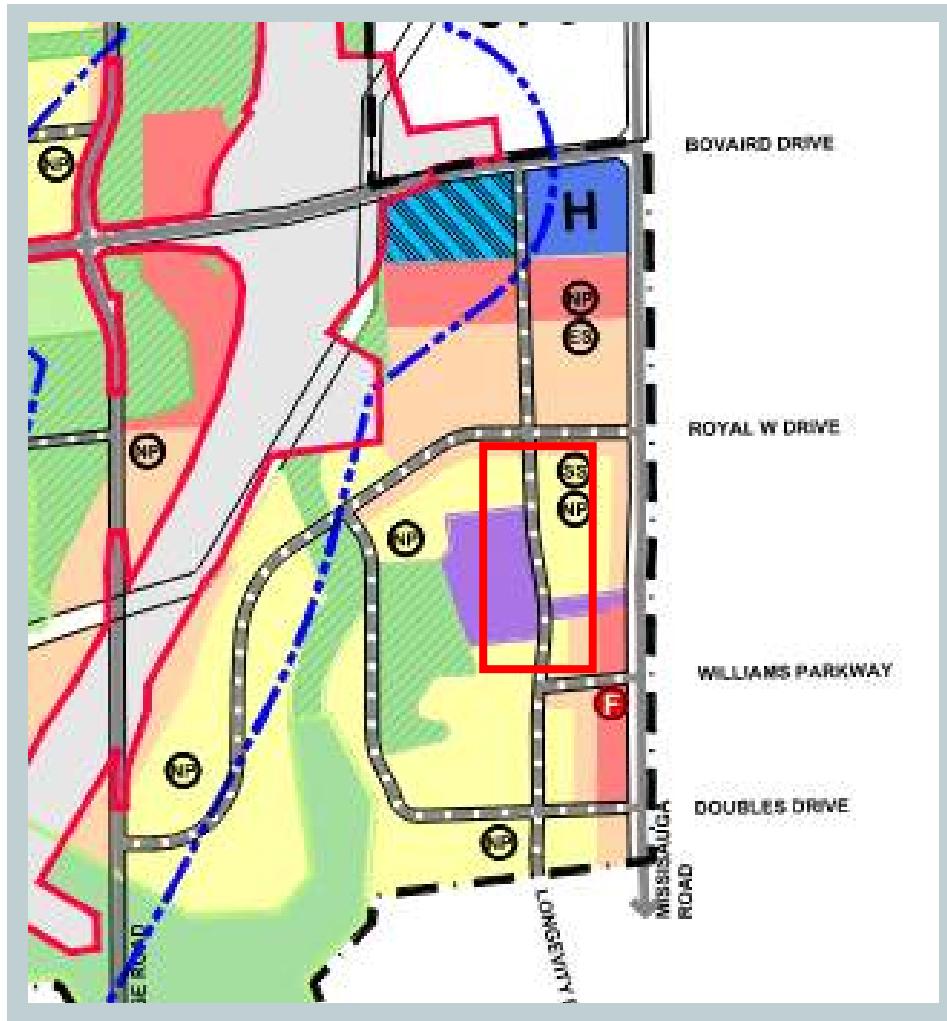
26



Peel Region Pumping Station Crossing in Precinct 52-1

Peel Region Pumping Station Crossing in Precinct 52-1

28



Mount Pleasant Secondary Plan Area - City of Brampton

Modelling

Linda Wu

Modelling and Analytics
Transportation Planning
City of Brampton

Adrian Lorion

Associate
BA Consulting Group Ltd.

Yahya Deen

Lead Analyst
BA Consulting Group Ltd.

TMP Project Management

Richa Dave, MCIP RPP

Project Manager, Transportation Planning
City of Brampton

Emily Ecker, P.Eng.

Senior Associate
BA Consulting Group Ltd.

Project Email: heritageheights@brampton.ca



BA Group

MOVEMENT IN URBAN ENVIRONMENTS



MEETING MINUTES

TO

Meeting Attendees (May 16, 2025)

LOCATION

Virtual (Zoom)

FROM

BA Consulting Group Ltd.

DATE

May 16, 2025

RE: HERITAGE HEIGHTS TRANSPORTATION MASTER PLAN – PEEL REGION ENGAGEMENT #1 – MAY 16, 2025 – VIRTUAL DRAFT

ATTENDEES

City of Brampton

- Richa Dave
- Vikram Hardatt
- Nelson Cadete
- David Monaghan

Peel Region

- Dana Jenkins
- John Hardcastle
- Robert Jay
- Sabrina Khan
- Sally Rook
- William Turner
- Rani Kol
- Catherine Barnes

Heritage Heights**Landowners Group**

- James Reed

Consultant (BA Group)

- Emily Ecker
- Adrian Lorion
- Yahya Deen
- Kevin Xu
- Hansen Rao

REGRETS

City of Brampton

- Brian Lakeman
- Linda Wu
- Henrik Zbogar

Peel Region

- Hashim Hamdani
- Lindsay Edwards
- Marzuq Shamsi

Consultant (BA Group)

- Michael Giallonardo

PRESENTATION (EMILY ECKER & YAHYA DEEN)

- Emily E. and Yahya D. delivered PowerPoint presentation outlining the Heritage Heights Secondary Plan, Heritage Heights Transportation Master Plan (TMP), and the associated modelling work. Specific topics of discussion included:
 - recommended number of lanes along Mayfield Rd and Bovaird Drive adjacent to Highway 413,
 - intersection spacing along Mayfield Road adjacent to Highway 413, and
 - the proposed Peel Region Pumping Station crossing in Precinct 52-1.
- The presentation slides will be shared alongside these meeting minutes.

OPEN DISCUSSION / PEEL REGION FEEDBACK

Modelling Results, Recommended Regional Road Widening

- Robert Jay mentioned that Peel Region is working on 2051 modelling work to support the Peel Region TMP and is generally interested in the Heritage Heights mesoscopic modelling work and the recommendations resulting from said modelling work. Results from the Heritage Heights modelling work will be taken into consideration as Peel Region conducts their 2051 modelling work.
 - Adrian L. asked if there are any additional considerations to account for in the Heritage Heights modelling work
 - Robert Jay noted that they are working through model development and calibration, will review slides and inform the group if there are any other considerations. Will take the slides back to his team and review.

Modelling Results, Recommended Regional Road Widening

- Sally Rook noted that the Region would like a more detailed modelling exercise that assesses intersection operations and queueing along Mayfield Road adjacent to Highway 413 in order to support a spacing less than the 450m minimum
 - Emily E. advised that we make this intersection a special focus area for which BA Group can conduct a more detailed analysis

Peel Region Pumping Station Crossing

- Dana Jenkins noted that William Turner (had to depart meeting early) should be included in discussions surrounding the 52-1 pumping station
 - Emily E noted this topic should be further discussed as a focussed meeting given limited time left in meeting and importance of pumping station to the Region and importance of road network to the City.
 - Region noted concerns were previously raised and meetings have continued with City of Brampton on this topic.
- Dana Jenkins noted that large development coming to the pumping station area with significant heavy vehicle traffic, therefore the connection would not be a viable option in the future given planned growth, not a place for a public right-of way
 - Sally Rook agrees, there are issues with delivering the Longevity connection

- Dave Monaghan asks if there are concerns with construction or if heavy vehicle traffic is from long-term operations
- John Hardcastle notes that there are a few issues, including operations, constructions, safety, and maintaining a secure facility
- Vikram notes that he will reach out to City development services on ongoing discussions (via Allan Parsons), will set up a meeting to understand where the City and Region are at with ongoing discussions.

MEETING OUTCOME AND NEXT STEPS

- Attendees to review slides/minutes and provide corrections and any follow-up questions/clarifications.
- BA Group / City of Brampton to prepare modelling results for remaining Focus Area 5 (Norval) and will discuss at subsequent meetings.
- Vikram notes that he will reach out to City development services on ongoing discussions (via Allan Parsons), will set up a meeting to understand where the City and Region are at with ongoing pumping station discussions.
- Request for Engagement 2 expected to follow within a month.

DRAFT



HERITAGE HEIGHTS TRANSPORTATION MASTER PLAN

Caledon Engagement 1

Prepared For: Town of Caledon

June 9, 2025



BRAMPTON

*Heritage Heights
Landowners Group*



BA Group

- Introductions
- Heritage Heights Secondary Plan
- Transportation Master Plan (TMP) Process Overview
- Intersection Spacing Along Mayfield Road Adjacent to Highway 413
- Status of Alloa Secondary Plan

- City of Brampton
 - Vikram Hardatt
 - Richa Dave
 - David Monaghan
- BA Group
 - Emily Ecker
 - Michael Giallonardo
 - Yahya Deen
 - Adrian Lorion
- Town of Caledon
 - Tanjot Bal
 - Kavleen Younan
 - Jay Menary

Heritage Heights Secondary Plan

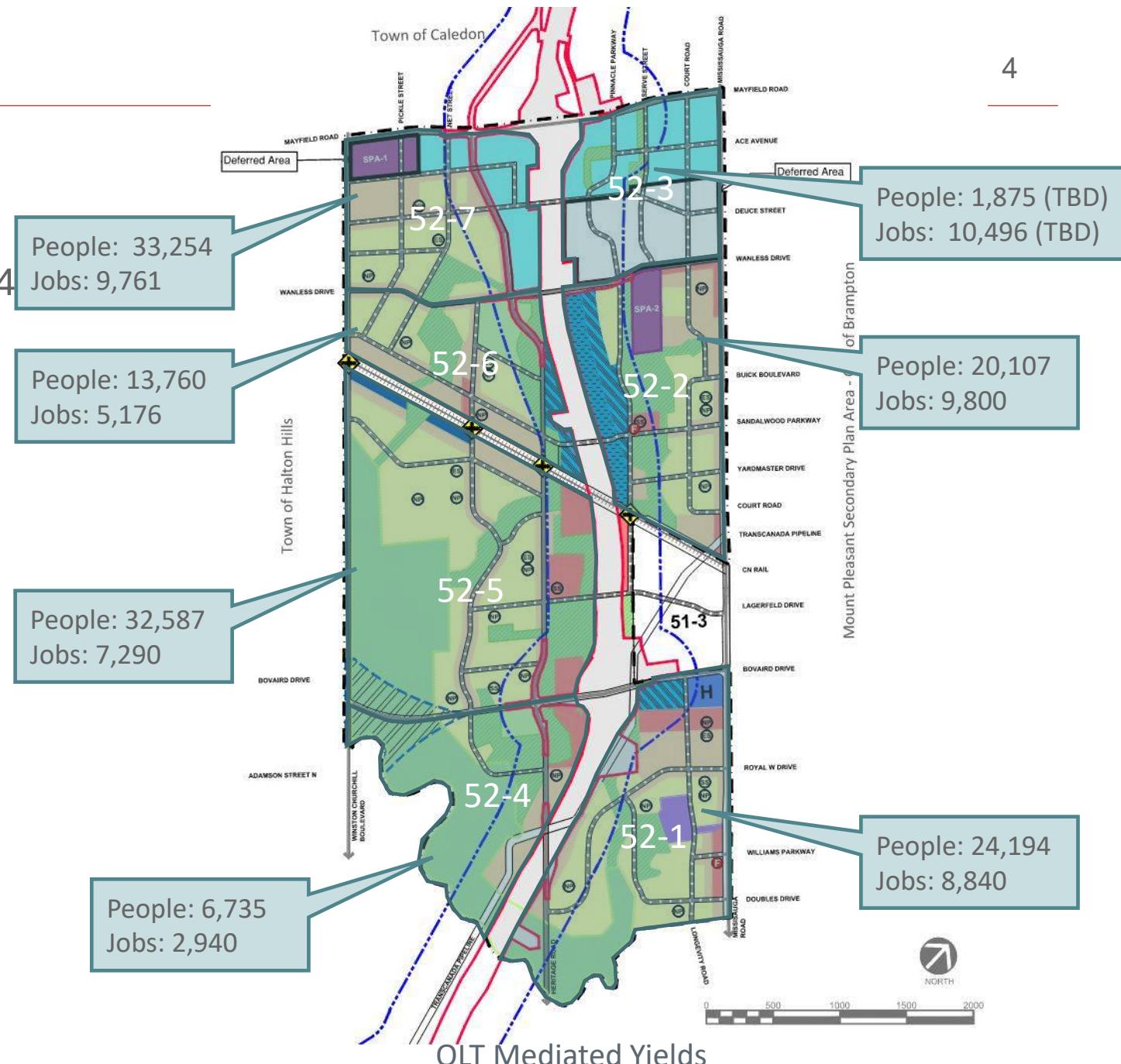
4

The Secondary Plan

- Secondary Plan Approved August 21, 2024

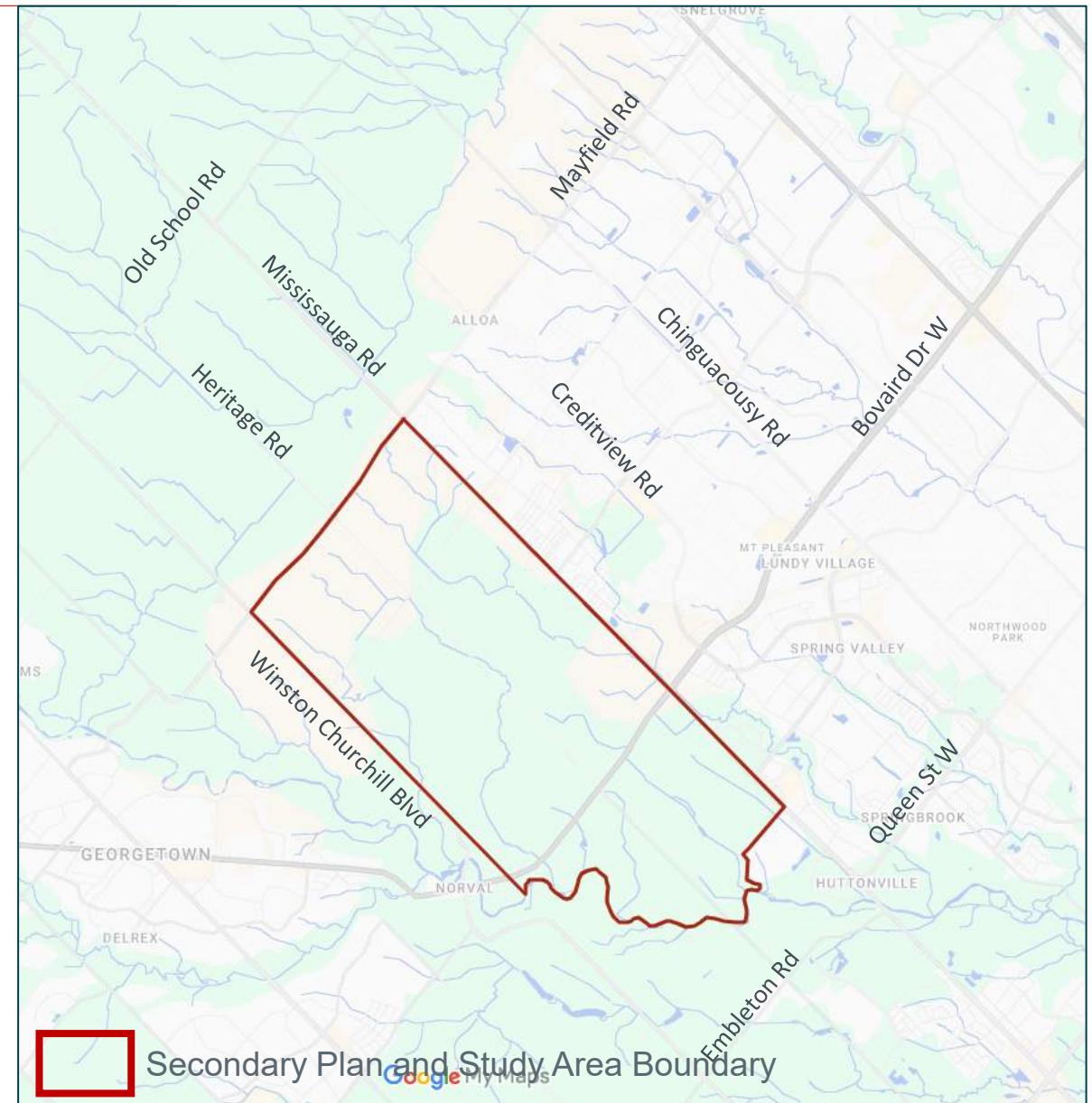
Supporting Studies Underway

- Infrastructure Servicing Study (ISS)
- Subwatershed Study (SWS)
- Transportation Master Plan (TMP)



The Study

The City of Brampton and the Heritage Heights Landowners Group are working together as co-proponents to undertake a Transportation Master Plan (TMP) to identify the transportation infrastructure required to support the growth envisioned for the Heritage Heights Secondary Plan Area.



MCEA Process

- The TMP is being undertaken in accordance with **Approach #2** of the Master Planning Process, as outlined in the Municipal Class Environmental Assessment (MCEA) (February 2024).
- The TMP will address Phases 1 and 2 of the MCEA process for any Schedule B projects and will form the basis for the recommended Schedule C transportation infrastructure projects identified within the TMP report.

4.5 Master Planning Process

The master planning process is discussed in Section A.2.7. A summary of the various approaches is provided below.

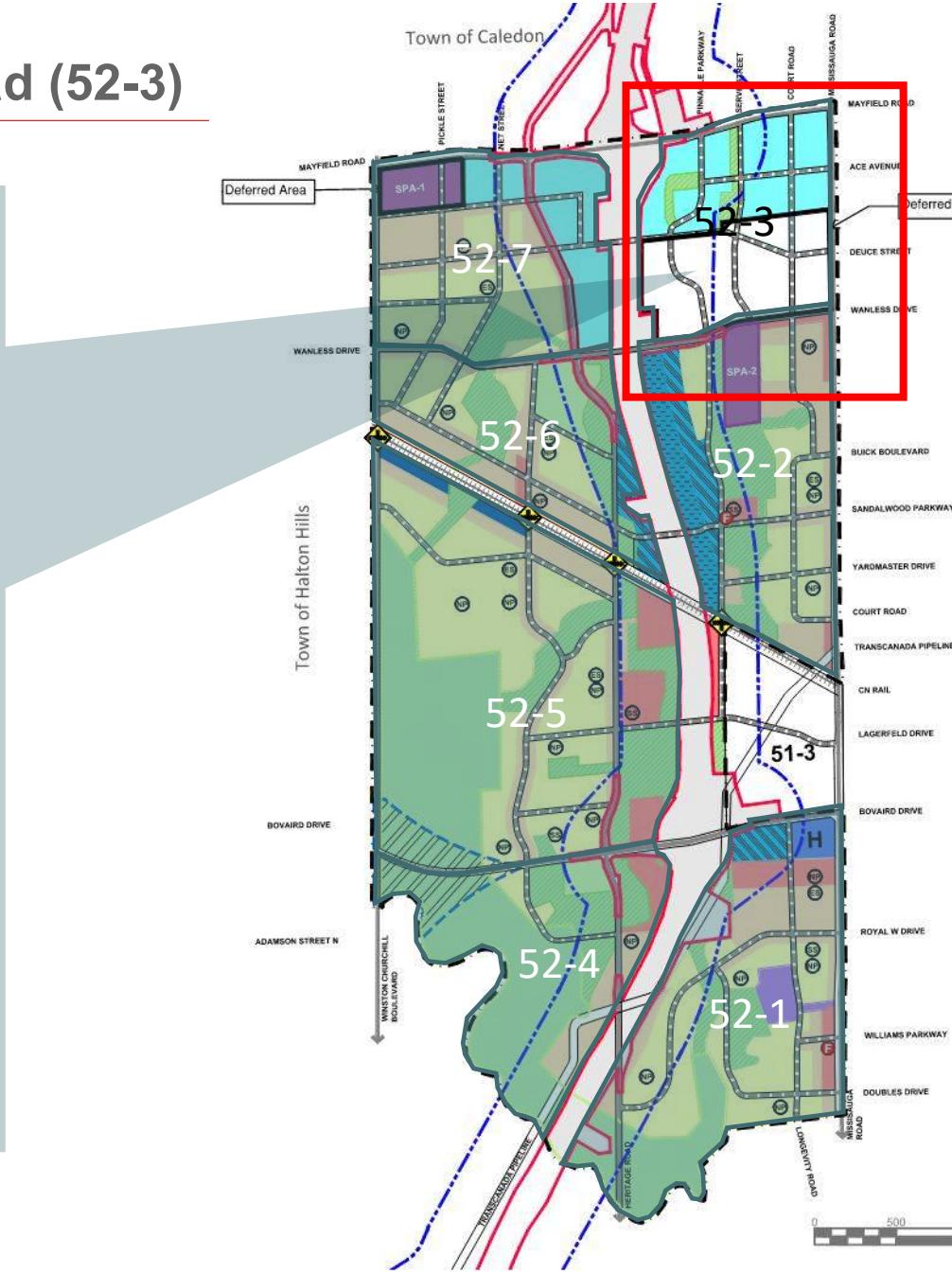
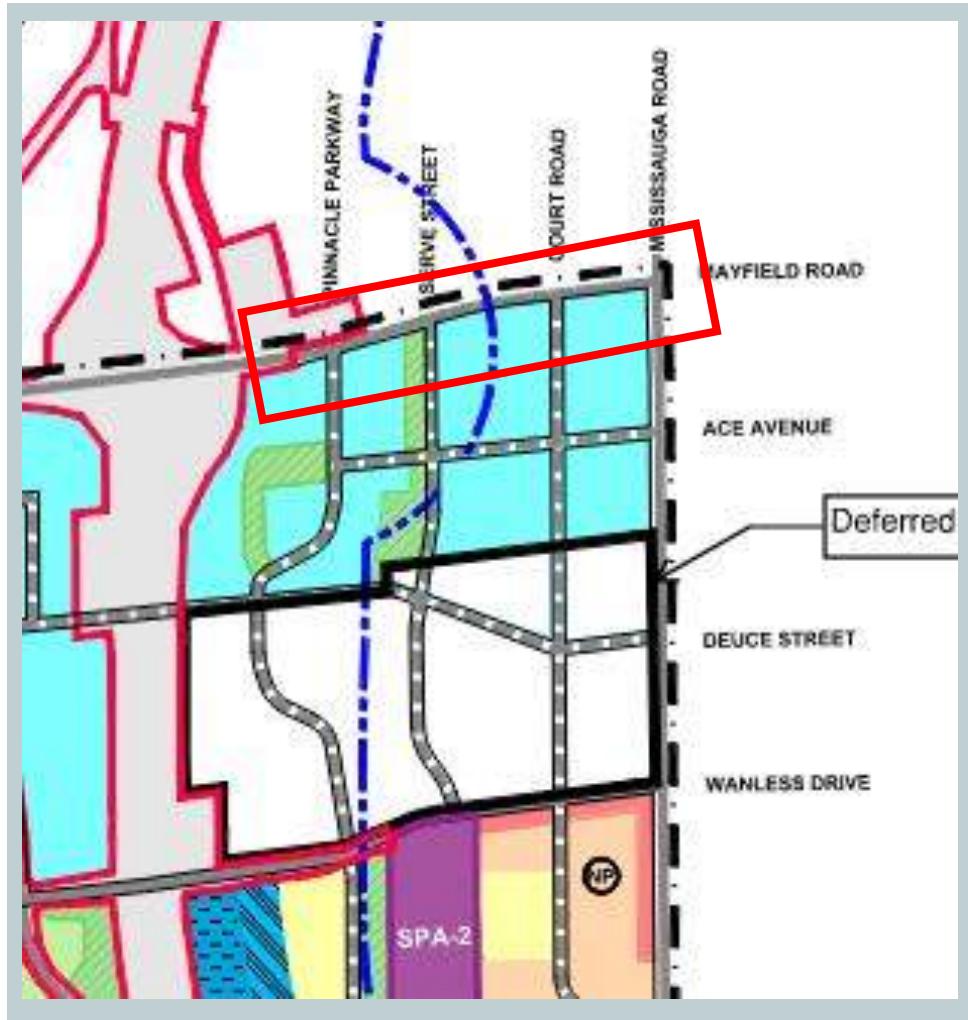
	Approach 1	Approach 2*	Approach 3*
Level of Detail	Broad (project specific information is minimal) Used as support for subsequent Schedule B and C project specific studies	Sufficient to fulfil requirements for Schedule B projects (more detailed project specific information is included) Used as support for subsequent Phase 3 and 4 for Schedule C project specific studies	Sufficient to fulfil requirements for Schedule B and C projects (more detailed project specific information is included)
Process	Preliminary Phase 1 and 2	Phase 1 and 2	Phase 1 to 4
Final Notice	Notice of Master Plan *This should not be called a Notice of Completion	Final notice becomes Notice of Completion for Schedule B projects	Final notice becomes Notice of Completion for Schedule B and C projects
Integration with Planning Act*	*Official Plan and Secondary Plan	*Official Plan and Secondary Plan, Plan of Subdivision, etc.	*Official Plan, Secondary Plan, Plan of Subdivision, etc.

* The various master planning approaches provide proponents with flexibility to customize their master plans to suit their needs. See section A.2.7 for information on modified approaches 2 and 3.

Intersection Spacing Along Mayfield Road Adjacent to Highway 413

Intersection Spacing Along Mayfield Road (52-3)

8



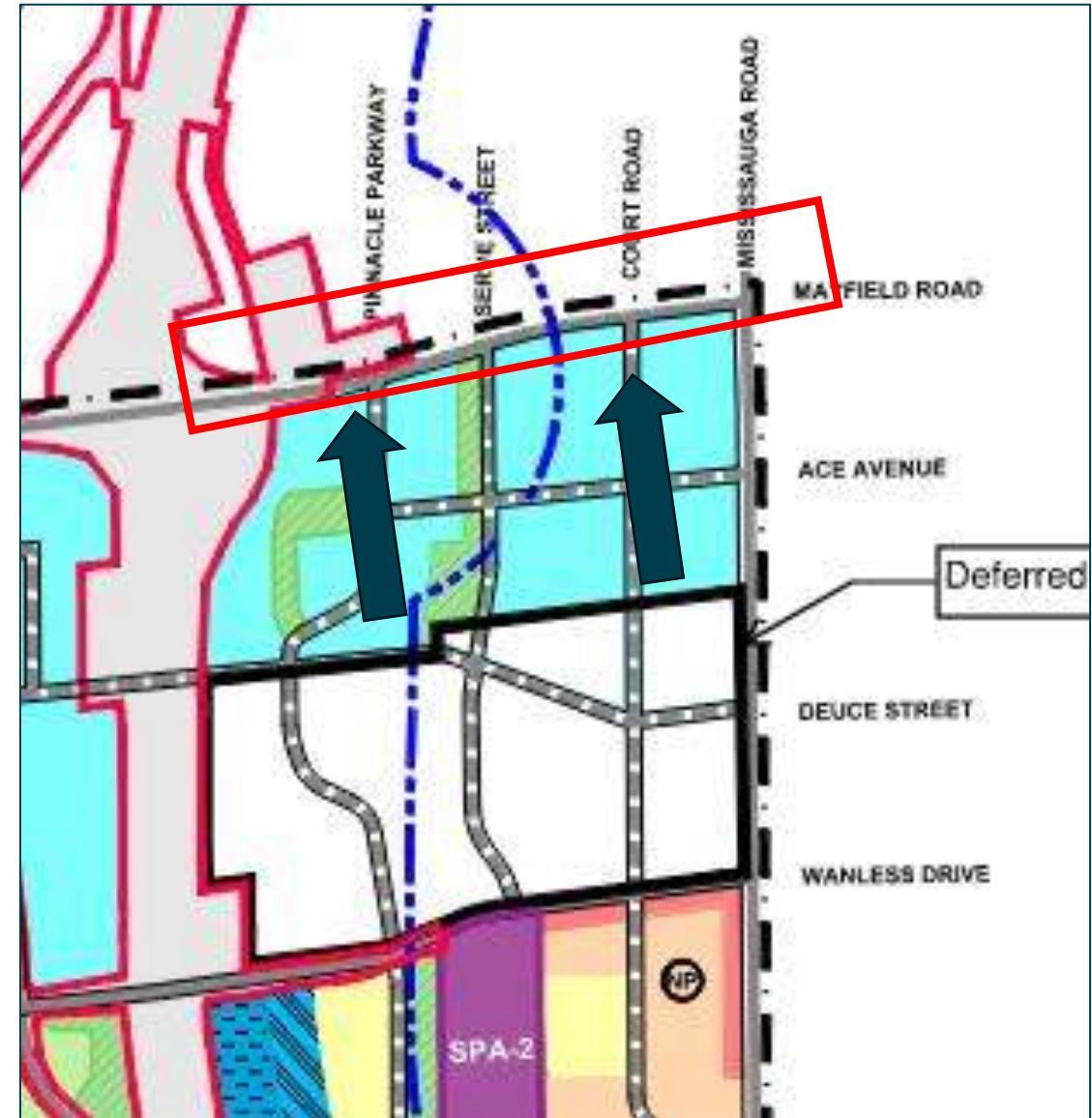
Mount Pleasant Secondary Plan Area - City of Brampton

Intersection Spacing Along Mayfield Road (52-3)

9

- From the Peel Road Characterization Study:
 - Mayfield Road Classification: Industrial Connector
 - Full-to-Full Intersection Spacing: 450m
- Mississauga Road Classification: Suburban-Commercial Connector
- Full-to-Full Intersection Spacing: 300m

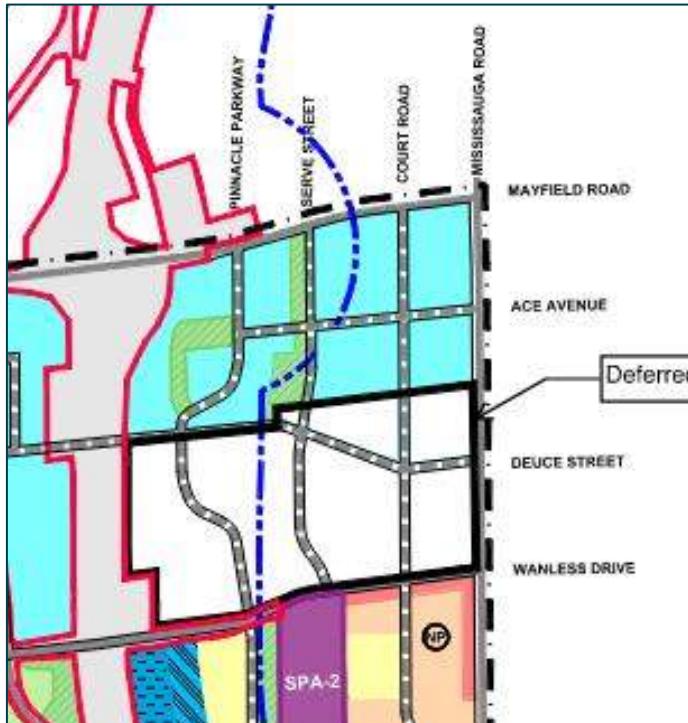
- From MTO Highway Corridor Management Manual:
 - MTO Intersection Offset Spacing: 400m min; 800m desirable



Intersection Spacing Along Mayfield Road (52-3)

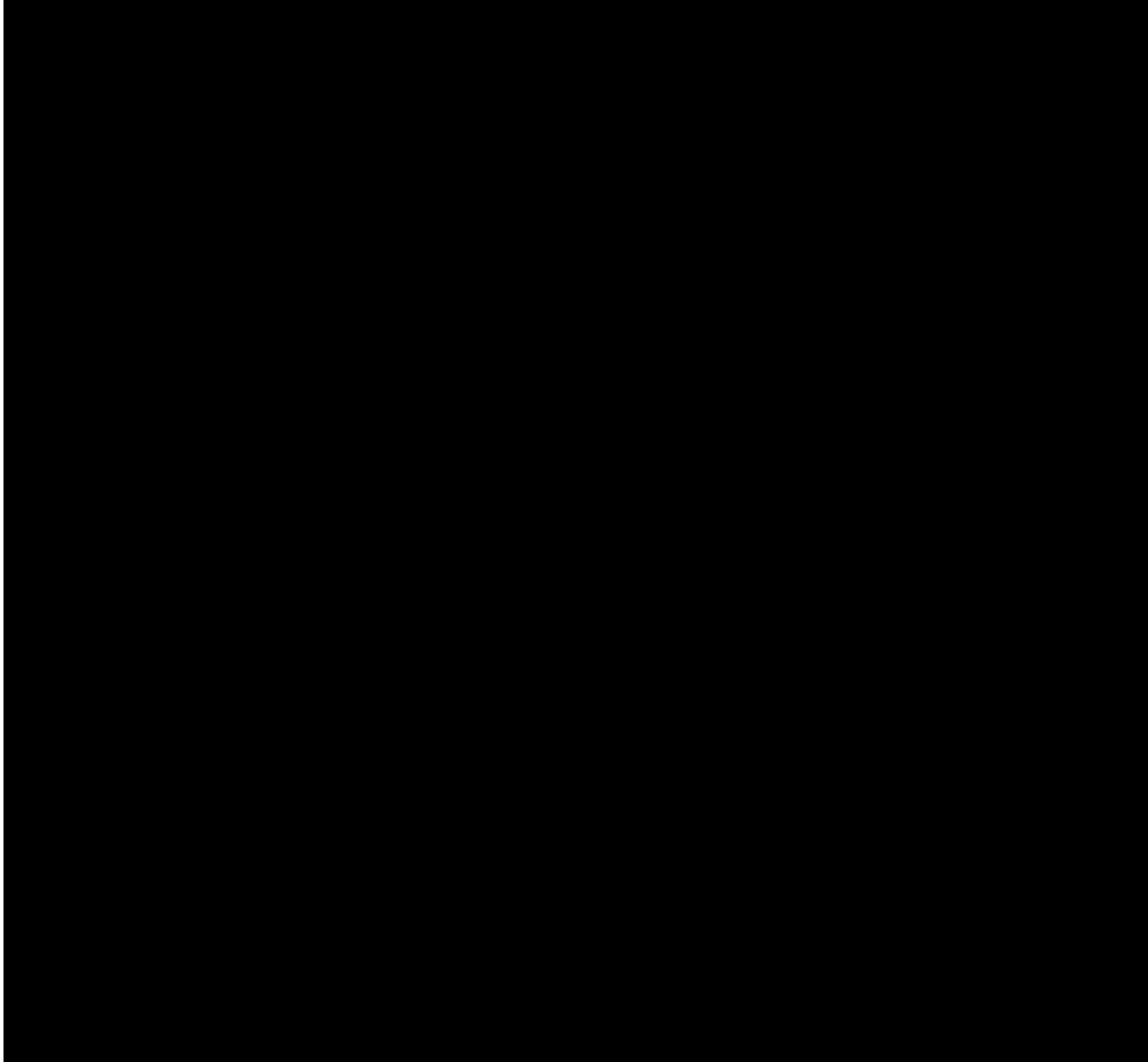
10

- Surrounding Context:
 - Highway 413
 - Existing Mount Pleasant
 - Caledon
 - Precinct 52-7
 - Environmental Features



Intersection Spacing Along Mayfield Road (52-3) – Proposed Concept

11



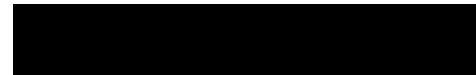
TMP Project Management

Richa Dave, MCIP RPP



Project Manager, Transportation Planning

City of Brampton



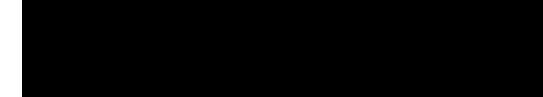
Project Email: heritageheights@brampton.ca

Emily Ecker, P.Eng.



Senior Associate

BA Consulting Group Ltd.





BA Group

MOVEMENT IN URBAN ENVIRONMENTS



HERITAGE HEIGHTS TRANSPORTATION MASTER PLAN

Prepared For: External Agencies

August 14, 2025



BRAMPTON

*Heritage Heights
Landowners Group*



BA Group

The City of Brampton is located on the traditional territories of the Mississaugas of the Credit, Haudenosaunee, and Wendat Nations who have called this land home since time immemorial. We acknowledge the agreements made in Treaty 19 – the Ajetance Purchase of 1818 – and are committed to our ongoing role in reconciliation through meaningful action rooted in truth, justice, and respect. We are grateful to the original caretakers of this land who have ensured we are able to work, play, and live in Brampton now and in the future.

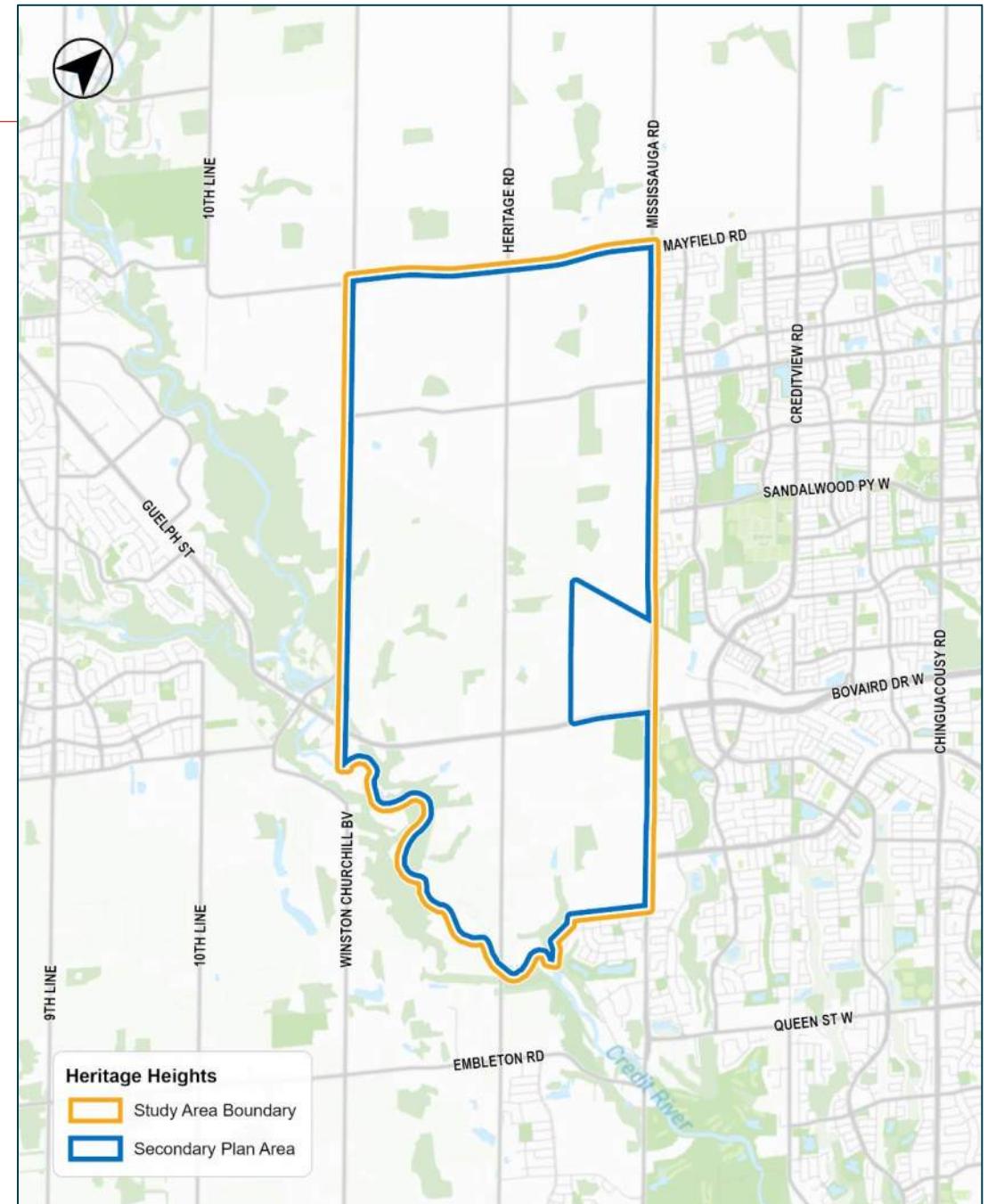
Transportation Master Plan

The Study

The City of Brampton is experiencing significant growth, including within the Heritage Heights Secondary Plan Area.

The City of Brampton and the Heritage Heights Landowners Group are working together as co-proponents to undertake a Transportation Master Plan (TMP) to identify the transportation infrastructure required to support the growth envisioned for the Heritage Heights Secondary Plan Area.

The TMP will provide the long-range planning necessary to identify a transportation network that supports the land use plan and the forecasted travel demand while encouraging more sustainable travel choices and behaviours.

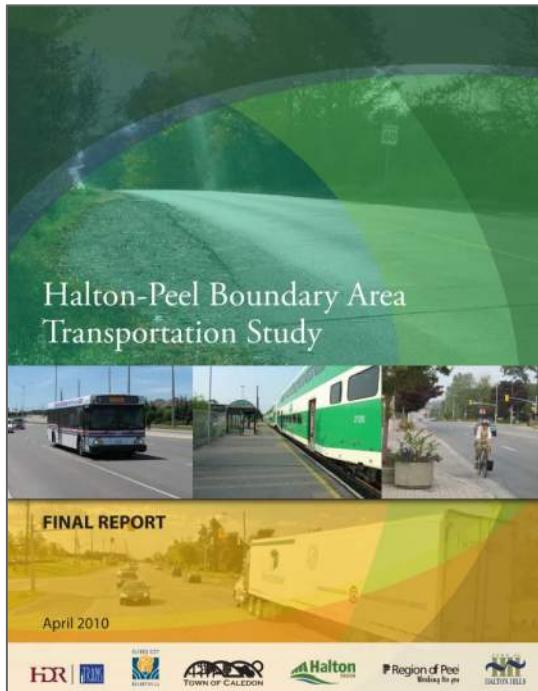


History

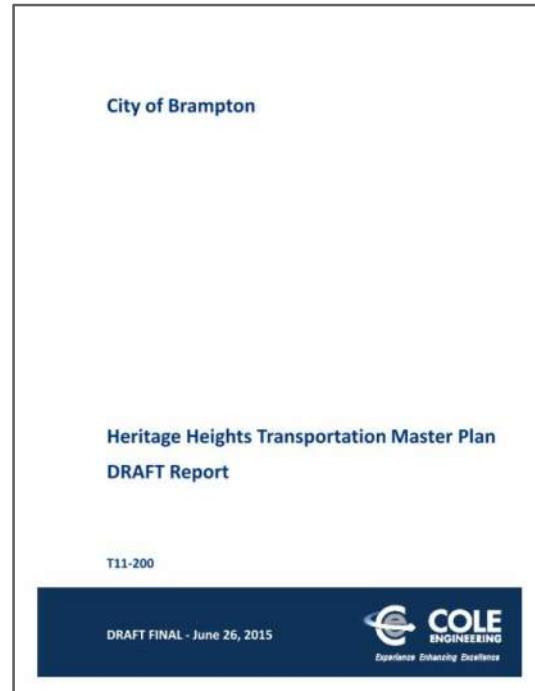
- **2006:** Peel Council extended Regional Urban Boundary to include “North West Brampton Urban Development Area”
- **2009:** Brampton Council initiated secondary plan planning for the Heritage Heights Community
- **2014:** Proposed Land Use Plan Approved for Public Consultation
- **2015:** Council directed staff to revisit the plan and incorporate broader stakeholder engagement which would better inform the plan.
- **2019:** Design charrettes with landowners, staff, agencies, and the public shaped a new vision and land use framework guided by Brampton 2040 Vision.
- **2020:** Council endorsed a new Conceptual Land Use Plan and directed further technical evaluation and policy development.
- **2022:** Heritage Heights Secondary Plan was formally adopted through an Official Plan Amendment.
- **2024:** After appeals, the Ontario Land Tribunal Decision was published, and the Secondary Plan’s details were **finalized and approved on August 21st 2024**.

Former Studies

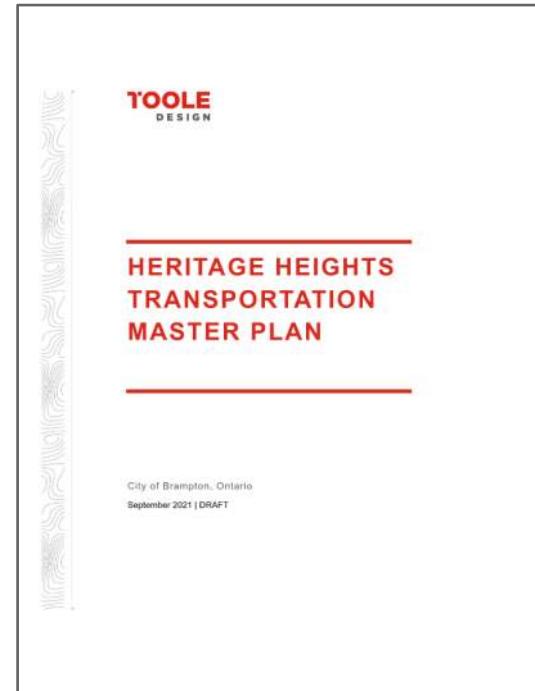
2010 Halton-Peel Boundary Area Transportation Study (HPBATS)



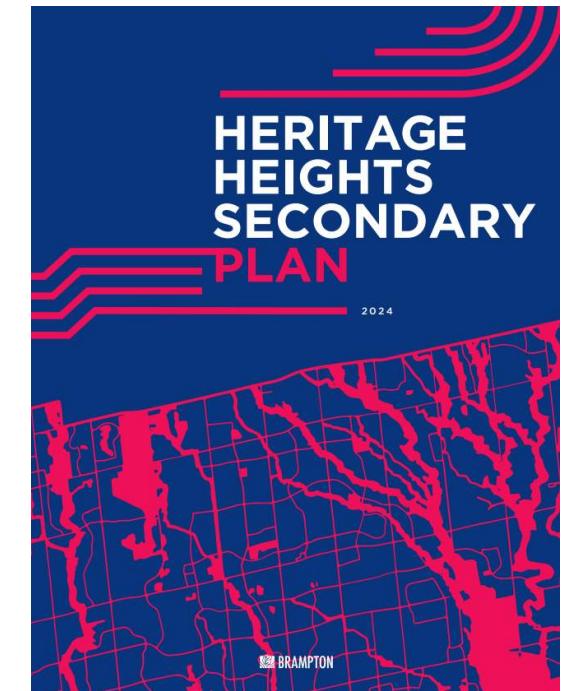
2015 Heritage Heights Transportation Master Plan (draft)



2022 Heritage Heights Transportation Master Plan



2024 Heritage Heights Secondary Plan (approved by the OLT)



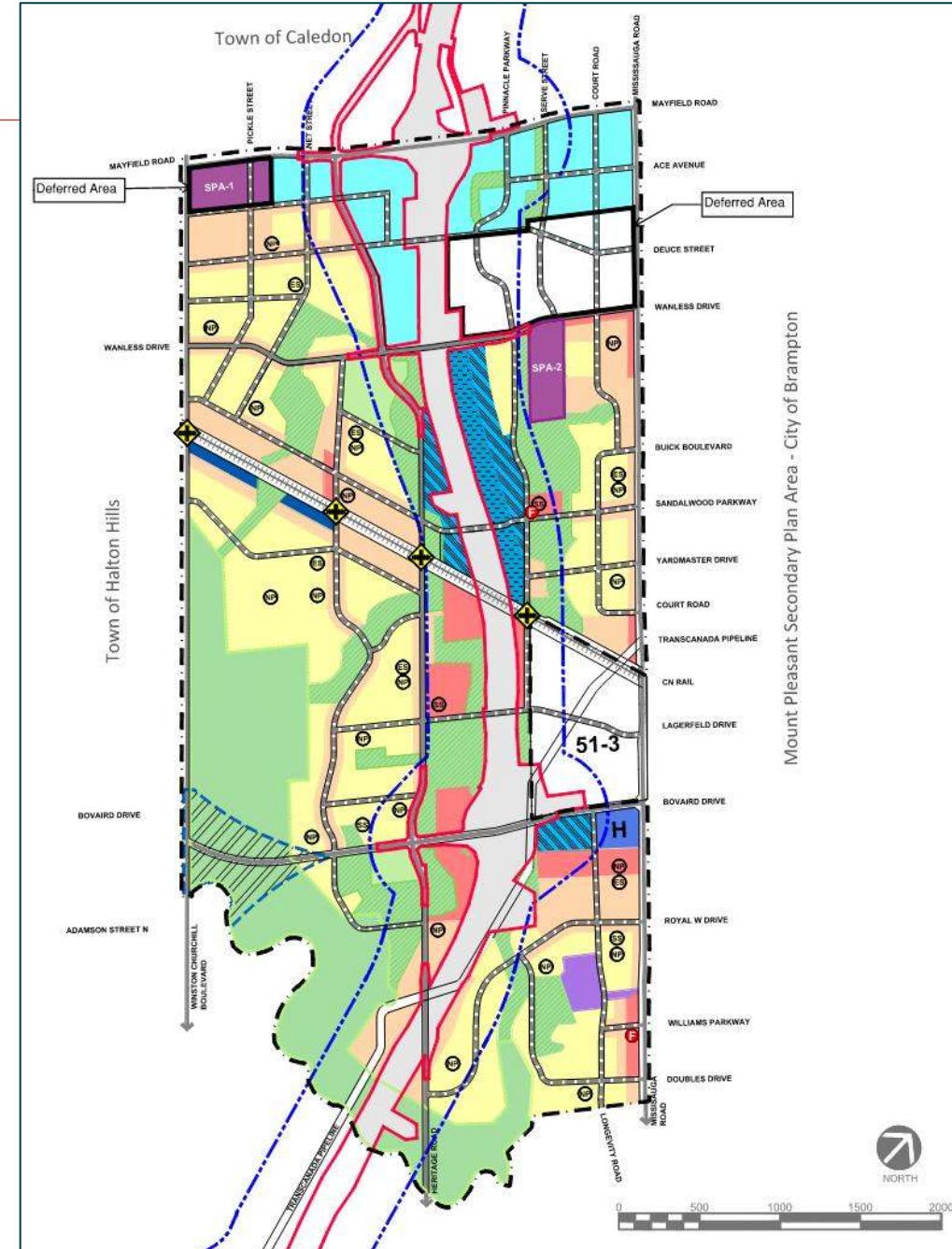
Heritage Heights Secondary Plan

The Secondary Plan

- The revised and approved Heritage Heights Secondary Plan (HHSP) represents a significant re-envisioning of the area that includes the Province's identified provincial transportation corridor.

Supporting Studies Underway

- Infrastructure Servicing Study (ISS)
- Subwatershed Study (SWS)
- Transportation Master Plan (TMP)



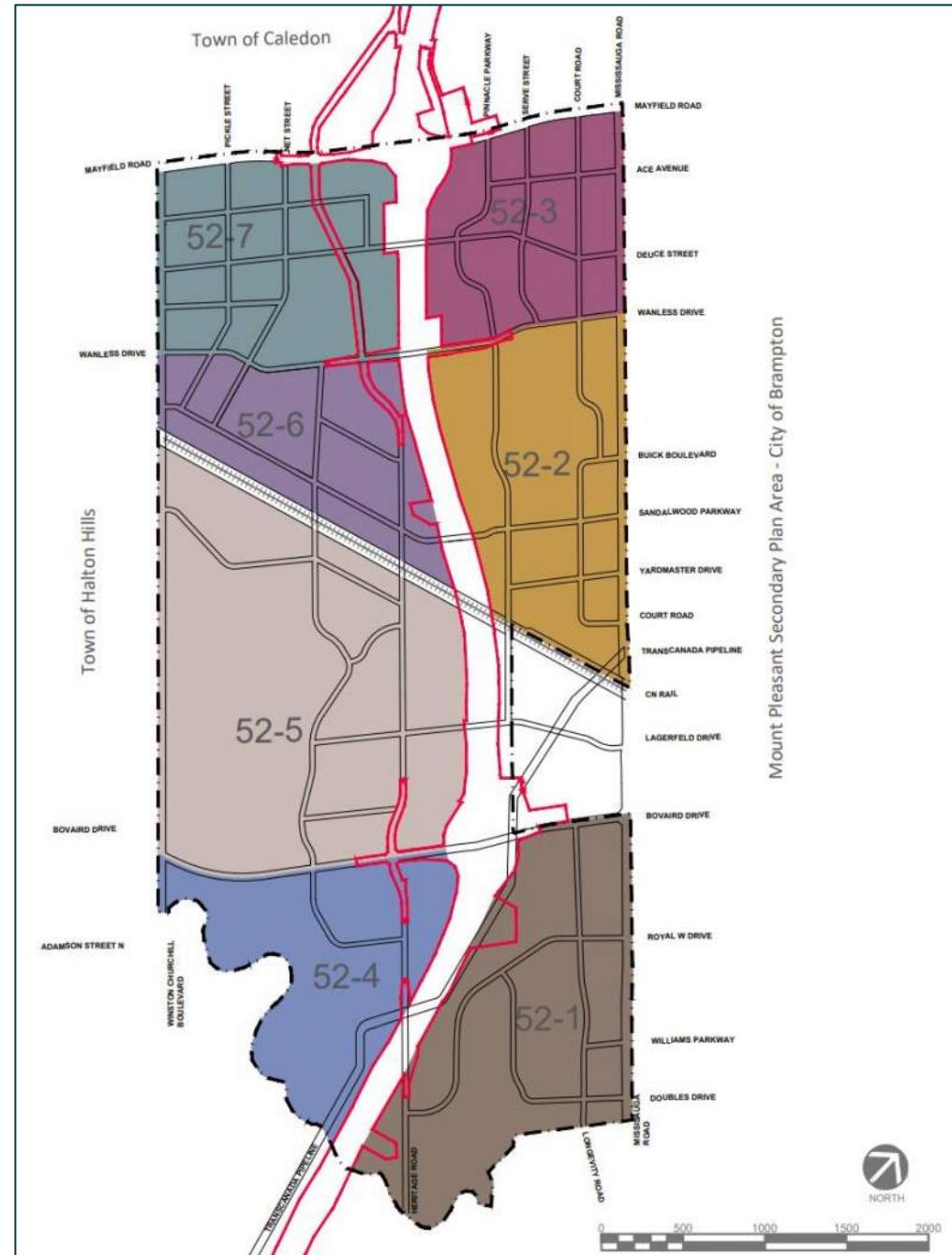
Secondary Plan Precinct Areas

“Precinct Plans will be developed incrementally over time for the Precinct Areas defined in this Secondary Plan. Precinct Plans are intended to outline development principles and guidelines at a more detailed level. Precinct Plans will demonstrate how lands can be developed to meet the vision and policies in this Secondary Plan.”

“Precinct Plans will a) Establish the location, scale and character of streets in addition to those already identified on Schedule 52-8 – Street Network; c) Identify infrastructure requirements to support development; f) Develop implementation strategies for ... infrastructure requirements to support growth in the Precinct;”

“Schedule 52-2 – Precinct Areas identifies the boundaries of seven (7) Precinct Areas, each of which will have a unique Precinct Plan.”

Section 3, “Precinct Area Structure” –
Heritage Heights Secondary Plan



Vision

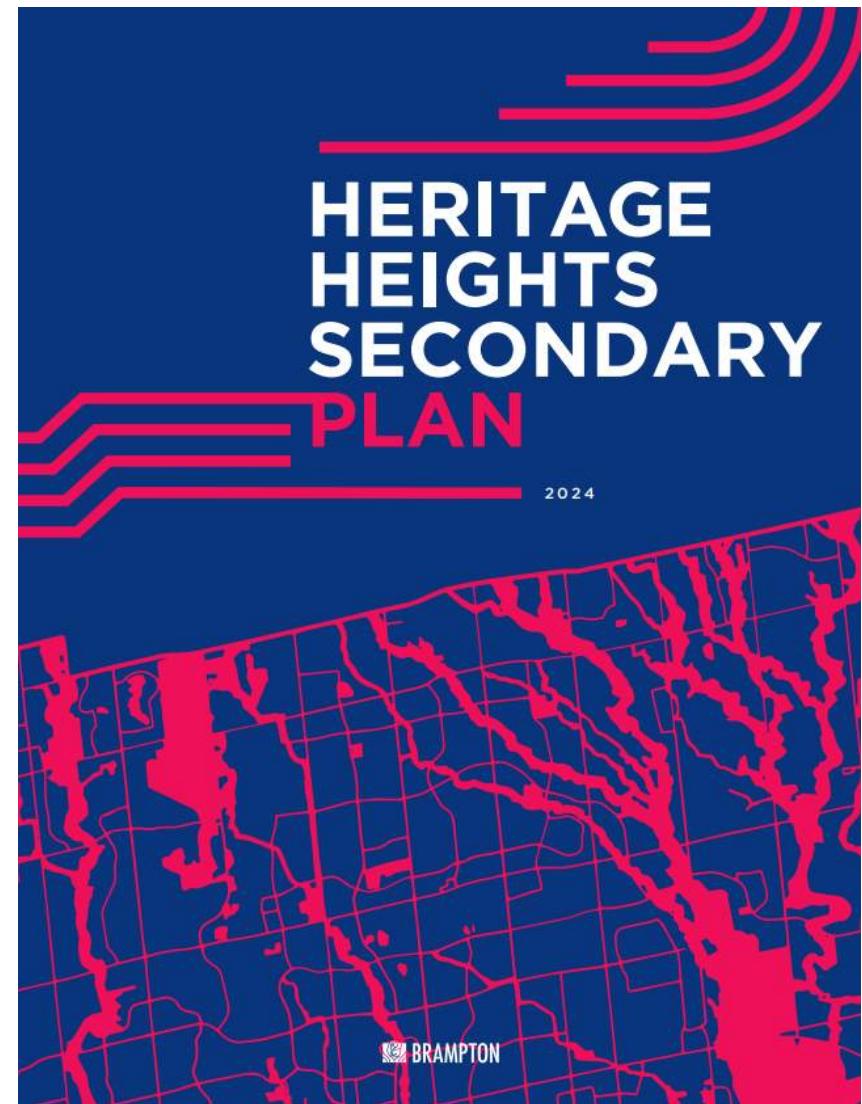
The following principles were created to guide the future design and growth policies in Heritage Heights and continue to be reflected in the revised Heritage Heights Secondary Plan:

1. Create **walkable communities** for people to gather, recreate, work and live.
2. Development should be **compact and diverse** to achieve walkable, affordable, and active neighbourhoods.
3. Implement **sustainable and resilient plans**, technologies and design approaches.
4. Include **arts and cultural uses** to leverage Brampton's diversity and attract investment.
5. Conserve the **natural and cultural heritage** of the area, creating a destination for local and regional visitors.
6. Foster a competitive environment for **employment and economic development**.
7. Plan for well-being - physical, mental and social - through the design of **people-centric spaces that are safe and age-friendly**.
8. Integrate and **connect green and open spaces** into the design of neighbourhoods **while being sensitive to existing ecological systems**.

“The Secondary Plan Area will be premised on strategies that promote a balanced approach to transportation and land uses, which leverages investment in transit and enhancements in the pedestrian and cycling environment to provide a range of travel choices and encourage more sustainable travel behaviours.

The Heritage Heights Secondary Plan will be planned to encourage a safe and attractive walking and cycling environment, public transit, and new streets and connections through precincts by providing pedestrian and cycling facilities, enhancing the public realm, and ensuring midblock connections are introduced with development to enhance connectivity and circulation.”

Section 10, “Mobility” – Heritage Heights Secondary Plan



Process

- The TMP is being undertaken in accordance with **Approach #2** of the Master Planning Process, as outlined in the Municipal Class Environmental Assessment (MCEA) (February 2024).
- The TMP will address Phases 1 and 2 of the MCEA process for any Schedule B projects and will form the basis for the recommended Schedule C transportation infrastructure projects identified within the TMP report.

4.5 Master Planning Process

The master planning process is discussed in Section A.2.7. A summary of the various approaches is provided below.

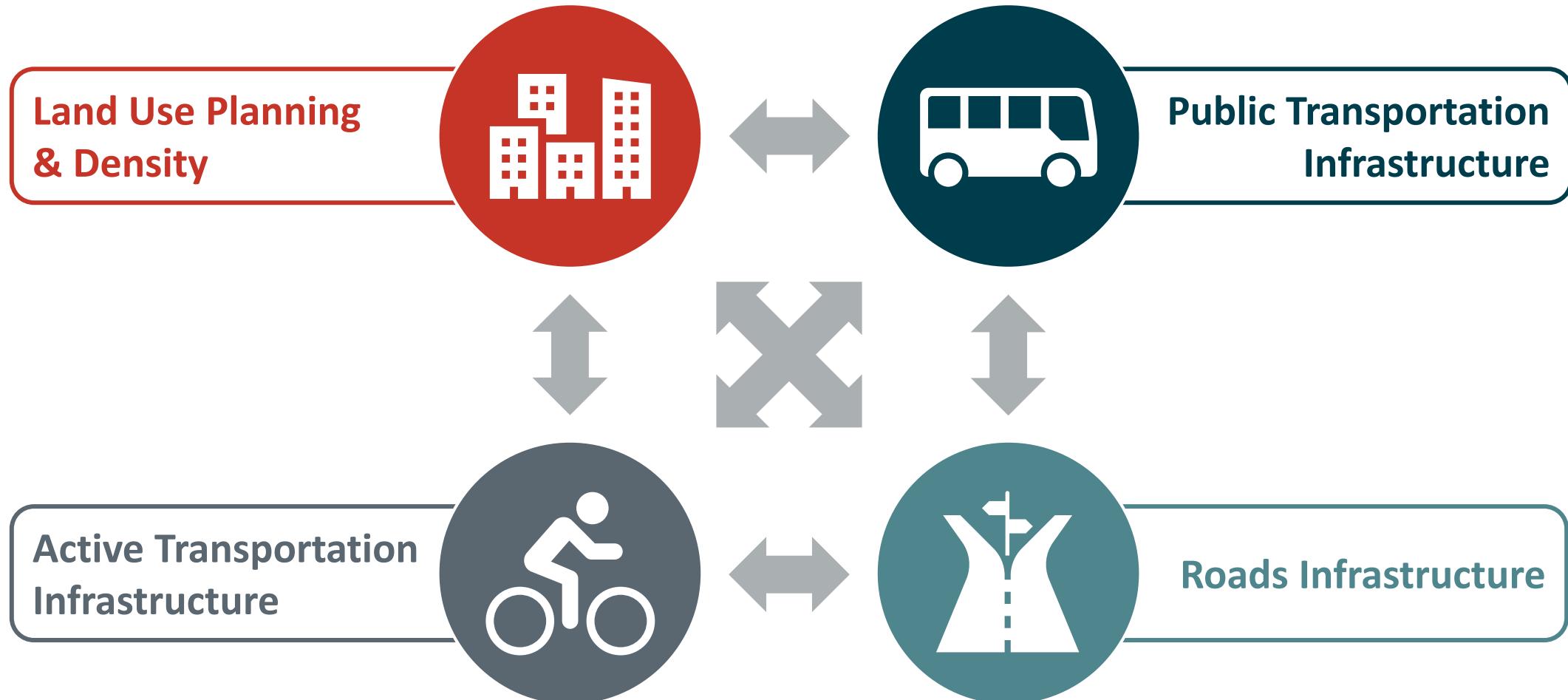
	Approach 1	Approach 2*	Approach 3*
Level of Detail	Broad (project specific information is minimal) Used as support for subsequent Schedule B and C project specific studies	Sufficient to fulfil requirements for Schedule B projects (more detailed project specific information is included) Used as support for subsequent Phase 3 and 4 for Schedule C project specific studies	Sufficient to fulfil requirements for Schedule B and C projects (more detailed project specific information is included)
Process	Preliminary Phase 1 and 2	Phase 1 and 2	Phase 1 to 4
Final Notice	Notice of Master Plan *This should not be called a Notice of Completion	Final notice becomes Notice of Completion for Schedule B projects	Final notice becomes Notice of Completion for Schedule B and C projects
Integration with Planning Act*	*Official Plan and Secondary Plan	*Official Plan and Secondary Plan, Plan of Subdivision, etc.	*Official Plan, Secondary Plan, Plan of Subdivision, etc.

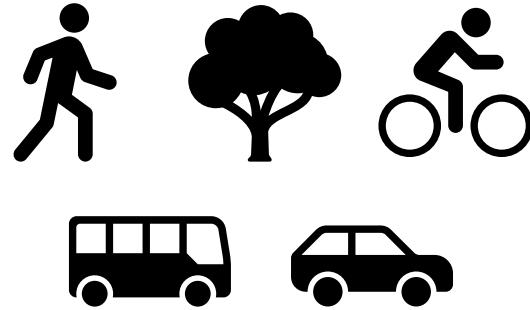
* The various master planning approaches provide proponents with flexibility to customize their master plans to suit their needs. See section A.2.7 for information on modified approaches 2 and 3.

Why a Transportation Master Plan?

1. **Ground Truth the Land Use Plan and the Transportation Network in the Secondary Plan**
2. **Test Options**
3. **Identify a Transportation Network** (roads, transit, active transportation) that supports the Land Use Plan.
4. **A Collaborative Approach.** Iterative Process and Benefits to future Planning Application processes.







Complete
Street Design

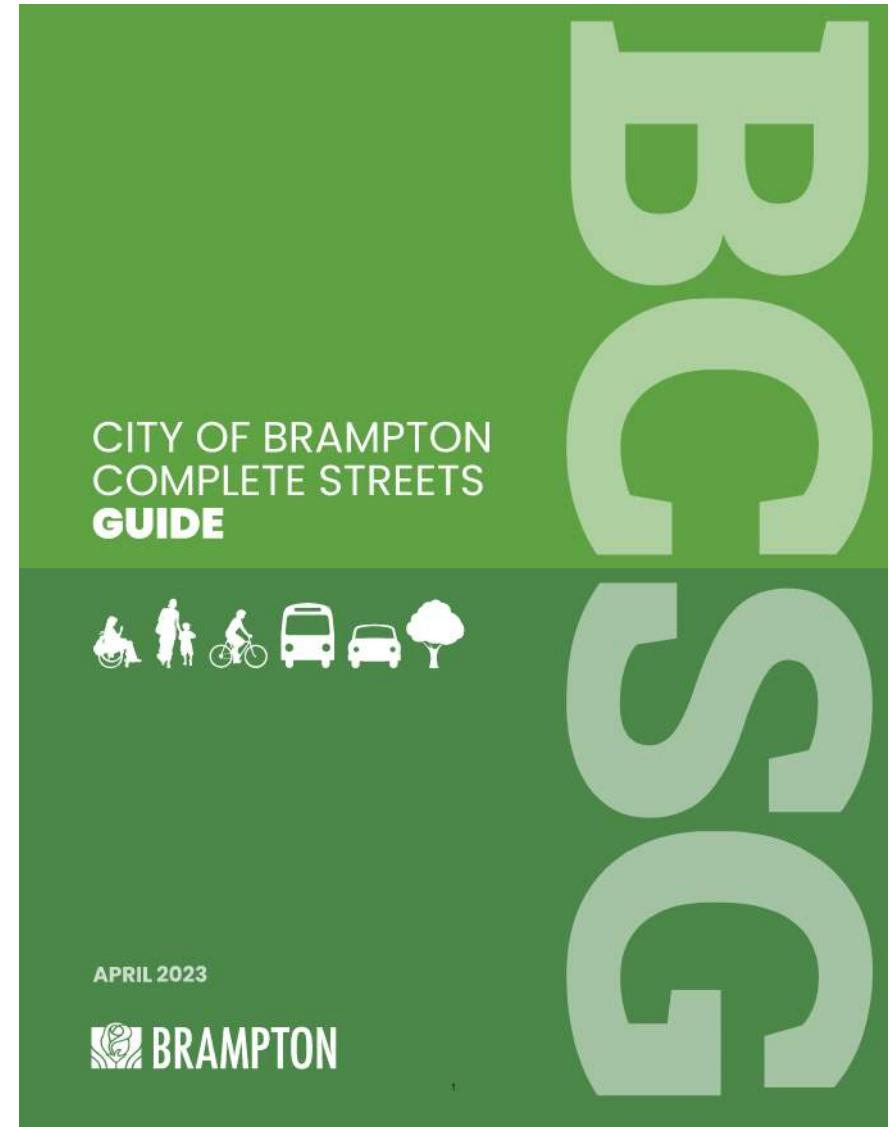


Future Mobility
Conditions
(Transportation
Forecasting Model)



Environmental
Assessment
Evaluation

- As reflected in municipal policy, **the City of Brampton**:
 - “**promotes a multi-modal transportation system**, with the objective of designing, building, and maintaining streets (including multi-use paths) that **safely and comfortably accommodate all users**, including motorists, motorcyclists, bicyclists, pedestrians, individuals with disabilities, transit and school bus riders, delivery and service personnel, freight haulers, and emergency responders.”
- Streets to be located in the Heritage Heights Secondary Plan area of all classification types – Arterial, Collector, Local – were studied for their street characteristics. A collaborative workshop series involving local landowners and City of Brampton staff was undertaken to establish street design principles. **New cross-sections are developed to be located within the Heritage Heights Secondary Plan area.**

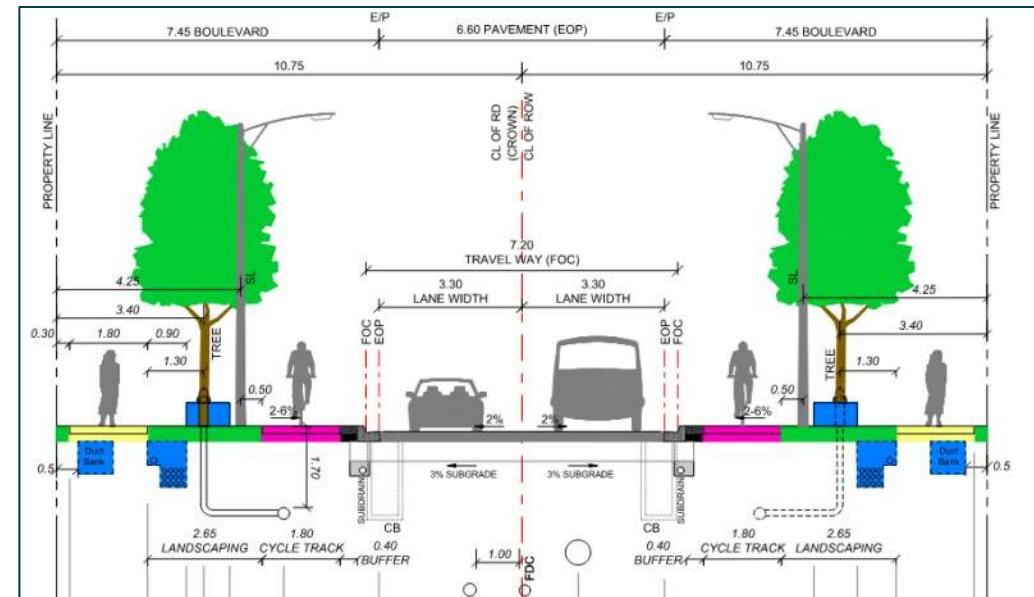
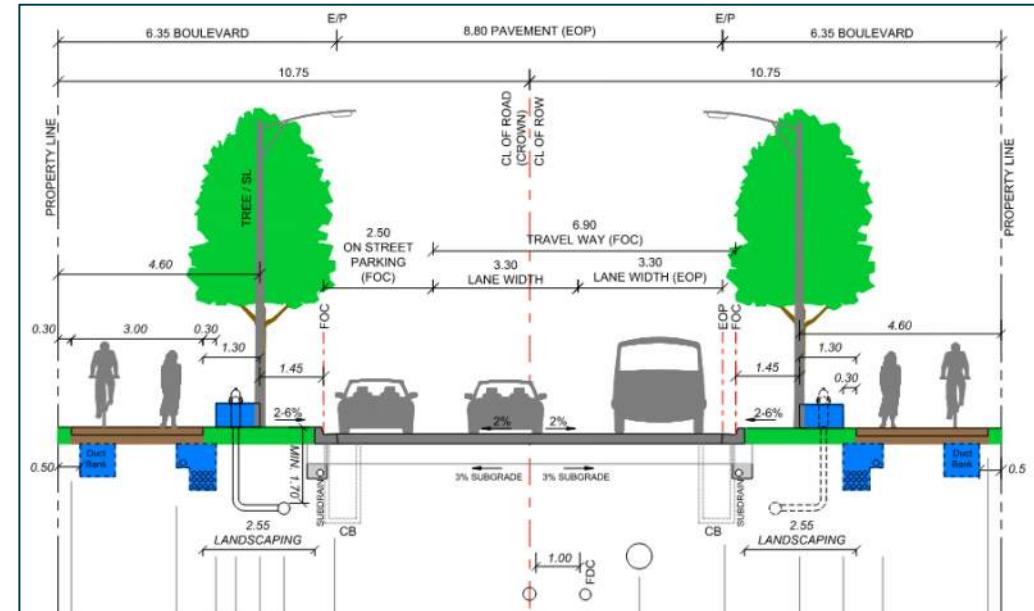


Heritage Heights Street Design

15

- Streets in the Heritage Heights Secondary Plan will look different than the rest of the City.
- Heritage Heights has built on the City's Complete Streets Guide to develop cross-sections unique to the Secondary Plan area that balance:
 - Cycling Facilities
 - Sidewalks
 - On-Street Parking
 - Transit-Friendly Travel Lanes
 - Landscaping
 - Aboveground-Underground Services
 - Compact Urban Design

Draft Street Section Examples



Future Mobility Conditions adopts a standard multi-resolution modelling approach (macroscopic-to-mesoscopic). Tools used include:

1. City of Brampton's Macroscopic Regional Demand Model

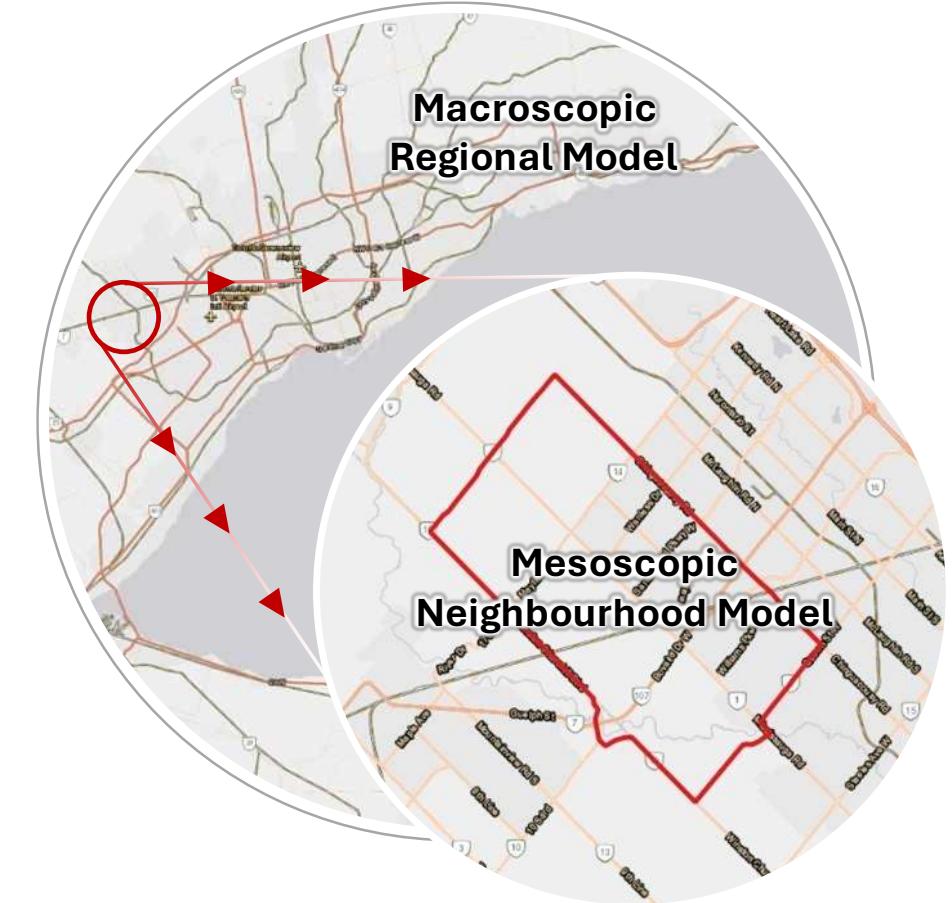
Forecasts transportation demand throughout the Greater Toronto-Hamilton Area

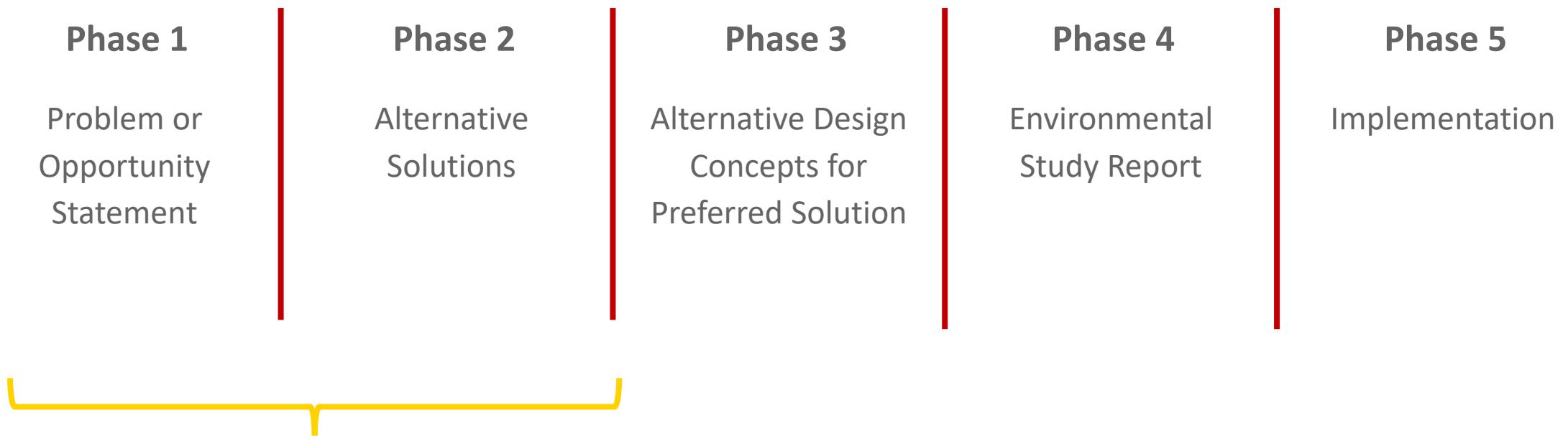
2. Heritage Heights Mesoscopic (“Neighbourhood”) Model

Assesses transportation network operations in and around the Heritage Heights Secondary Plan Area. Model boundaries:

- **Old School Rd to the North, Chinguacousy Rd to the East, Embleton Rd / Queen St W to the South, Winston Churchill Blvd to the West**

Macro-model outputs are used as meso-model inputs – allows for detailed and reliable corridor-level traffic volume projections that account for future planned regional and local growth and transportation infrastructure improvements.





The Heritage Heights Transportation Master Plan follows **Phases 1 and 2**

Alternative Solutions (Phase 2) in the Heritage Heights Transportation Master Plan consider road alignment alternatives and road crossings within the Master Plan road framework.



Street Network

- Road Network and Classification
- Transit Network
- Active Transportation Infrastructure
- Highway 413 Protected Corridor



Traffic Operations

- Weekday Morning and Afternoon Peak Hour Volumes and Capacity



Environmental Features

- Natural Heritage System, Greenbelt, Watercourses
- Terrestrial
- Archaeology Studies*

**Archaeology not required in Phase 1 and 2. Archaeology required prior to road construction.*



Constraints

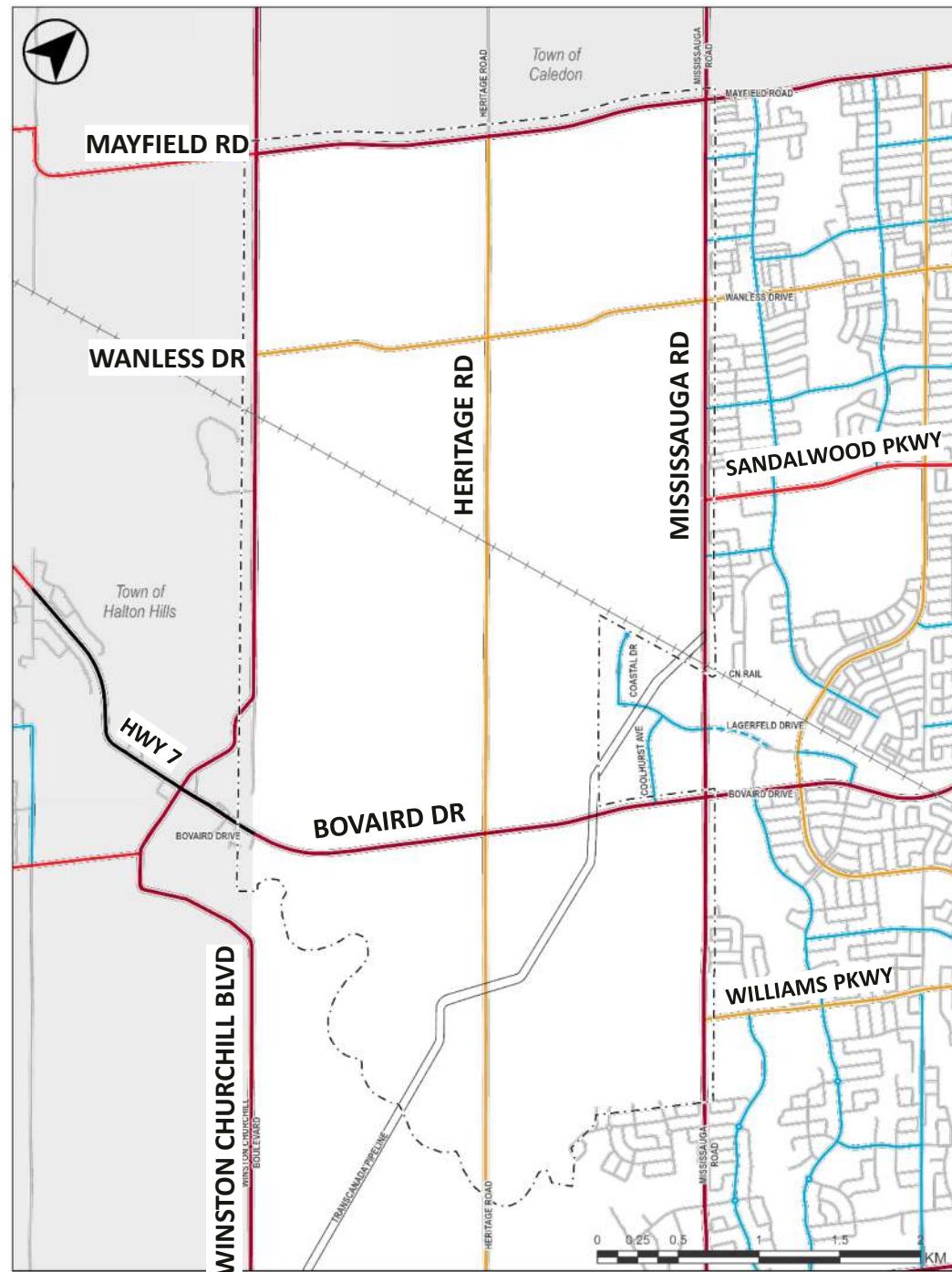
- Cultural Heritage Land and Built Heritage Resources
- Built and Planned Constraints

Existing Roads

- Major Arterial Roads (Regional)
 - Mayfield Road, Bovaird Drive*, Winston Churchill Boulevard, Mississauga Road
- Minor Arterial Roads (City)
 - Wanless Drive, Heritage Road

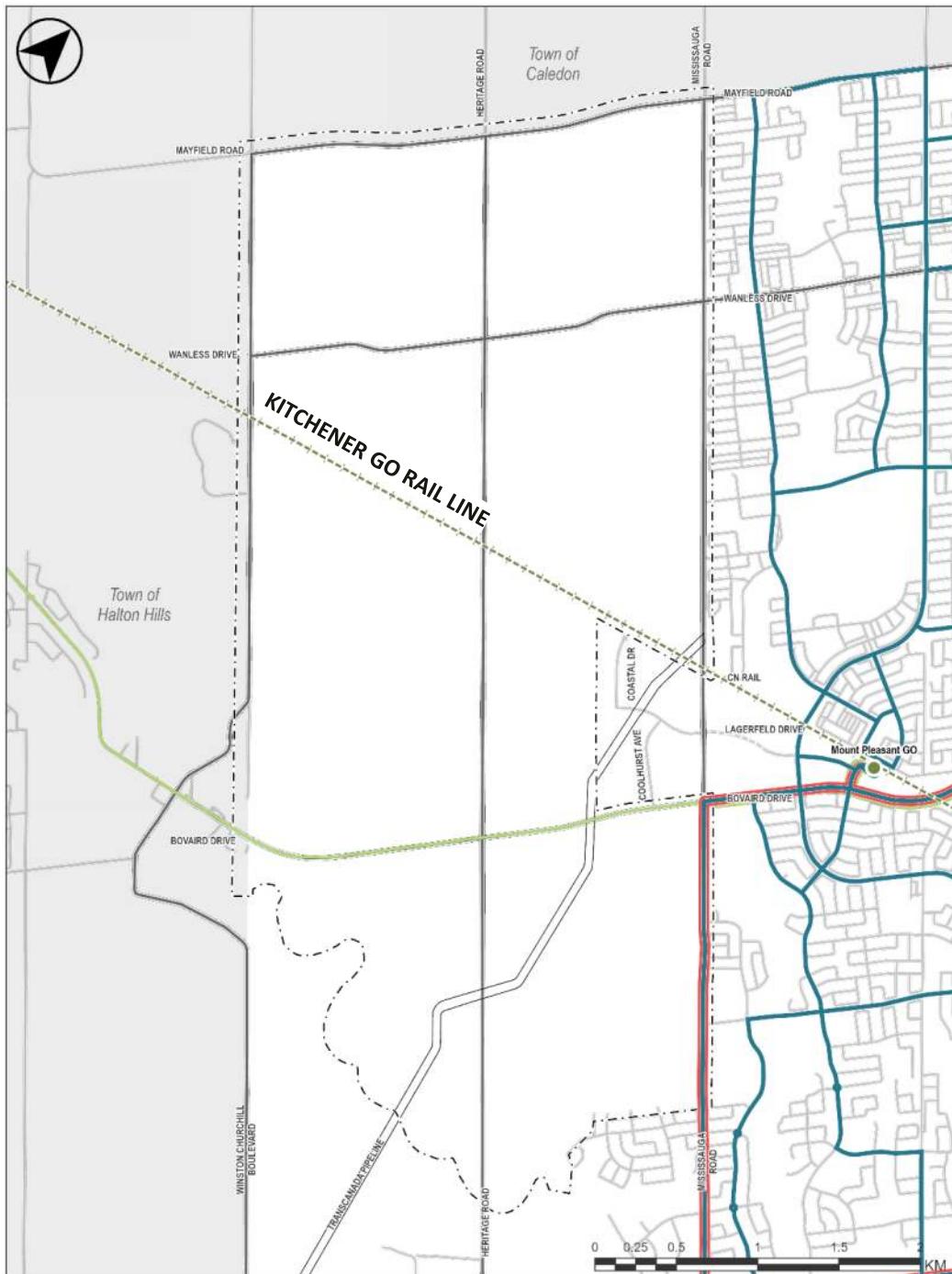
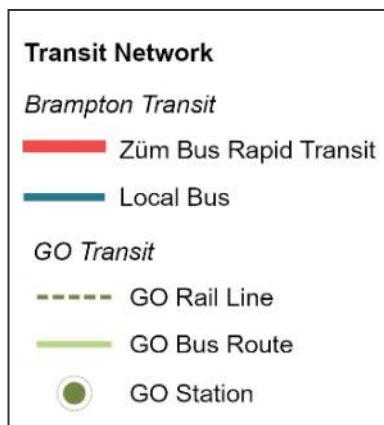
Road Classification	
■	Highway
■	Major Arterial (Regional)
■	Major Arterial (City)
■	Minor Arterial (City)
■	Collector (City)
■	Local (City)

*Bovaird Drive transitions into Highway 7 (Provincial Highway) in Norval (Halton Hills)



Existing Transit

- **Brampton Transit** local bus routes operate east of the Secondary Plan area in Mount Pleasant.
 - Züm bus and local bus routes are provided along Mississauga Road and Bovaird Drive.
- The **GO Transit Kitchener Line** runs along the CN Rail corridor through the Secondary Plan area.
- **Mount Pleasant GO Station** is located along the CN Rail corridor, approximately 1.25-km east of the Secondary Plan area boundary.



Existing Active Transportation

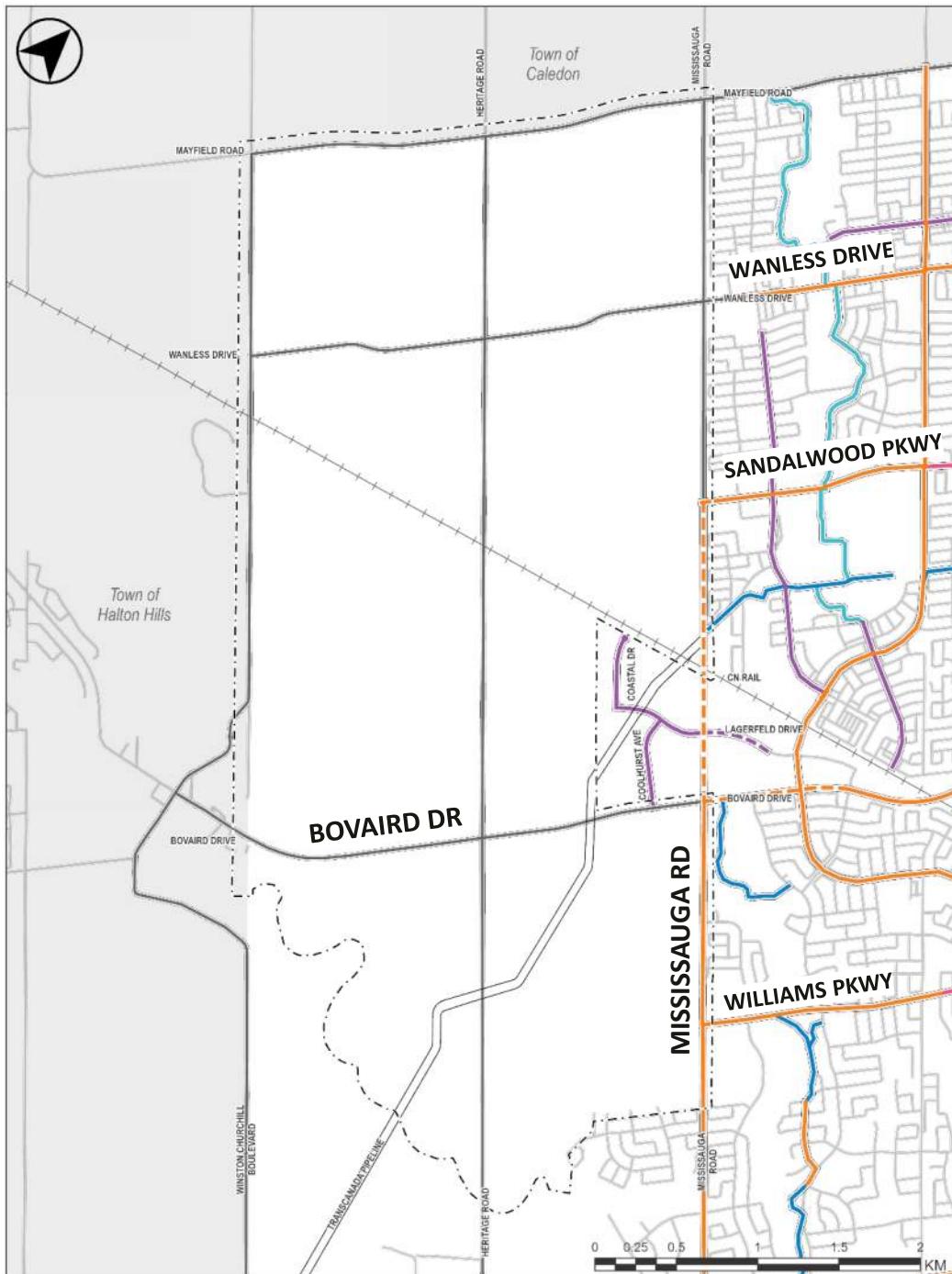
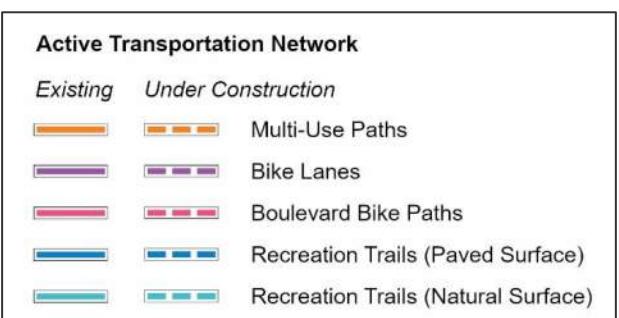
- Active Transportation infrastructure is provided east of the Secondary Plan area.

- **Multi-Use Paths**

- Mississauga Road
- Bovaird Drive
- Sandalwood Parkway
- Williams Parkway
- Wanless Drive

- **Bike Lanes**

- Lagerfeld Drive
- Coolhurst Avenue
- Coastal Drive

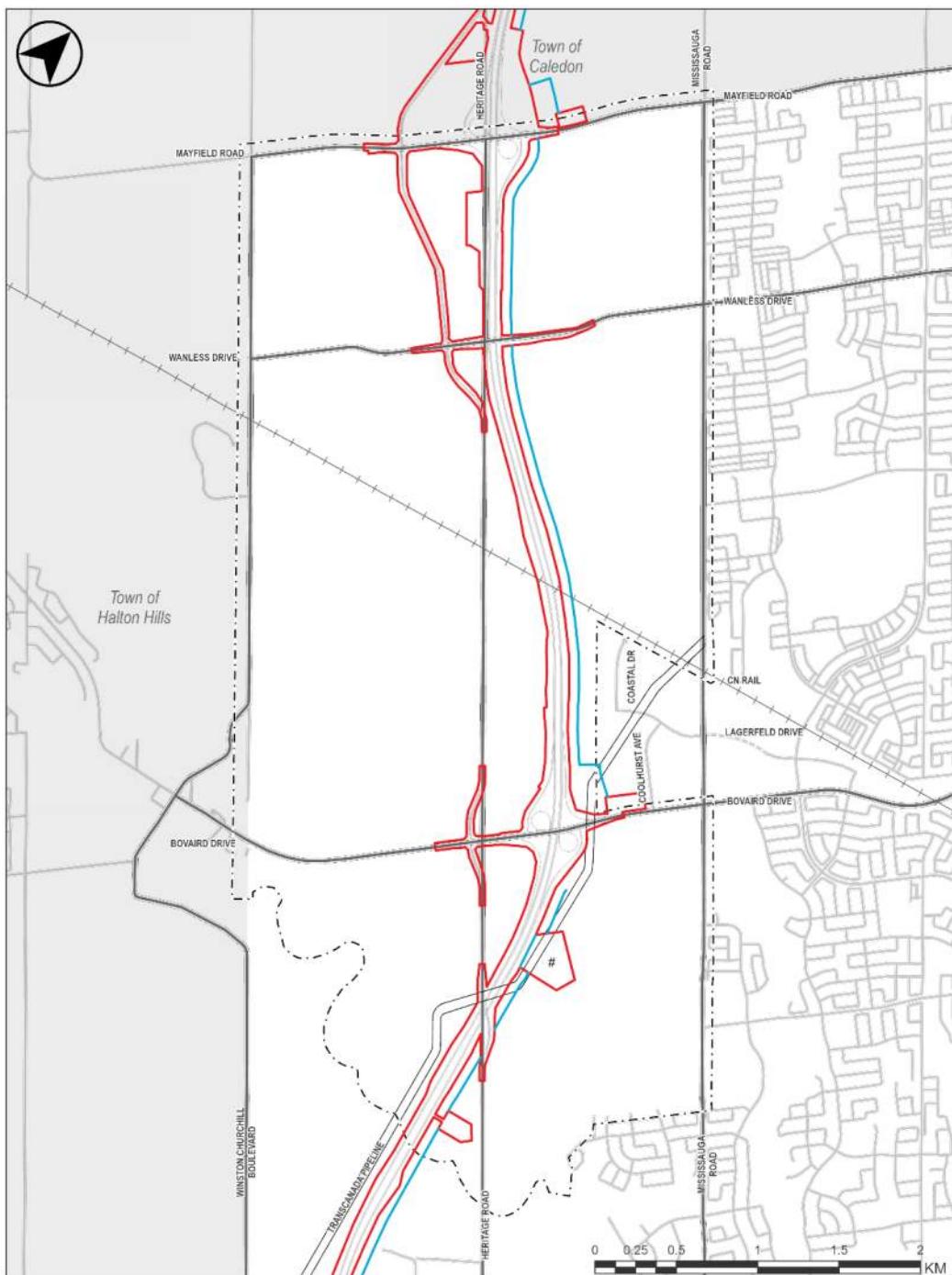


Highway 413 Protected Corridor

- Highway 413 is a:
 - proposed provincial highway connecting Halton, Peel, and York Regions.
 - protected corridor currently at 90% design and is subject to change as planning progresses.
- Highway 413 corridor includes provisions for highway and transitway incorporated on road crossings of the corridor.



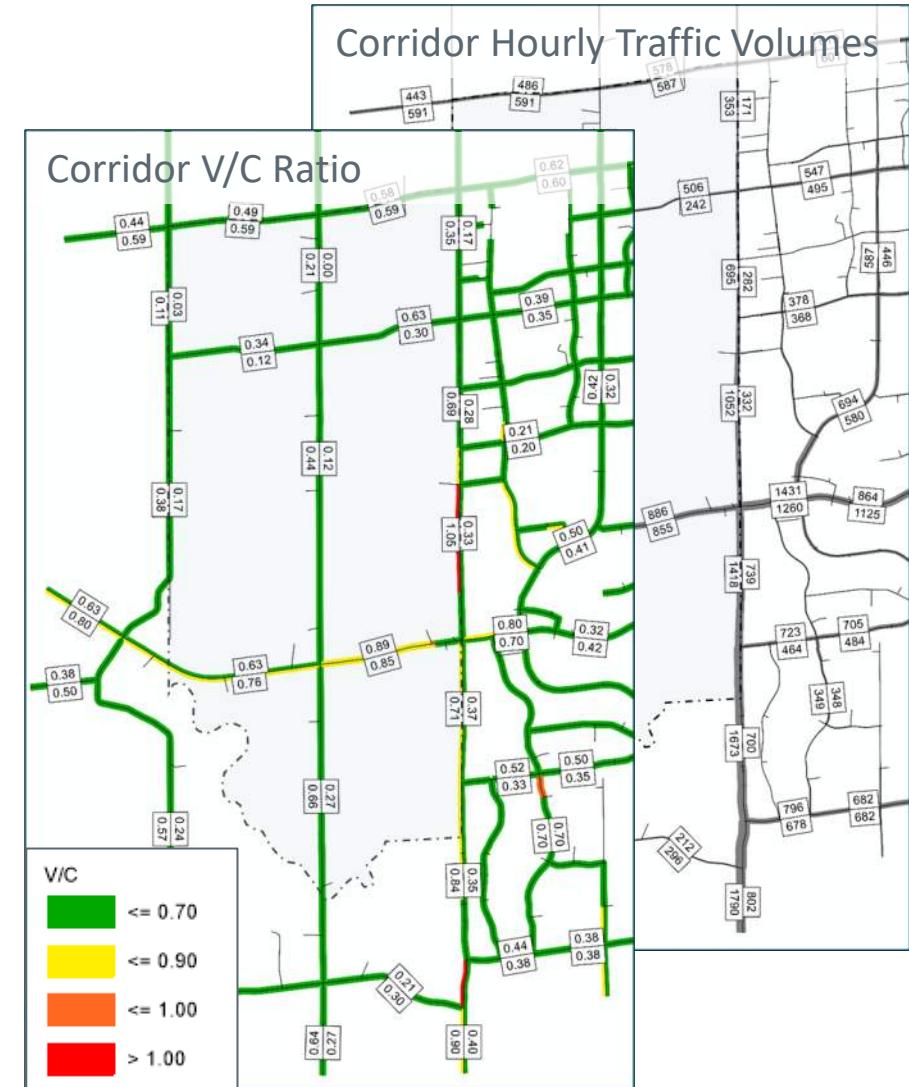
Highway 413 Corridor at 90% preliminary design, subject to change.
SWM pond in Precinct 52-1 to be relocated in consultation with Province.



Traffic Operations – Metrics

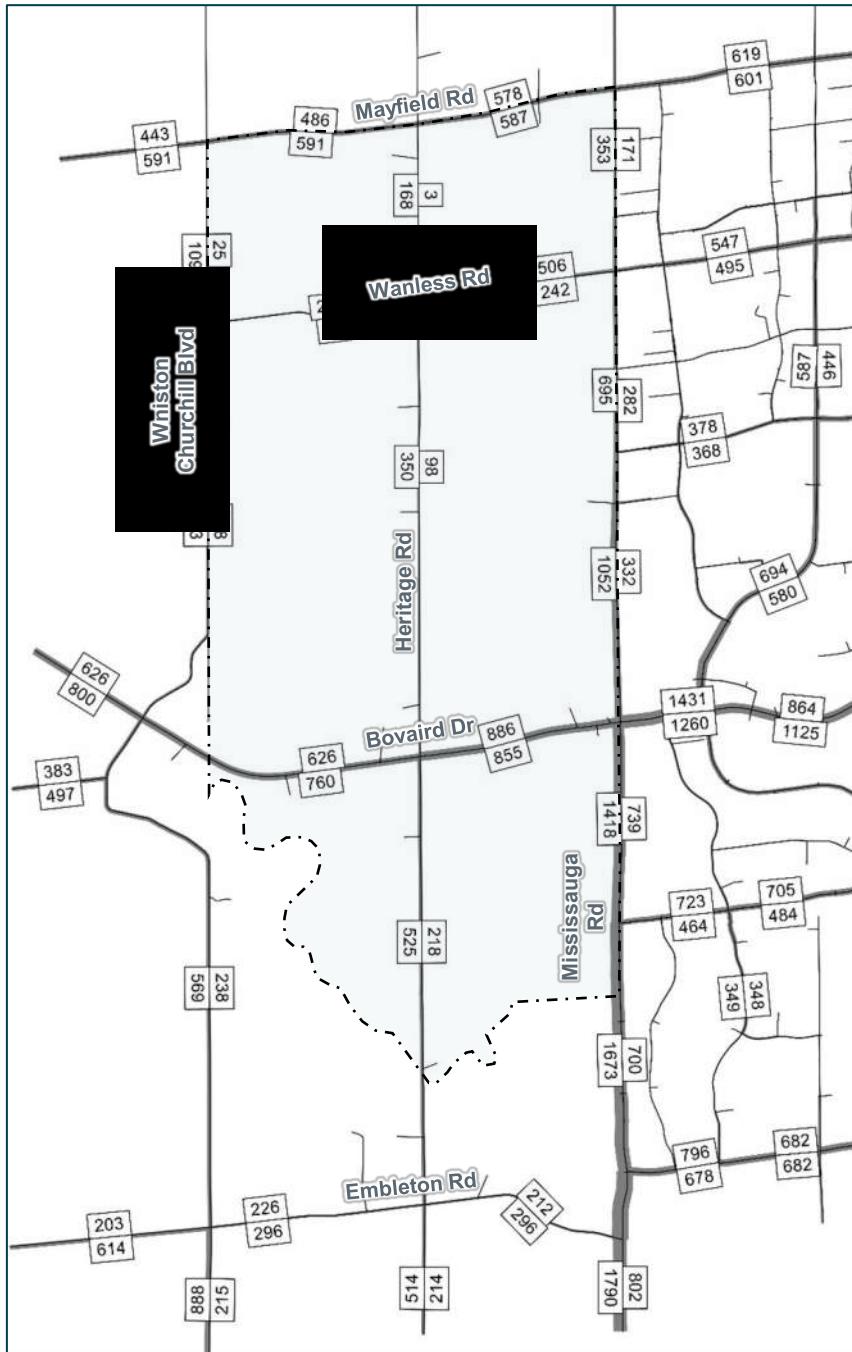
Output Samples

- Traffic operations outputs/metrics include **corridor hourly traffic volumes** and **volume-to-capacity (V/C) ratios**
- V/C ratio is a metric that measures the level of service on a given corridor segment.
- **V/C Ratio = Hourly Traffic Volume ÷ Theoretical Hourly Carrying Capacity**
 - **Green (V/C ≤ 0.70):** not busy, well within capacity
 - **Yellow (V/C ≤ 0.90):** moderately busy, still within capacity
 - **Orange (V/C ≤ 1.00):** very busy, nearing capacity
 - **Red (V/C > 1.00):** over capacity, congested with significant delays

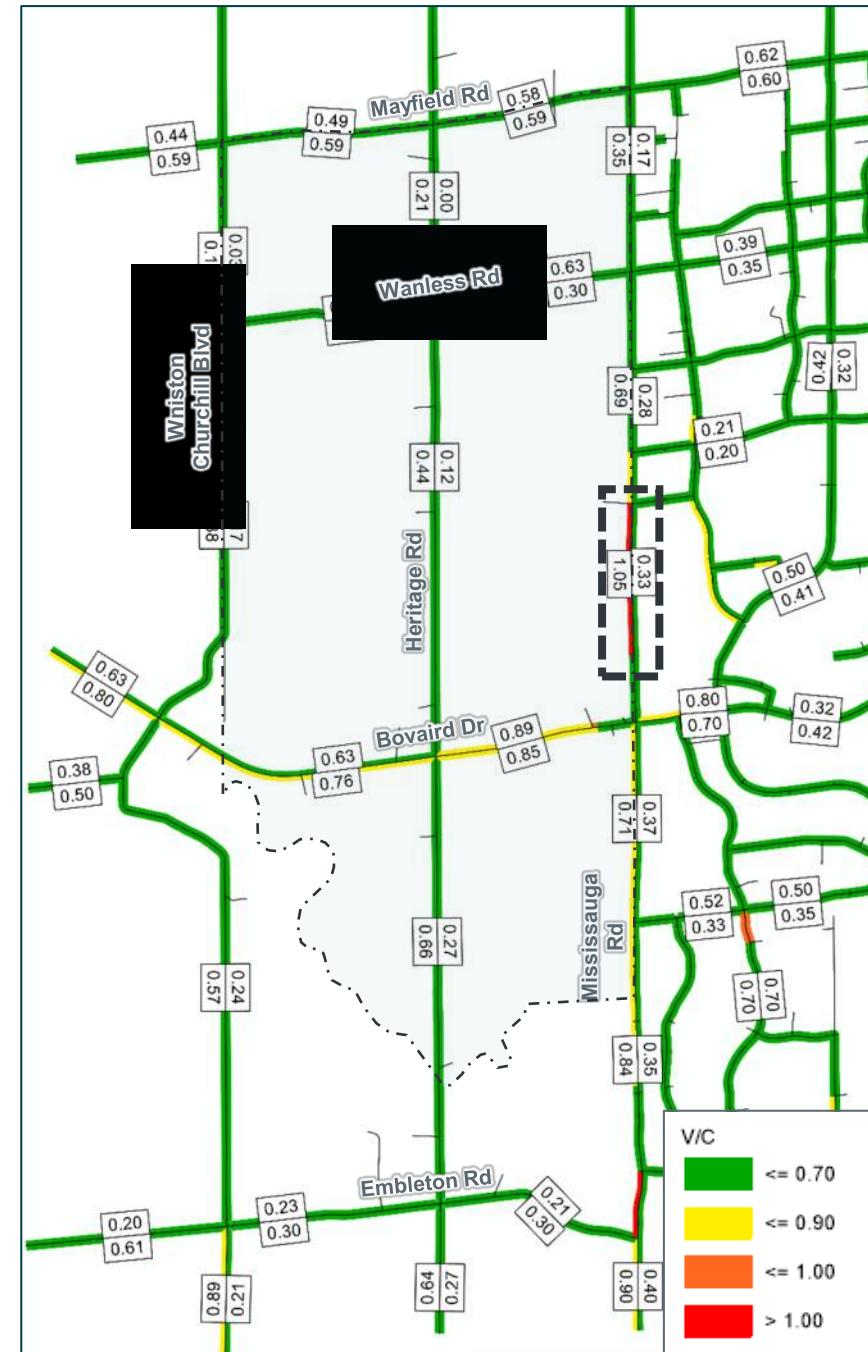


Existing Traffic Operations Weekday AM Peak Hour

- Traffic operations during the **weekday morning peak hour** in the Heritage Heights SPA are generally well within acceptable limits
- Mississauga Road in the southbound direction north of Bovaird Dr is over capacity.



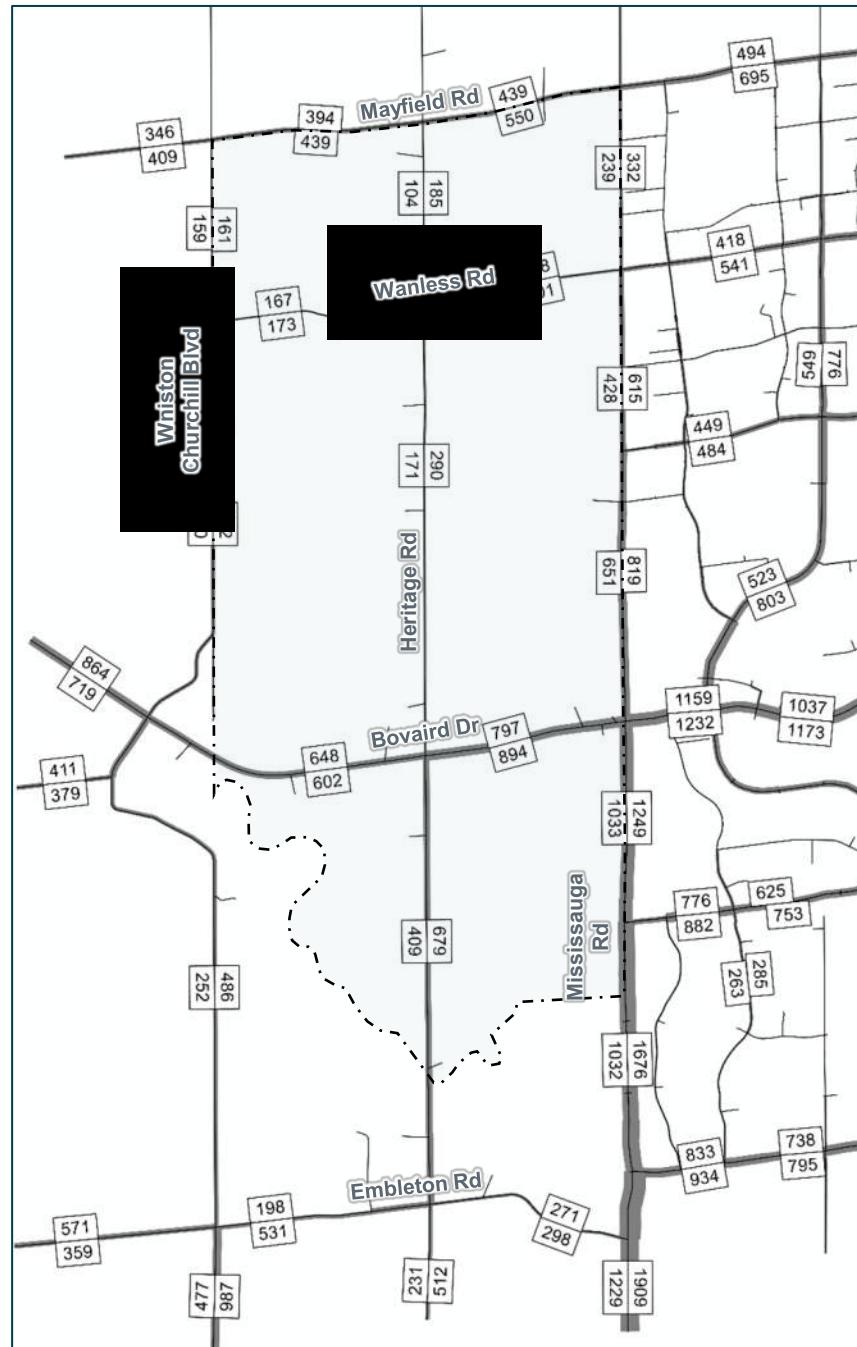
Existing Weekday Morning Peak Hour - Corridor Traffic Volumes (per hour)



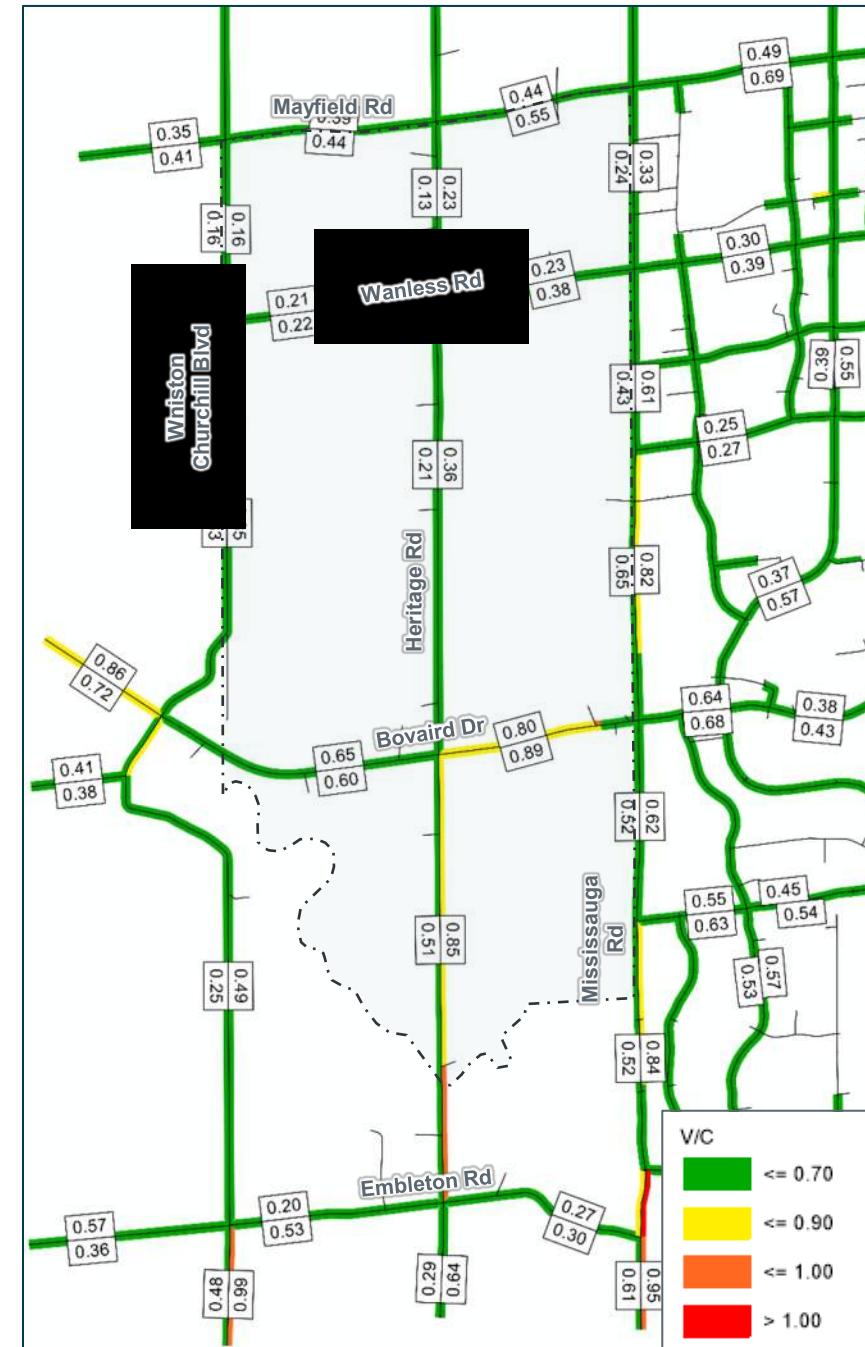
Existing Weekday Morning Peak Hour - Corridor Volume-to-Capacity Ratios

Existing Traffic Operations Weekday PM Peak Hour

- Traffic operations during the **weekday afternoon peak hour** in the Heritage Heights SPA are generally well within acceptable limits



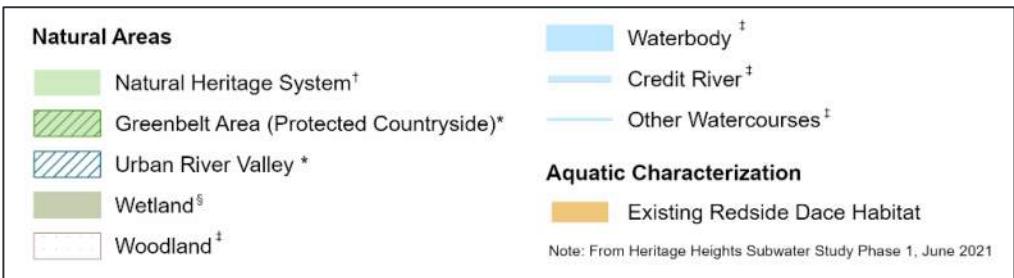
Existing Weekday Afternoon Peak Hour - Corridor Traffic Volumes (per hour)



Existing Weekday Afternoon Peak Hour - Corridor Volume-to-Capacity Ratios

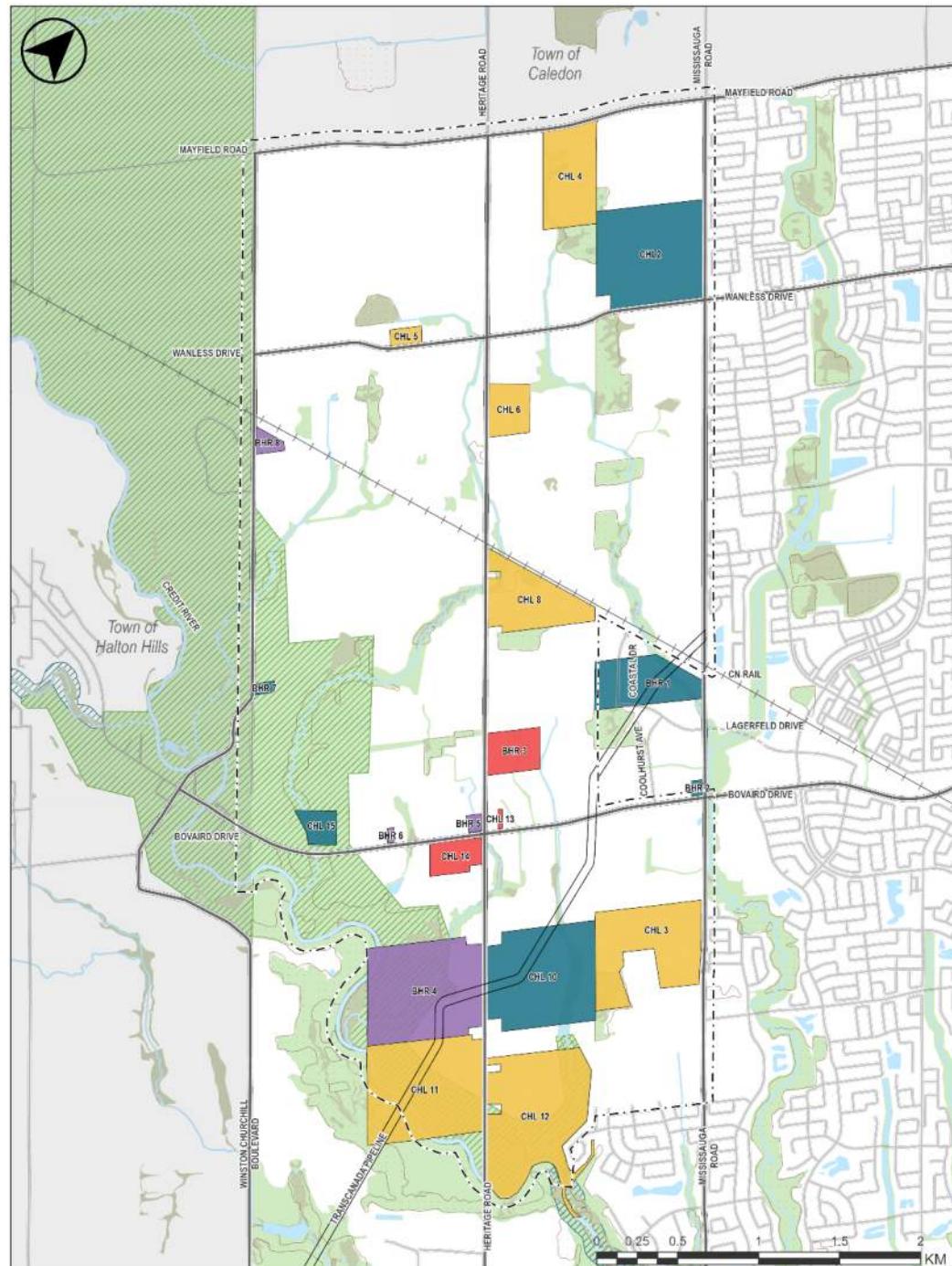
Existing Natural Areas

- Natural Heritage System
- Greenbelt Area (Protected Countryside)
 - Urban River Valley
- Watercourses & Waterbodies
 - Credit River and Credit River Valley
- Woodland & Wetlands
- Redside Dace Habitat



Existing Cultural and Built Heritage Resources

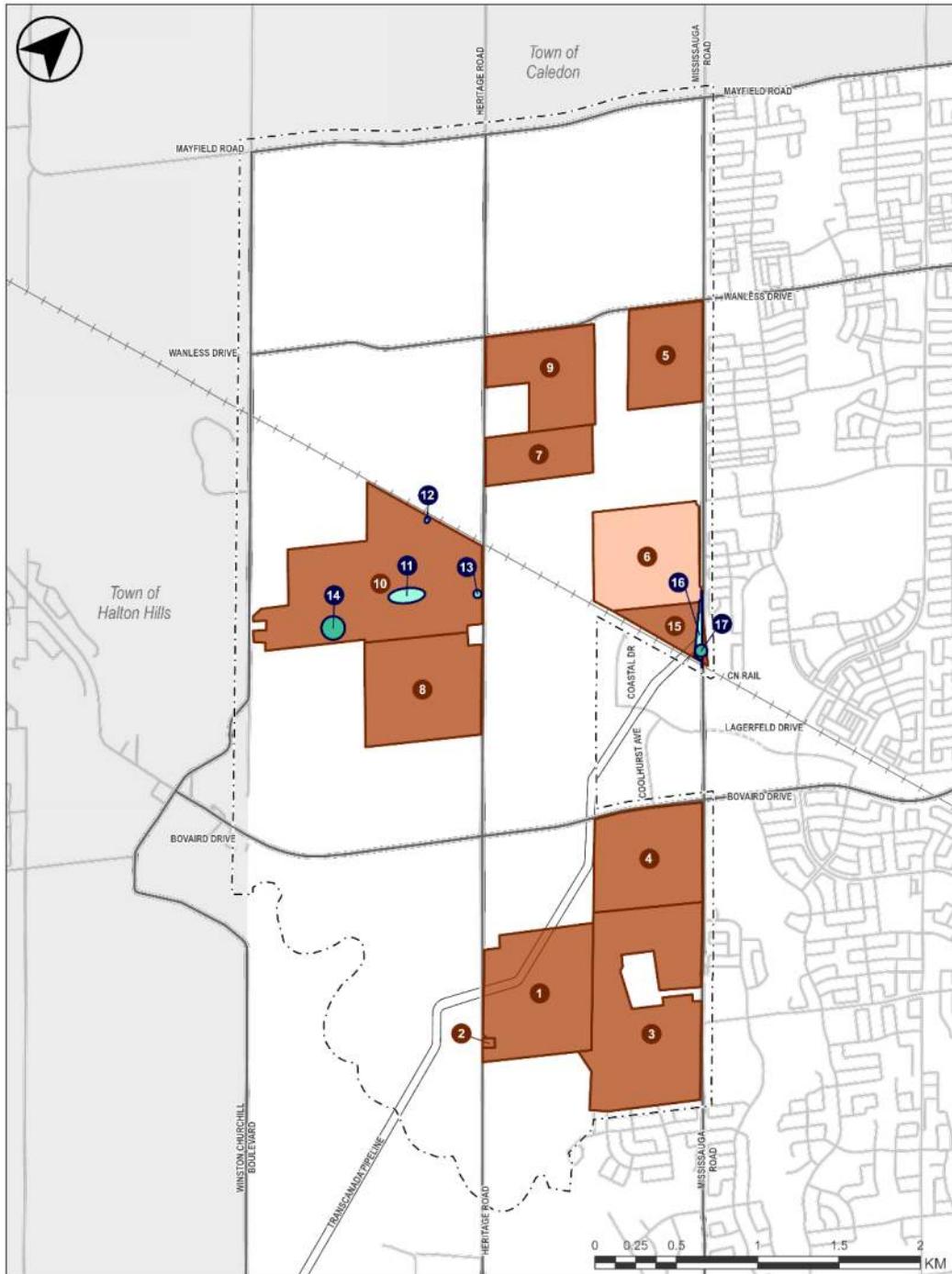
- ASI completed a Cultural Heritage Study for Heritage Heights Community study area in October 2020
- Area has a rural land use history dating back to early nineteenth century with 21 active resources (13 farmscapes, 6 rural residential properties, 1 place of worship and 1 cemetery) as shown in the Figure
- Some of the resources are potentially impacted by road alignments and these could consider implementation of management or mitigation strategies to minimize impacts



Existing Archaeology Studies

- Archaeological Assessments (AA) (Stages 1 and 2) have been completed or are underway in most of the study area and provides an understanding of potential archaeological impacts from road construction
- Most of the Stage 2 work has resulted in clearance under the Heritage Act however there are a few areas that require additional Stages 3 and 4 AA and these have been or will be completed during detailed design and prior to road construction
- Any areas where Stages 1-2 have not been completed will be assessed during detailed design to confirm road alignments
- While AA are not required for a Transportation Master Plan/Phases 1 and 2 of the Class EA process these must be completed prior to initiation of road construction activities in an area

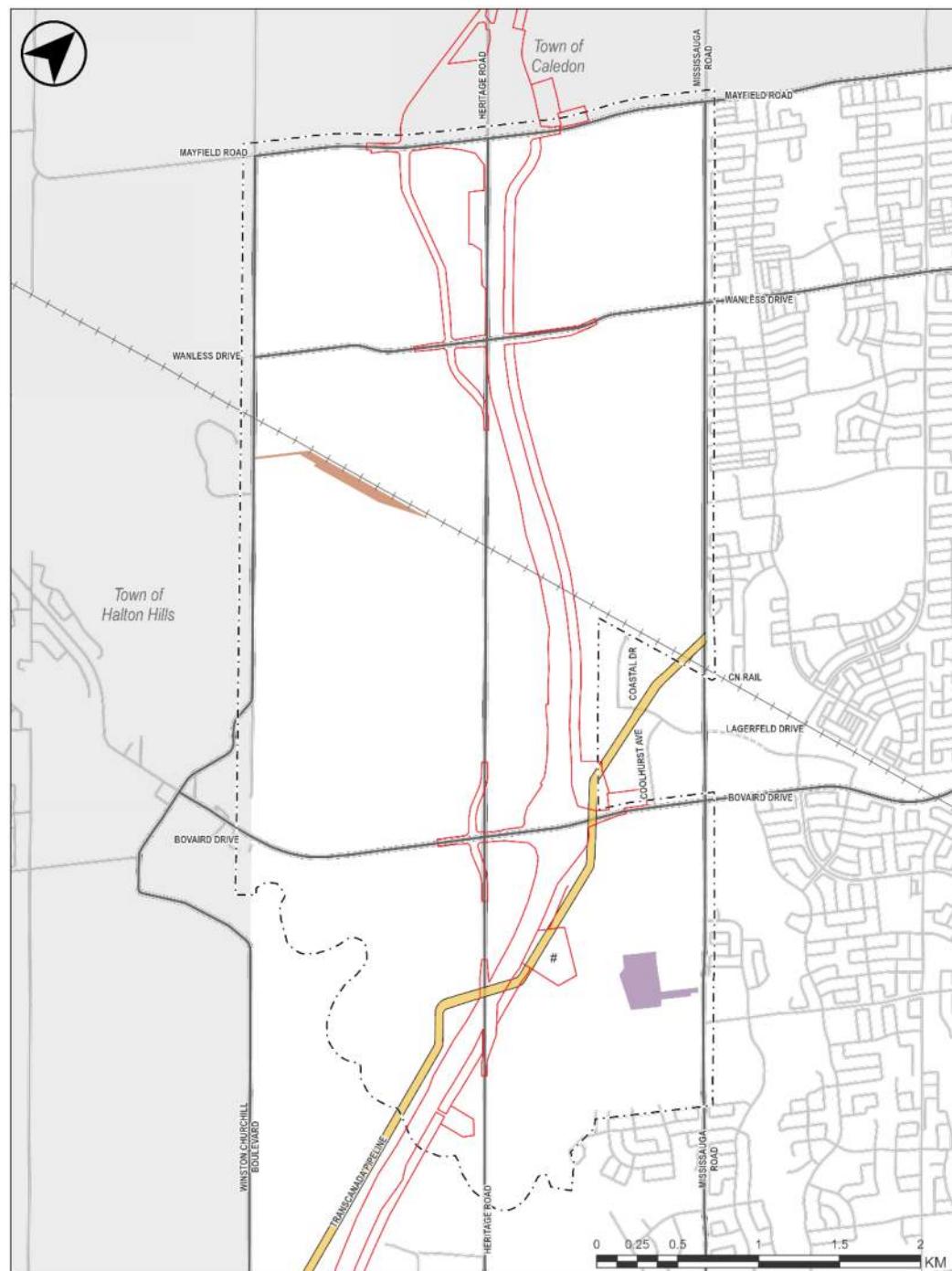
Archaeological Assessment Stage	Completed Archaeology Studies
Stage 1-2	(9) TFP Heritage Wanless Inc.
Stage 1-3	(1) Argo TFP III
Stage 3	(2) Argo TFP V
Stage 4	(3) DG Lands
	(4) 2055 Bovaird Drive W
	(5) Lot 15, Concession 5
	(6) Lot 13, Concession 5
	(7) 1761540 Ontario Limited Lands
	(8) 10294 & 10378 Heritage Road
	(10) 10510 Heritage Road
	(11) Cameron Site
	(12) McNichol's Cemetery
	(13) 10510 H5 Site
	(14) McNichol Site
	(15) Lot 12, Concession 5
	(16) McDowell Site - Stage 3
	(17) McDowell Site - Stage 4



Existing Built and Planned Constraints

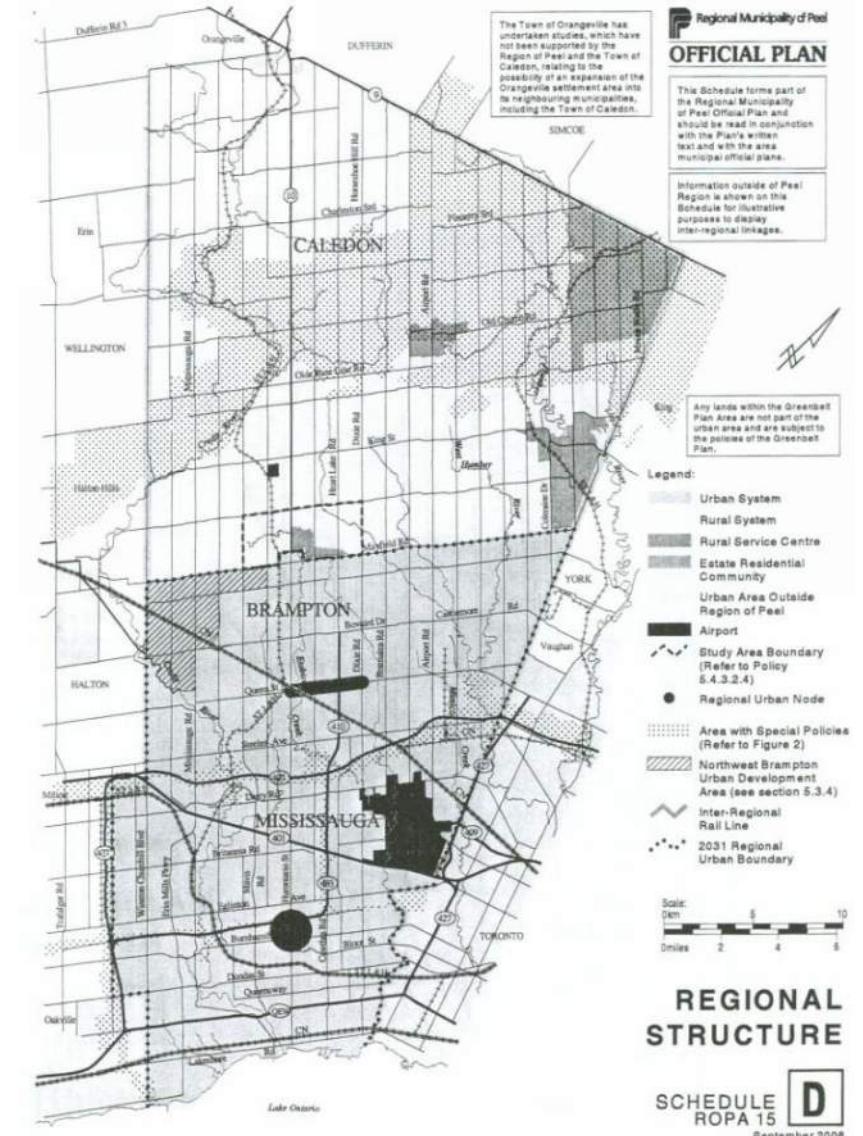
- Highway 413 Transportation Corridor (Planned)
- TransCanada Pipeline
- CN Rail Line (Kitchener GO Line)
- Metrolinx Heritage Road Layover Facility
- Peel Region Pumping Station

Constraints
— Highway 413 & Transitway Preliminary Right-of-Way #
—+— CN Rail Line
— TransCanada Pipeline
— Metrolinx Heritage Road Layover Facility
— Peel Region Pumping Station



Moving from Existing to Future

- Much of the City of Brampton is comprised of existing neighbourhoods that are largely built out. Aside from intensification in Strategic Growth Areas, new development will occur in the City's remaining greenfield areas, the largest of which is the Heritage Heights Secondary Plan area.
- In the **Region of Peel Official Plan, the entirety of the Heritage Heights Secondary Plan area – with the exception of the Greenbelt lands – is designated as “Urban System”** in Schedule E-1 (Regional Structure).
 - The “Urban System” designation has been in effect since 2006, originally legislated by City of Brampton Official Plan Amendment OP93-245 and Peel Region Official Plan Amendment 15 (ROPA 15), and subsequently approved by the Ontario Municipal Board (OMB).

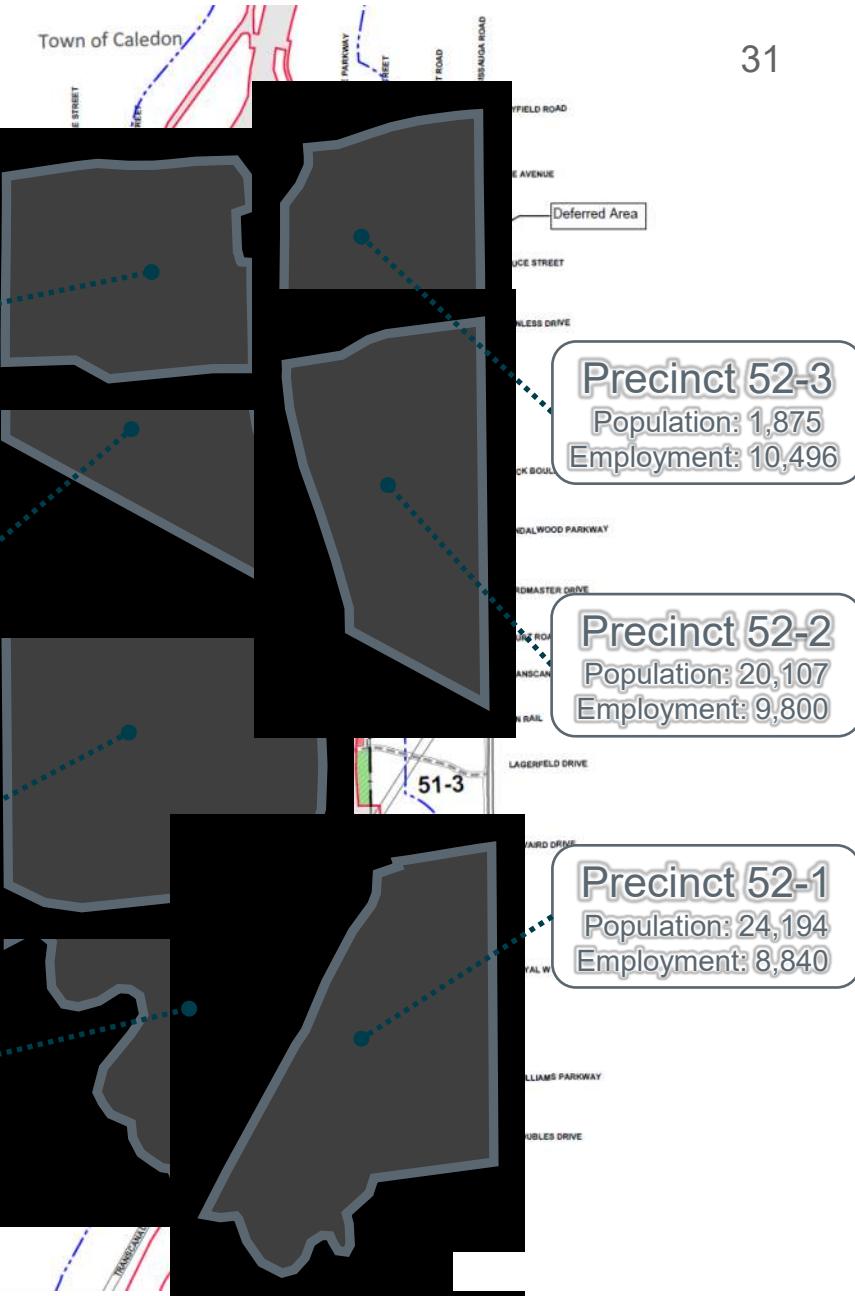
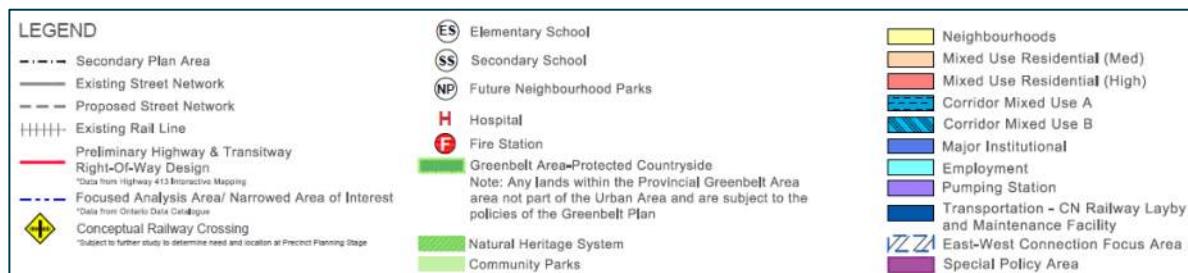


What's Planned for Heritage Heights?

“Seven distinct Precincts, which will accommodate both people and jobs through a full range of uses including employment, mixed-use, institutional and civic, and various housing forms, tenures and types” – Section 2, “Vision and Guiding Principles” - Heritage Heights Secondary Plan

Heritage Heights Secondary Plan Area is planned to accommodate **132,188 people** and **54,307 jobs**

“A connected system of green corridors and pedestrian, cycling, and street networks will promote walkability, accessibility, and connections to transit, and local and regional destinations” – Section 2, “Vision and Guiding Principles” - Heritage Heights Secondary Plan



Testing the Proposed Secondary Plan Road Network

- The Proposed Secondary Plan road network was tested based on planned growth in population and employment to identify:
 - **Critical constraints in transportation capacity** that call for new infrastructure or infrastructure improvements, and
 - **Opportunities to respond to known environmental, built, and planned features** through:
 - Road re-alignments and
 - Re-evaluation of Road, Rail, and Environmental Crossings

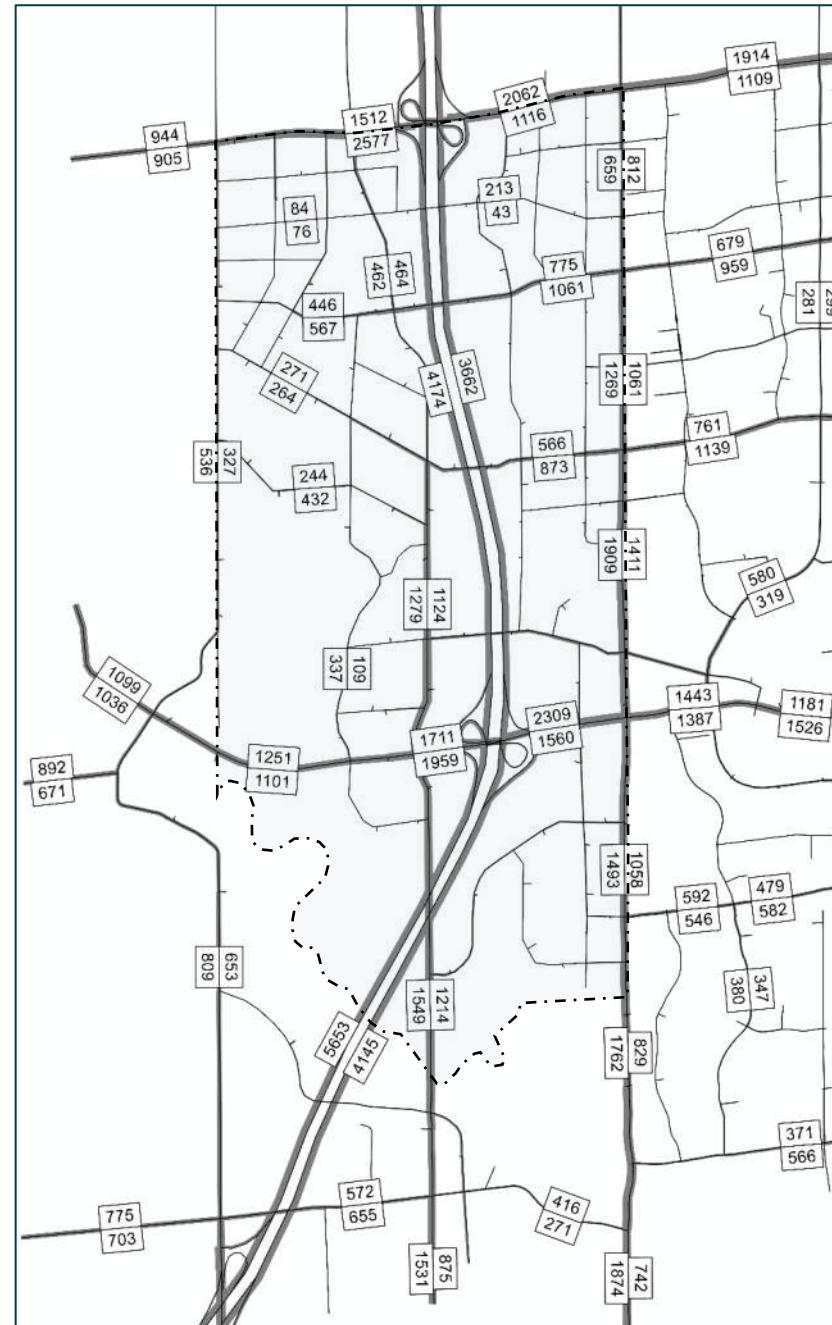


Future Traffic Operations 2051, Morning Peak Hour

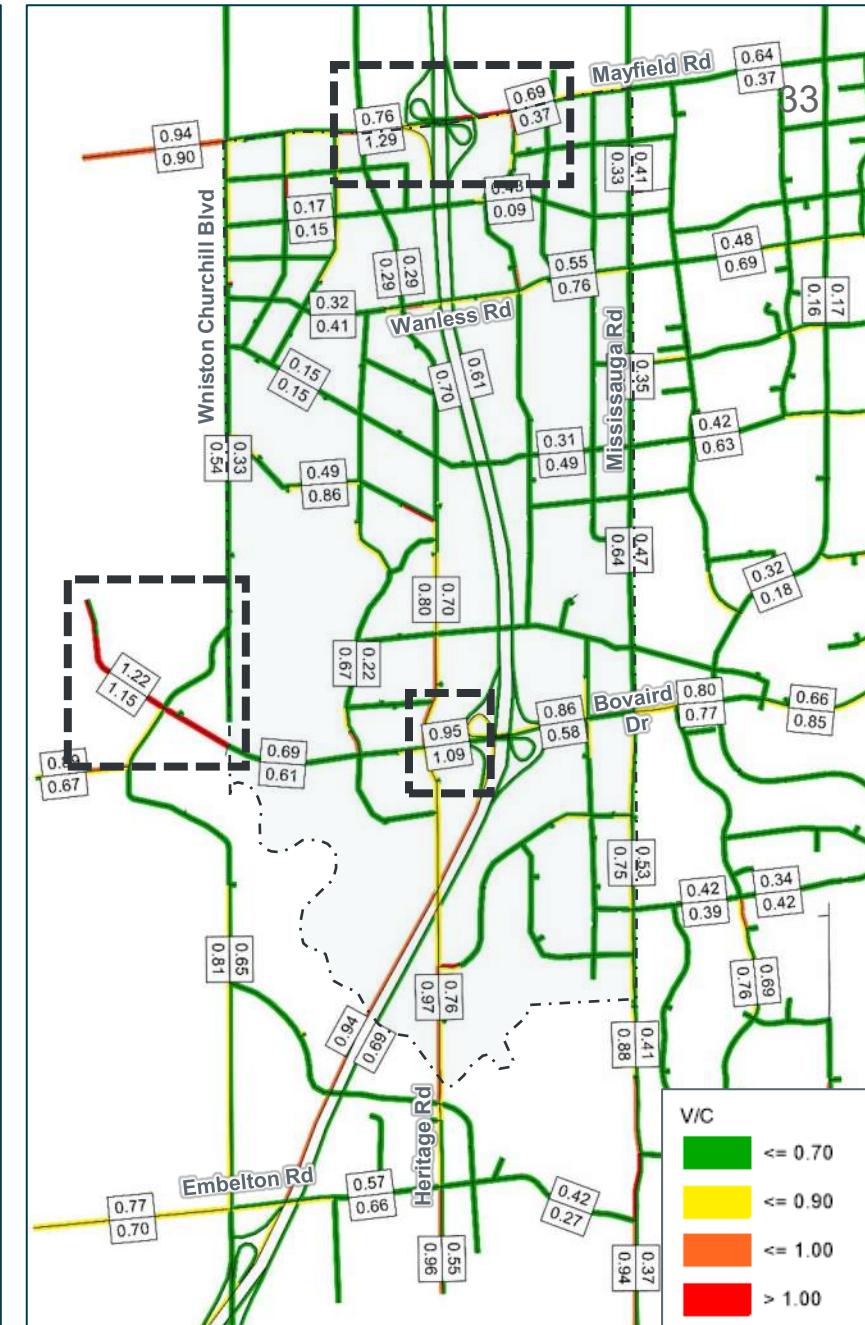
Forecasted traffic operations during the **weekday morning peak hour** in the Secondary Plan area are generally within acceptable modelling limits

Over-capacity road segments:

- Mayfield Road at Highway 413 (between Pinnacle Parkway and Heritage Road)
- Bovaird Drive between Highway 413 and Heritage Road
- Bovaird Drive/Highway 7 west of Halton-Peel Boundary



Future Weekday Morning Peak Hour - Corridor Traffic Volumes (per hour)



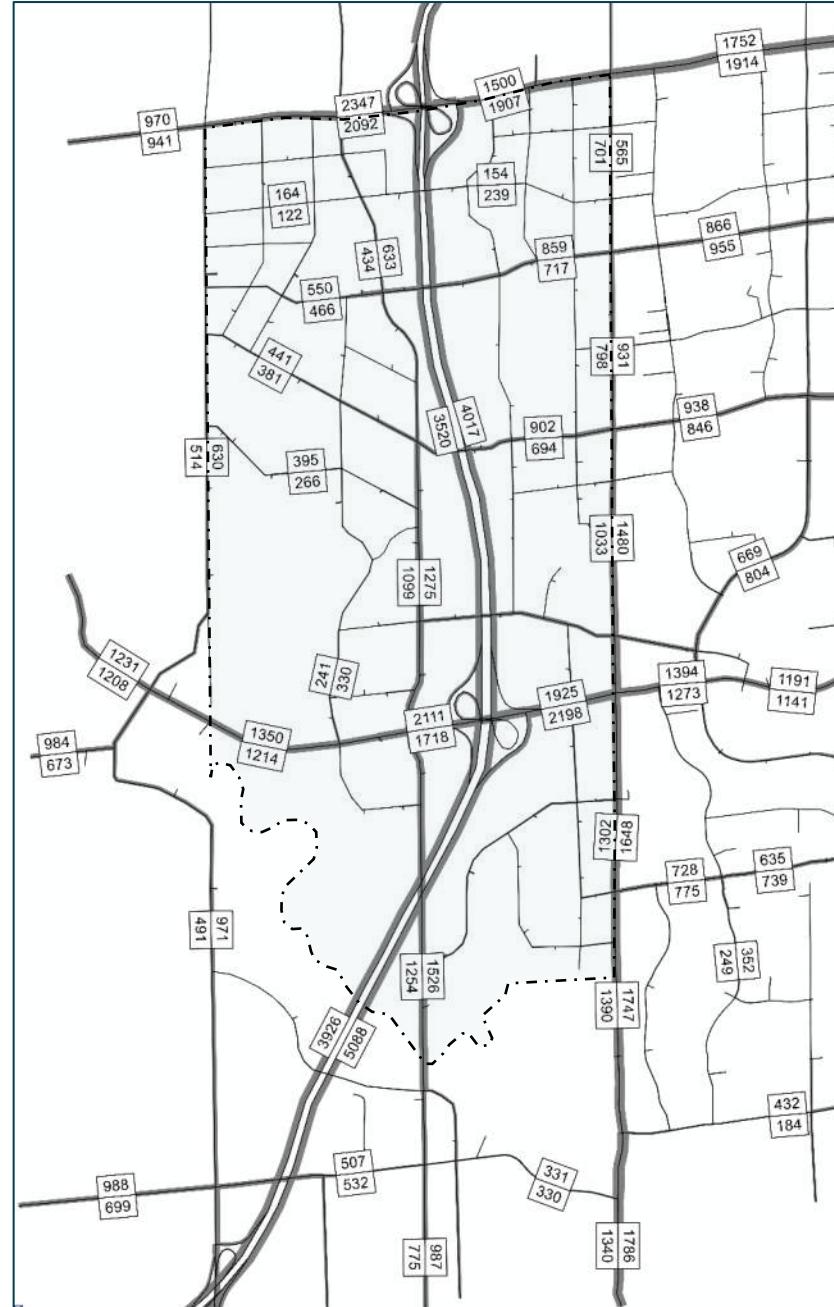
Future Weekday Morning Peak Hour - Corridor Volume-to-Capacity Ratios

Future Traffic Operations 2051, Afternoon Peak Hour

Forecasted traffic operations during the **weekday afternoon peak hour** in the Secondary Plan area are generally within acceptable modelling limits

Over-capacity road segments:

- Mayfield Road at Highway 413 (between Pinnacle Parkway and Heritage Road)
- Bovaird Drive between Highway 413 and Heritage Road
- Bovaird Drive/Highway 7 west of Halton-Peel Boundary

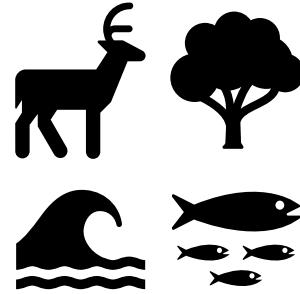


Future Weekday Afternoon Peak Hour - Corridor Traffic Volumes (per hour)

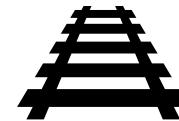


Future Weekday Afternoon Peak Hour - Corridor Volume-to-Capacity Ratios

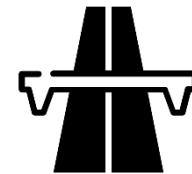
- There are three (3) types of major physical opportunities and constraints that influence changes in the Secondary Plan road network alignment:



**Crossing Natural Heritage
Systems: Watercourses,
Wetlands, Terrestrial Habitats**



**Crossing the Railway and
the TransCanada Pipeline**

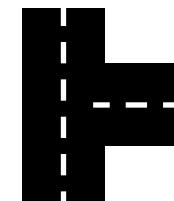


**Crossing Future
Highway 413**

- Alternatives to the Secondary Plan road network to address traffic capacity and environmental considerations were categorized as:



Road Widening



**Road Alignment
Alternatives**



**Refinements to Road
Crossings (of natural and
built environment features)**

Network Alternatives

Road Widenings

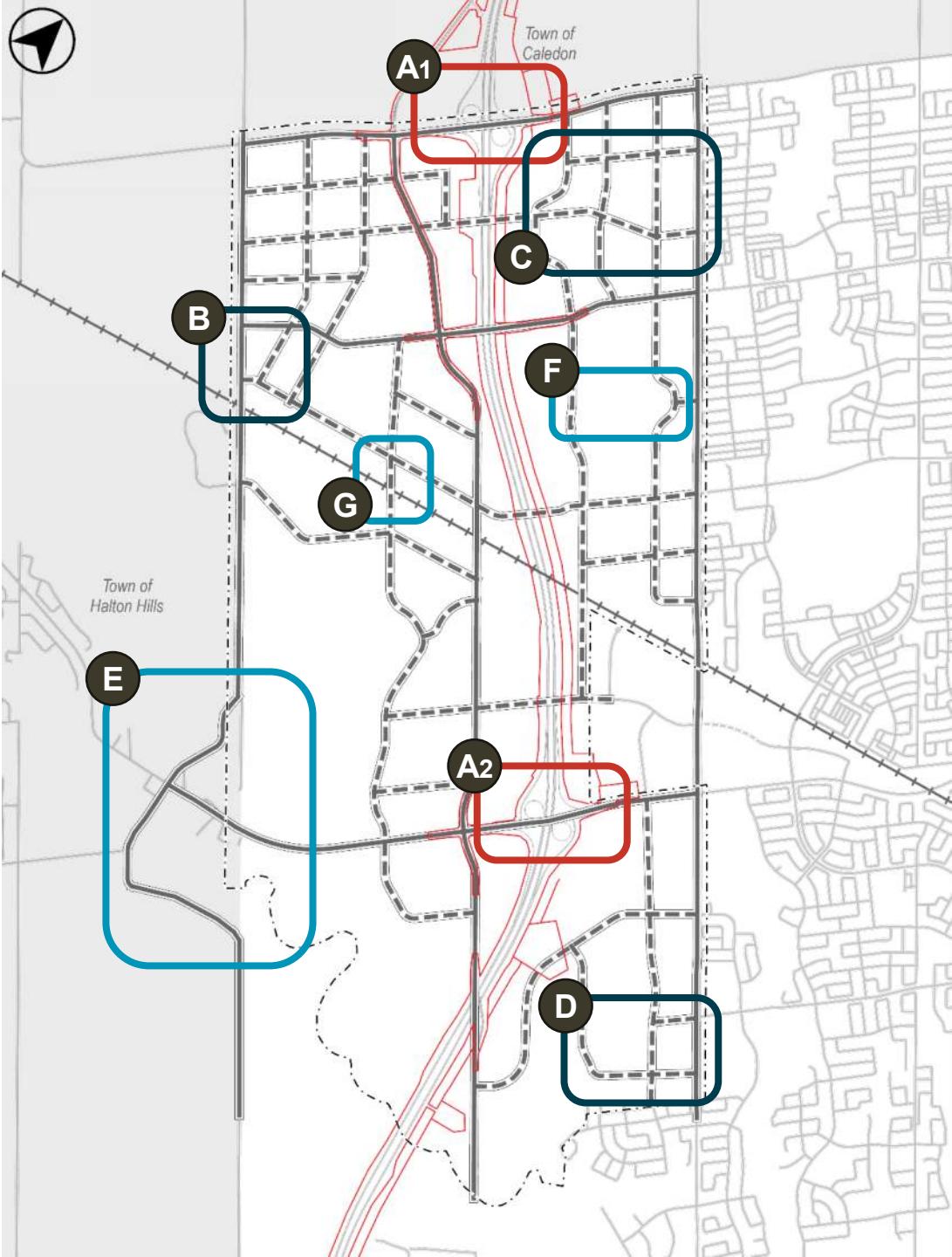
- A. Bovaird Drive & Mayfield Road Lane Widening

Road Alignments

- B. Sandalwood Parkway & Winston Churchill Boulevard (52-6 / 52-7)
- C. Precinct 52-3 Road Network
- D. Williams Parkway & Doubles Drive (52-1)

Crossings of Existing or Planned Constraints

- E. East-West Connection Focus Area (Inter-Regional)
- F. Buick Boulevard (52-2)
- G. North-South Rail Crossing (52-5 / 52-6)



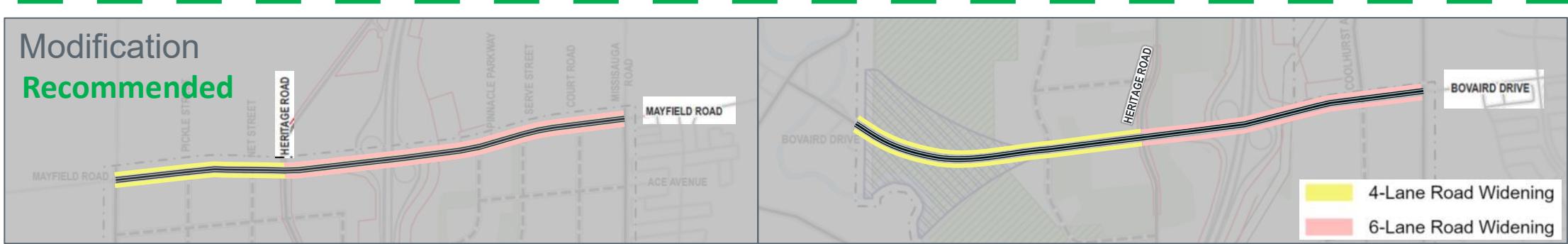
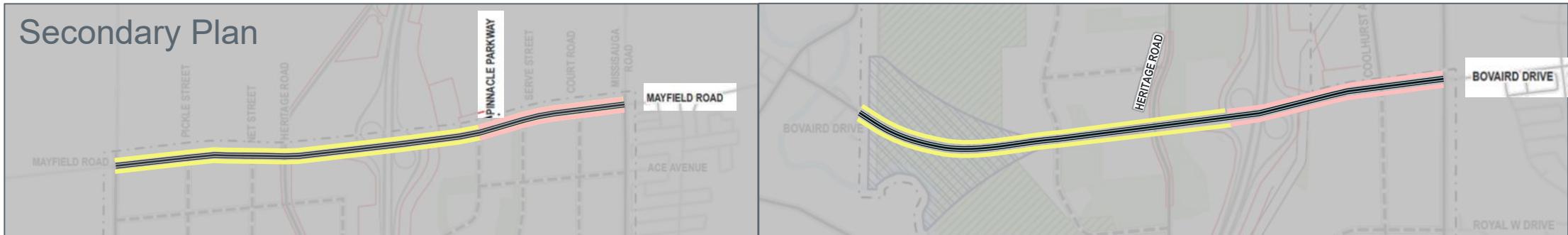
The **Long List** of transportation network modifications to the Secondary Plan has been reviewed and screened as:

- **Recommended:** Addresses Transportation Master Plan Problem Statement with similar or less impacts to the Secondary Plan transportation network, can be adopted and will not require further evaluation
- **Carried Forward for Further Evaluation:** Addresses the Transportation Master Plan Problem Statement but requires further Environmental Evaluation
- **Not Recommended:** Does not address the Transportation Master Plan Problem Statement or improve on the Secondary Plan transportation network

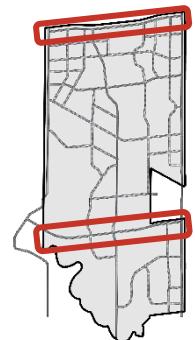
A **Short List** of transportation modifications screened as **Carried Forward for Further Evaluation** has been developed for evaluation through the Municipal Class Environmental Assessment process.

Long List Alternative A: Bovaird Drive and Mayfield Road (Regional Roads)

39

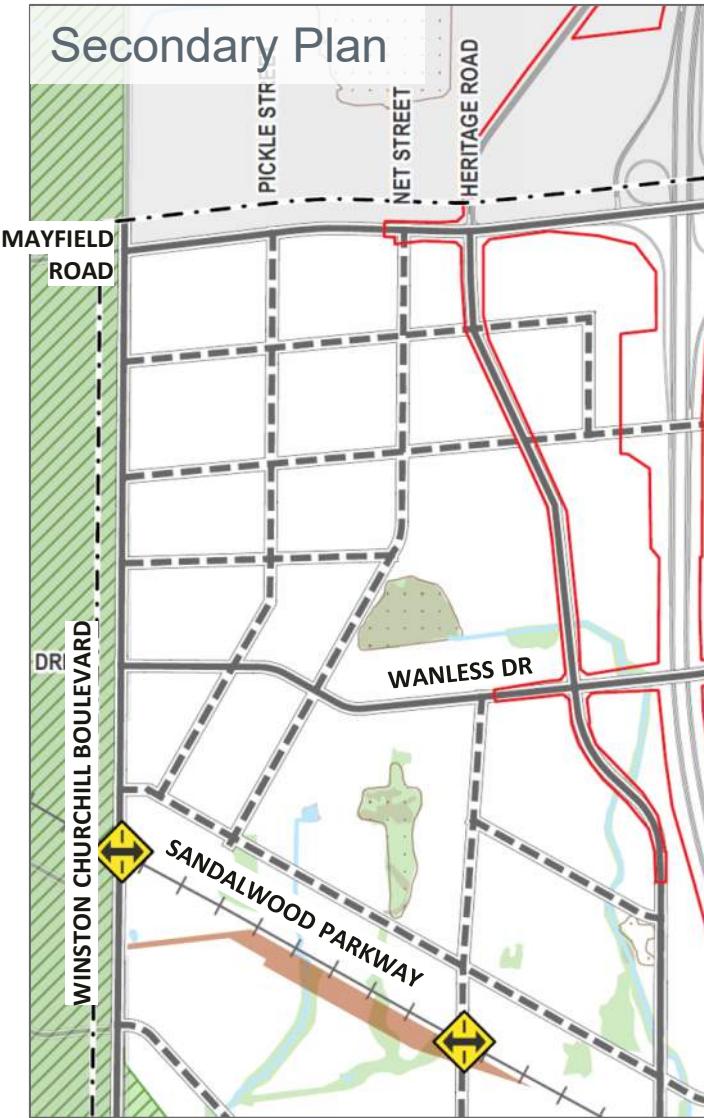


- Description of Modification:** Widenings to 6-lanes along Mayfield Road and Bovaird Drive extended westward to Heritage Road
- Description of Need/Impacts:**
Required to accommodate high east-west volumes travelling to/from to the Highway 413 interchanges. Widenings in this area are also under consideration by the Region's Long Range Transportation Plan, Completed, and Ongoing Municipal Class Environmental Assessment **Widening Recommended**

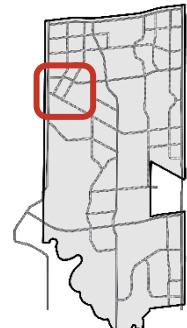


Long List Alternative B: Sandalwood Parkway & Winston Churchill Boulevard

40



- **Description of Modification:**
Re-alignment of the planned Sandalwood Parkway terminating at Wanless Drive.
- **Description of Need/Impacts:**
Grading, Fill, environmental impacts related to Sandalwood Parkway intersecting future elevated Winston Churchill Boulevard (CN Railway crossing).
- Negligible impact to traffic operations.
- Precinct 52-6 and 52-7 roads also aligned to normalized intersection angles and curvature.
- **Realignment Modification Recommended**



Long List Alternative C: Precinct 52-3 Road Network

41

Secondary Plan



Option 2 – Continuous NS Connection

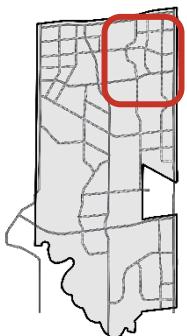


Option 3 – Continuous EW Connection

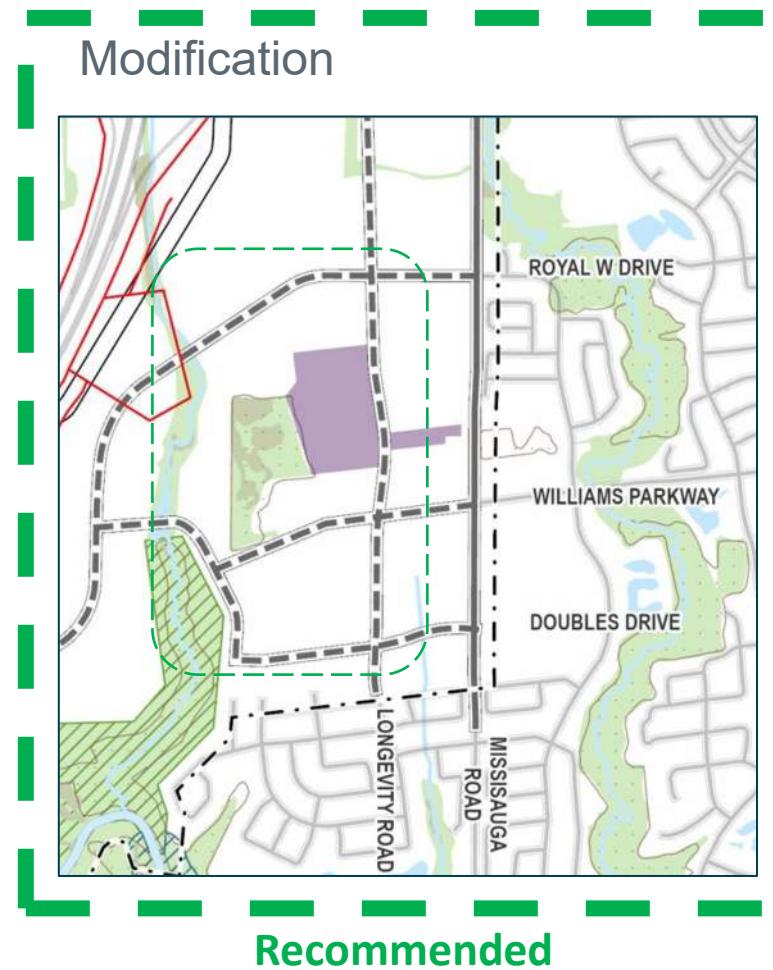


All Options
Carried
Forward for
Further
Evaluation

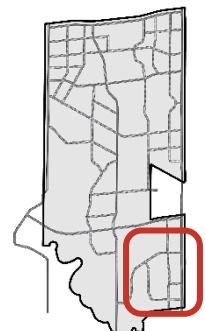
- Description of Modification:** Realignment of Nightjar Drive (Deuce Street), extension of Goderich Drive (Ace Avenue), and realignment of Pinnacle Parkway to avoid wetland features.
- Description of Need and/or Impacts:** Need to maintain Pinnacle Parkway and Court Road north-south connectivity to Mayfield Road and east-west employment connectivity across Highway 413. Continuous Grid Network compromised by realignment although environmental features are avoided.
- All Options Carried Forward for Further Evaluation** given natural heritage impacts and connectivity needs.



Secondary Plan

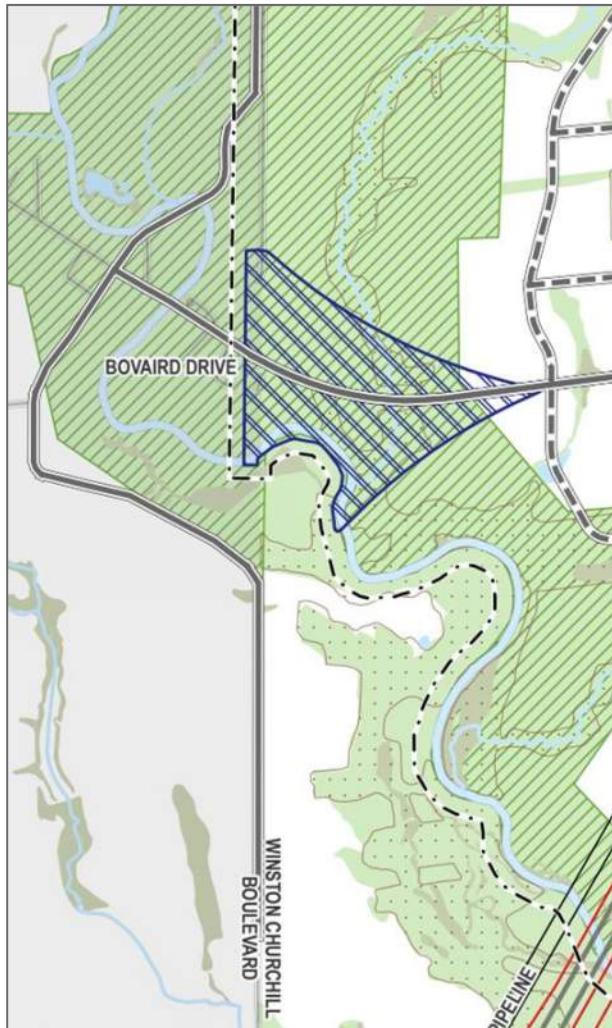


- **Description of Modification:**
Realignment of Doubles Drive and Williams Parkway reflecting emerging Precinct Planning
- **Description of Need/Impacts:**
Negligible impact to traffic operations. Increased connectivity to west Precinct.
- Similar environmental impacts to Secondary Plan road network.
Avoids road impacts next to woodland.
- **Realignment Modification Recommended**



Long List Alternative E: East-West Connection Focus Area (Inter-Regional)

43



Secondary Plan

Widening, By-Pass or Do Nothing Carried Forward for Further Evaluation

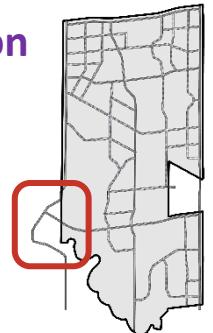
Table 7-21: East-West Options Evaluation

Indicators/Options	Option 1	Option 2	Option 3	Option 4
DO NOTHING – No additional East-West Connection	Not Recommended	New Link South of Norval from 10 Sideroad to Bovaird	New Link to from 10 Sideroad to Williams Parkway	New Link North of Norval to Bovaird
Transportation Service Impacts	Circle	Circle	Circle	Circle
Impacts on Natural Environment	Circle	Circle	Circle	Circle
Community Impacts	Circle	Circle	Circle	Circle
Economic Impacts	Circle	Circle	Circle	Circle
Construction Costs	Circle	Circle	Circle	Circle
Overall	Not Recommended	Carry Forward to EA Study	Not Recommended	Carry Forward to EA Study



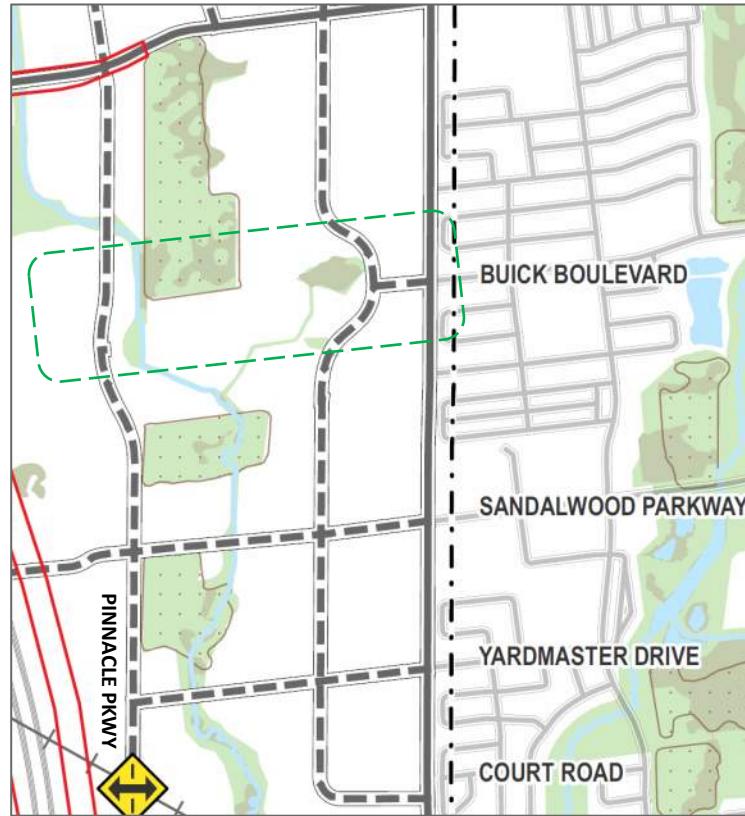
Source: 2010 HPBATS Study

- **Description of Modification:** Widening, By-Pass, or Do Nothing to address potential future traffic capacity constraints.
- **Description of Need/Impacts:** Modelling indicates capacity constraints in 2051 conditions. Modifications addressing capacity occur within Norval community and Greenbelt at inter-jurisdictional boundary
- **Widening, By-Pass, and Do Nothing Alternatives Carried Forward for Further Evaluation** given significant natural and cultural heritage impacts of potential alternatives

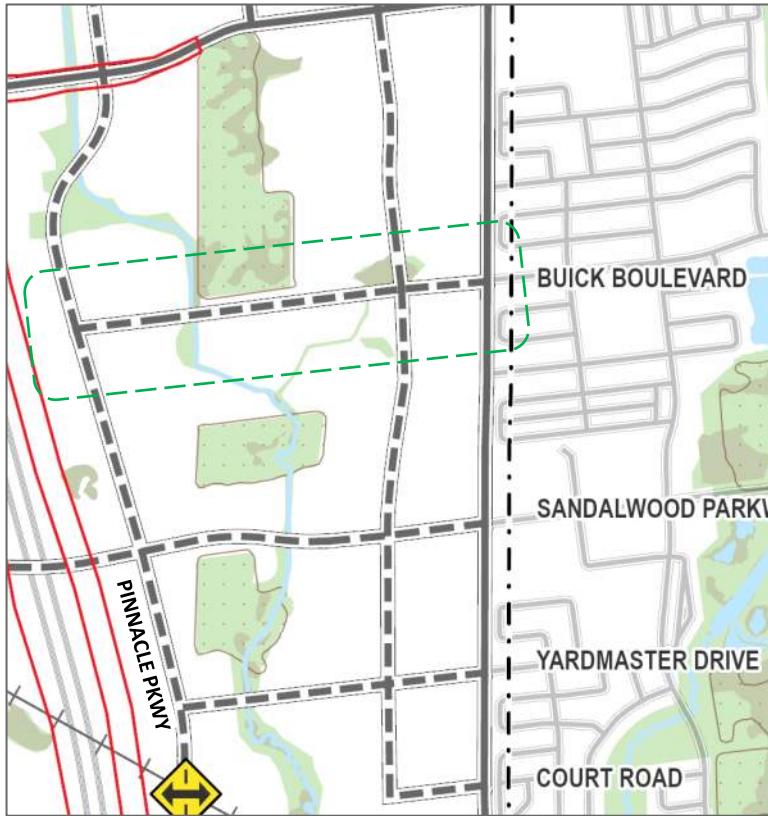


Long List Alternative F: Extension of Buick Boulevard (Crossing Natural Heritage System) 44

Secondary Plan

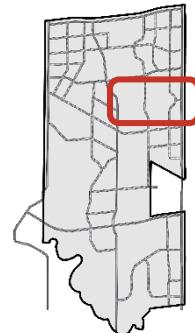


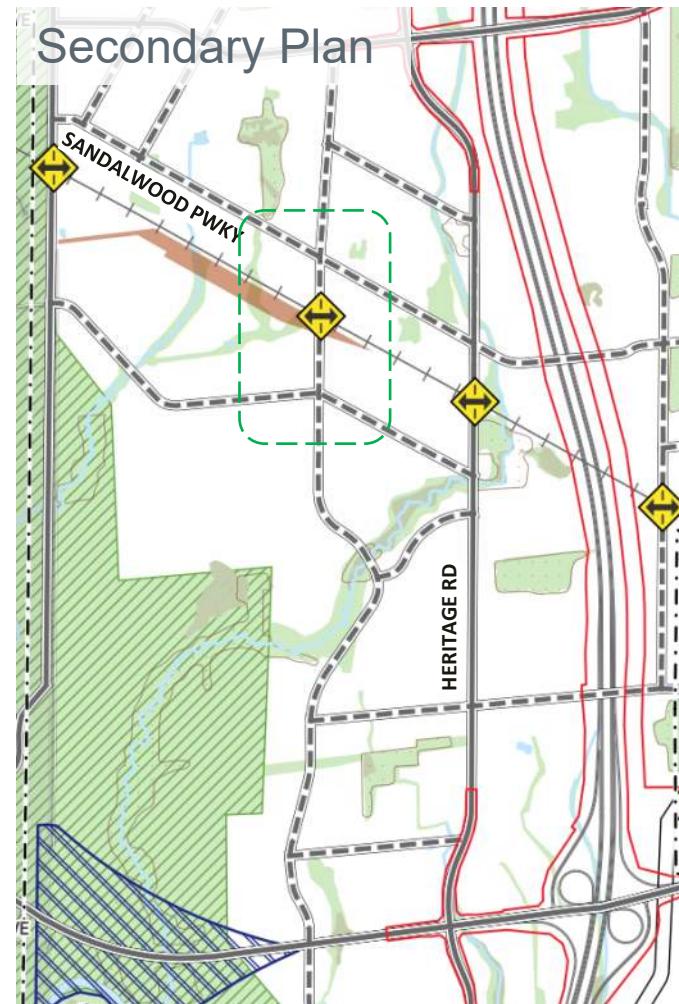
Modification



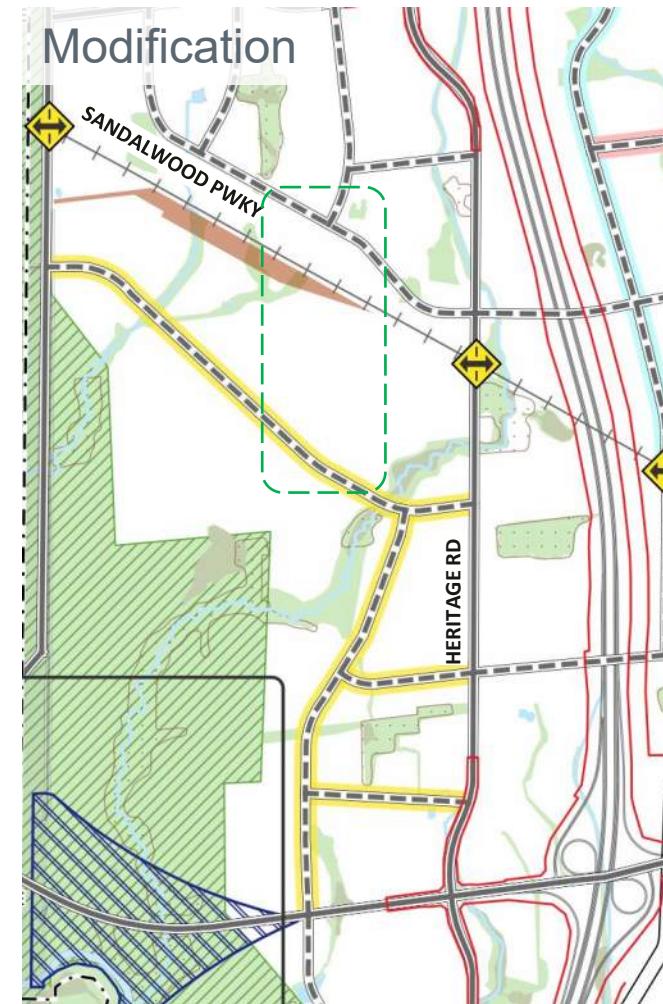
- **Description of Modification:** Extension of Buick Boulevard (Crossing Natural Heritage) to Pinnacle Parkway
- **Description of Need/Impacts:** Addresses fine-grained street connectivity.
 - Crosses North-South watercourse, wetland, woodland, wildlife habitat.
- **Both Options Carried Forward for Further Evaluation** given significant natural heritage impacts of potential alternatives.

Both Options **Carried Forward for Further Evaluation**

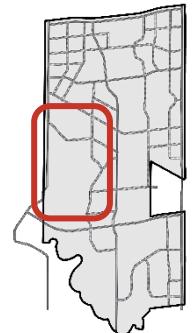




Both Options Carried Forward for Further Evaluation



- **Description of Modification:**
Re-evaluation of Secondary Plan CN railway crossing and related road alignments.
- **Description of Need/Impacts:**
Addresses fine-grained street connectivity between arterial crossings.
- Crosses Metrolinx Heritage Road Layover Facility, impacts grading, drainage, developable area, and existing Headwater Drainage Feature
- Both Options **Carried Forward for Further Evaluation** given natural features, development impacts, and Metrolinx Heritage Road Layover Facility.

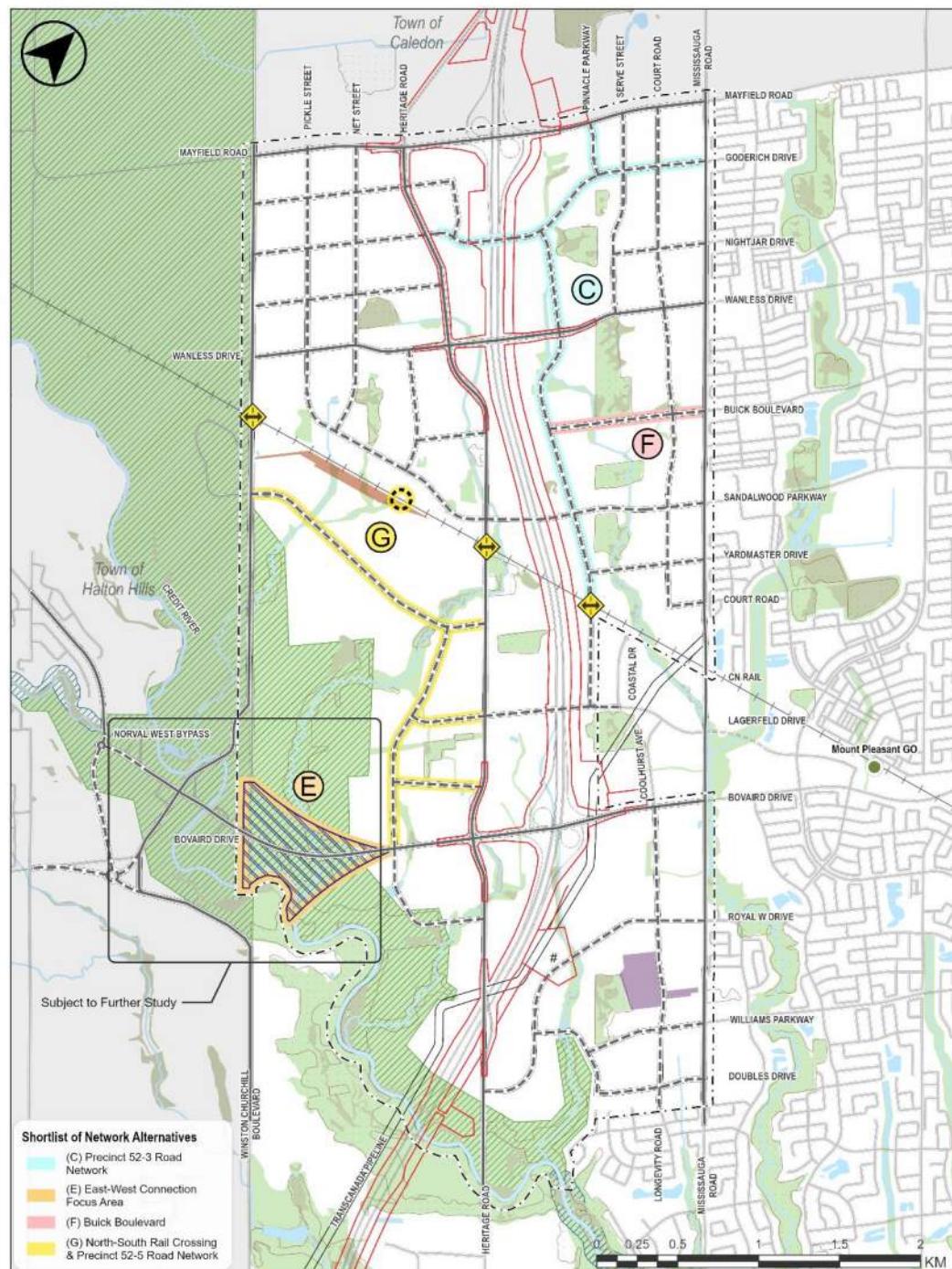


#	Long List Network Alternative	Type	Description	Assessment Result
A	Bovaird Drive & Mayfield Road Lane Widening (Regional Roads)	Road Widening	Widenings to 6-lanes along Mayfield Road and Bovaird Drive extended westward to Heritage Road	Recommended Adopted. Does not require further evaluation.
B	Sandalwood Pkwy & Winston Churchill Blvd (52-6, 52-7)	Road Alignment	Re-alignment of the planned Sandalwood Parkway terminating at Wanless Drive.	Recommended Adopted. Does not require further evaluation.
C	Precinct 52-3 Road Network	Road Alignment	Realignment of Nightjar Drive (Deuce Street), extension of Goderich Drive (Ace Avenue), and realignment of Pinnacle Parkway to avoid wetland features.	Carried Forward for Further Evaluation
D	Williams Extension / Doubles Drive (52-1)	Road Alignment	Realignment of Doubles Drive and Williams Parkway reflecting emerging Precinct Planning	Recommended Adopted. Does not require further evaluation.
E	East-West Connection Focus Area (Inter-Regional)	Crossings of Existing or Planned Constraints	Continuation of previously studied Norval Hamlet 'East-West Connection Focus Area' as identified in the 2010 Halton-Peel Boundary Area Transportation Study (HPBATS). Widening, By-Pass, or Do Nothing to address potential future traffic capacity constraints.	Carried Forward for Further Evaluation
F	Extension of Buick Boulevard (52-2)	Crossings of Existing or Planned Constraints	Extension of Buick Boulevard (Crossing Natural Heritage) to Pinnacle Parkway	Carried Forward for Further Evaluation
G	North-South Rail Crossing (52-5 / 52-6)	Crossings of Existing or Planned Constraints	Re-evaluation of Secondary Plan CN railway crossing and related road alignments.	Carried Forward for Further Evaluation

Short List Summary

Short List of Network Alternatives **Carried Forward for Further Evaluation :**

- ④ (C) Precinct 52-3 Road Network Realignment
- ④ (E) East-West Connection Focus Area (Inter-Regional Roads)
- ④ (F) Extension of Buick Boulevard (52-2)
- ④ (G) North-South Rail Crossing (52-5 / 52-6)



Transportation Criteria

- Enhance sustainable mobility and multi-modal travel options
- Address modelled traffic impacts
- Enhance connectivity for people and goods
- Integrate transportation and land use planning
- Protect public health and safety
- Leverage flexibility for future planning

Cultural Environment Criteria

- Archaeologic
- Cultural and Built Heritage

Natural Environment Criteria

- Terrestrial (species)
- Aquatic (watercourses, species at risk)
- Greenbelt & Natural Heritage Systems
- Stormwater & Surface Water (flooding, erosion)

Socio-Economic Criteria

- Transportation Equity
- Property Impacts/Requirements
- Nuisance Impacts (Noise, Vibration & Air Quality)
- Ease of Construction (Phasing, Geotechnical)
- Cost (Capital, Operating & Maintenance)

Short List Evaluation - Sample

- Each Short List Alternative was reviewed in detail based on the Environmental Assessment Criteria for Evaluation
- Evaluation of Transportation, Cultural Environment, Natural Environment, and Socio-Economic criteria determined an overall Recommendation for each Short List Alternative

Least Preferred

More/Less Preferred

Most Preferred

Evaluation Sample

Category	Measure	Option 1 – Do Nothing	Transportation Criteria			
			Option 2 – North Bypass	Option 3 – South Bypass	Option 4 – Widen Highway 7	
Enhance sustainable mobility and multi-modal travel options	Enhances public transit connectivity		Provides dedicated additional transit capacity in addition to maintaining Highway 7 through Norval.	Provides dedicated additional transit capacity in addition to maintaining Highway 7 through Norval. Not as effective as other options in addressing routing options of desired routes.	Provides transit capacity in mixed traffic with widening of Highway 7 through Norval.	
	Enhances Active transportation connectivity and Micromobility options		Provides dedicated additional active transportation routes in addition to maintaining Highway 7 through Norval.	Provides dedicated additional active transportation routes in addition to maintaining Highway 7 through Norval. Not as effective as other options in addressing routing options of desired routes.	May provide opportunity for dedicated active transportation with widening of Highway 7 through Norval.	
Traffic Impacts	Traffic operations & capacity impacts	Does not support the modelled traffic activity and EA Problem Statement	Provides the most effective relief of Norval traffic destined between Georgetown and Brampton.	Provides relief of Norval traffic destined between Georgetown and Brampton. Not as effective as other options in addressing capacity constraints of desired routes.	Traffic modelling shows the greatest operational improvements. However, traffic demand through Norval is induced with greater lanes.	
	Enhance connectivity for people and goods	Enhances General travel routing	Provides the most effective relief of Norval traffic destined between Georgetown and Brampton.	Provides relief of Norval traffic destined between Georgetown and Brampton. Not as effective as other options in addressing routing options of desired routes.	Traffic modelling shows the greatest operational improvements. However, traffic demand through Norval is induced with greater lanes.	
	Freight routing impacts		Provides dedicated goods movement corridor in addition to maintaining Highway 7 through Norval.	Provides dedicated goods movement corridor in addition to maintaining Highway 7 through Norval. Less direct than Option 2.	Provides goods movement in mixed traffic with widening of Highway 7 through Norval.	
Integrate transportation and land use planning	Compatibility with existing and proposed land uses.		Impact to existing Greenbelt and existing landowners north of Highway 7 / Bovaird Drive. (e.g. Old Pine Crest Road). Acts as a bypass of land uses in future, e.g. no interaction with future land uses.	Impact to existing Greenbelt and existing landowners south of Highway 7 / Bovaird Drive. Acts as a bypass of land uses in future, e.g. no interaction with future land uses.	Potential to impact existing land uses through Norval. Widening interacts with adjacent existing and future land uses.	
Protect public health and safety	Prioritizes vulnerable road users		Provides dedicated additional active transportation routes in addition to maintaining Highway 7 through Norval. Opportunities for enhanced pedestrian crossings.	Provides dedicated additional active transportation routes in addition to maintaining Highway 7 through Norval. Opportunities for enhanced pedestrian crossings. Limited opportunities for improvement for desired routes compared to other options.	Illustrative comparison Further detailed study recommended.	
	Promotes active living		Provides dedicated additional active transportation routes in addition to maintaining Highway 7 through Norval.	Provides dedicated additional active transportation routes in addition to maintaining Highway 7 through Norval. Limited opportunities for improvement for desired routes compared to other options.	May provide opportunity for dedicated active transportation with widening of Highway 7 through Norval. Opportunities for enhanced pedestrian crossings. Induced traffic demand with widening.	
Leverage flexibility for future planning	Protects for long range vision of Secondary Plan and flexible land use planning in adjacent Precincts		Provides options for routing that can respond to several land use scenarios.	Provides options for routing that can respond to several land use scenarios. Not as effective as other options in addressing routing options of desired routes.	May provide opportunity for dedicated active transportation with widening of Highway 7 through Norval. More direct than Option 3.	
Transportation Overall	Screened Out		Adjacent Precincts protect for connection	Adjacent Precincts protect for connection.	Options for widening would be closely tied to impacts/interface with existing Norval, while also providing new capacity, limiting flexibility.	
			Most Preferred	Less Preferred	Adjacent Precincts protect for connection.	More Preferred

Short List C: Precinct 52-3 Road Network

50



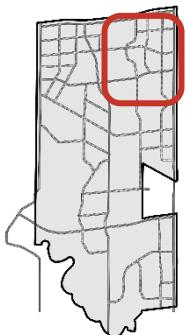
Secondary Plan



Option 2:
Continuous North-South
Connection



Option 3:
Continuous East-West
Connection

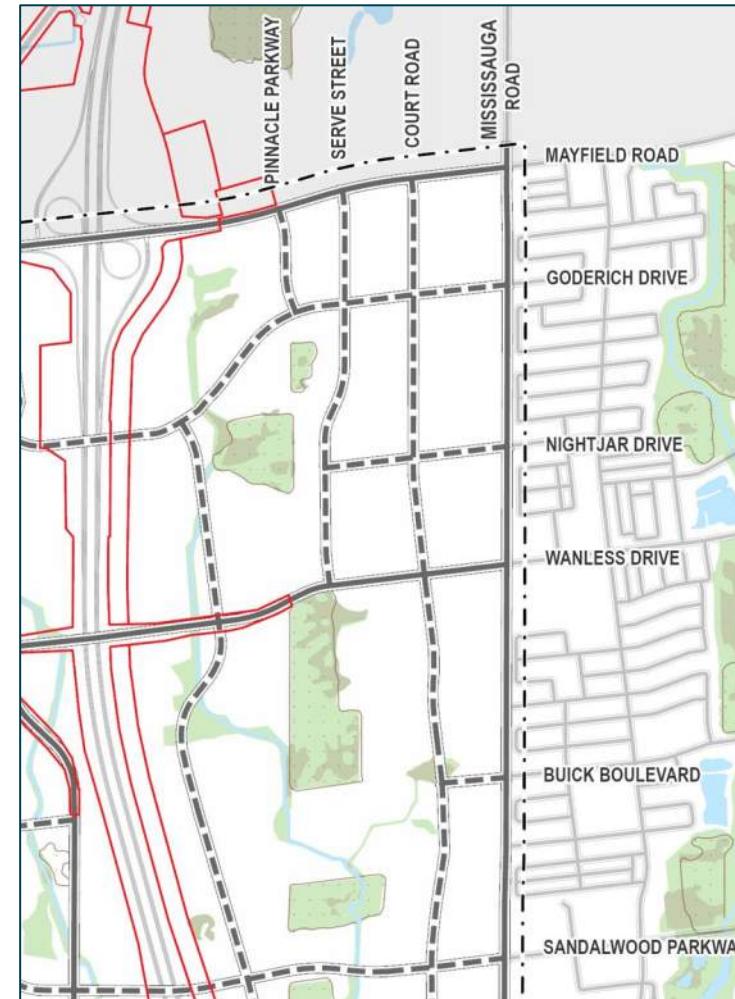


Precinct 52-3 Road Network

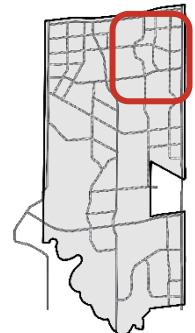
Categories	Option 1 – Secondary Plan Road Network	Option 2 – Continuous North-South Connection	Option 3 – Continuous East-West Connection
Transportation	<p>Provides the finest-grained connectivity for vehicular capacity, routing options, and transit connectivity.</p> <p>Provides two continuous north-south to/from the future highway and provincial transitway and one continuous east-west collector between employment areas.</p>	Maintains N-S transit connectivity. Introduces additional intersections along Goderich that breaks up continuity of east-west collector between employment areas.	<p>Maintains E-W transit connectivity. Introduces additional intersections along Pinnacle Parkway that breaks up continuity of north-south collector to/from the future highway and provincial transitway.</p> <p>Provides a continuous parallel traffic and transit relief corridor to Mayfield Road and Wanless Drive.</p>
Cultural Environment	Cultural Heritage Lands 2 and 4 (farmscape with brick farmhouses) crossed by future collector roads. To be mitigated/managed through detailed design.	Cultural Heritage Lands 2 and 4 (farmscape with brick farmhouse) crossed by future collector roads. To be mitigated/managed through detailed design.	Cultural Heritage Lands 2 and 4 (farmscape with brick farmhouse) crossed by future collector roads. To be mitigated/managed through detailed design.
Natural Environment	Pinnacle Parkway and Nightjar Drive extension crosses through wetland, woodlot, and Natural Heritage System that occupies a large portion of the west side of Precinct 52-3.	Eliminates E-W collector road going through wetland features and related buffer. No change to number of watercourse crossings compared with other options.	Eliminates E-W collector road going through wetland features and related buffer. No change to number of watercourse crossings compared with other options.
Socio-Economic	<p>Provides the finest grained connectivity for people by all modes.</p> <p>Geotechnical impacts related to crossing wetland, woodlot and Natural Heritage feature in Precinct 52-3.</p> <p>Most land acquisition compared to other options.</p>	<p>Greater vehicular activity on parallel corridors (Mayfield Road)</p> <p>Geotechnical impacts related to diversion of watercourse in Precinct 52-2W to match watercourse crossing to Precinct 52-3. No change to number of watercourse crossings compared with other options.</p>	<p>Greater vehicular activity on parallel corridors (Mayfield Road) mitigated by continuous east-west connection between Precinct 52-3 and 52-7 employment areas.</p> <p>Least land acquisition compared to other options.</p> <p>Connects east and west employment</p> <p>Geotechnical impacts related to diversion of watercourse in Precinct 52-2W to match watercourse crossing to Precinct 52-3. No change to number of watercourse crossings compared with other options</p>
Overall			Recommended

Recommendation:

- Maintain a continuous east-west crossing of future Highway 413 (Goderich Drive) between employment areas in Precincts 52-3 and 52-7.
- Maintain north-south collector road connectivity to Mayfield Road, serving Precincts 52-2 and 52-3 to/from future Highway 413 and Provincial Transitway while avoiding bisecting wetland and woodland feature.

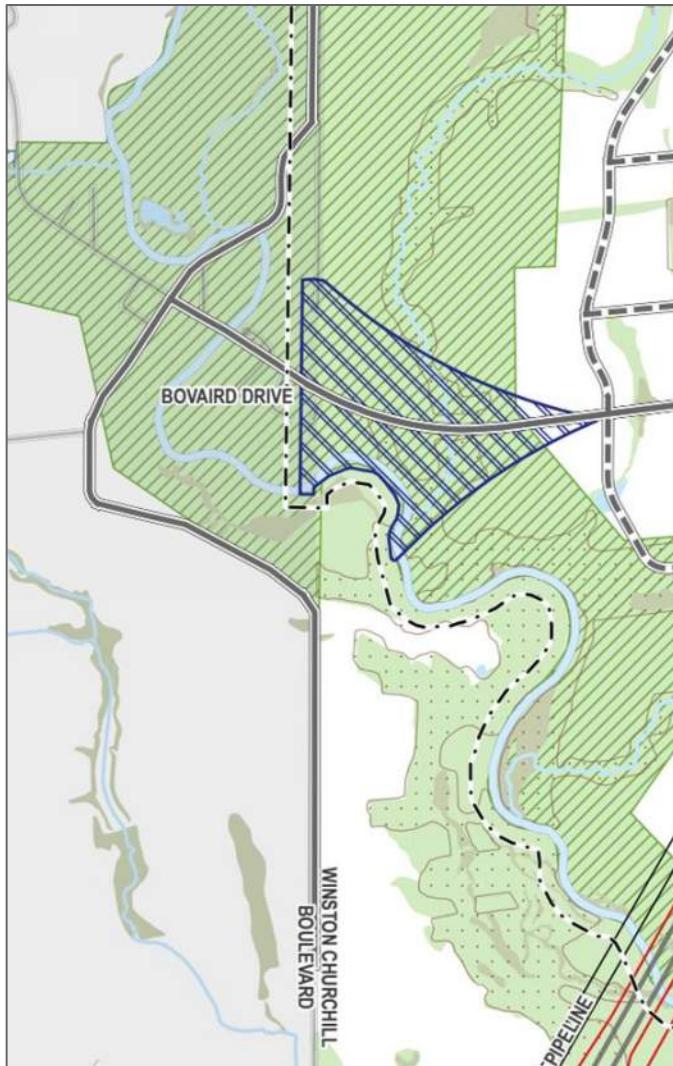


Option 3:
Continuous East-West
Connection

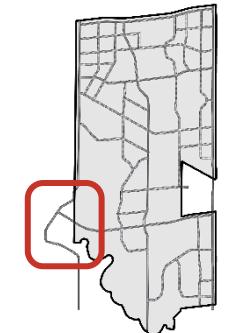
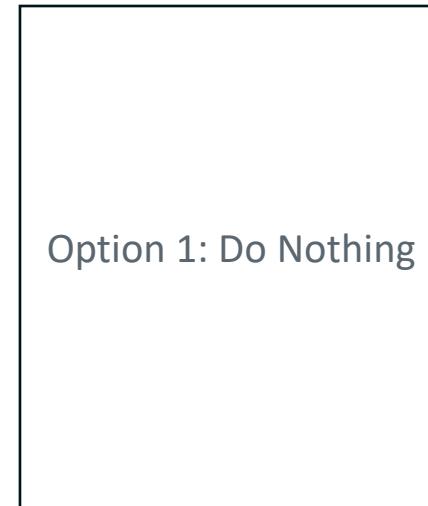


Short List E: East-West Connection Focus Area (Inter-Regional)

53



East-West Connection Focus Area



East-West Connection Focus Area (Inter-Regional) – Evaluation of Alternative Solutions Cont'd

54

Categories	Option 1 – Do Nothing	Option 2 – North Bypass	Option 3 – South Bypass	Option 4 – Widen Highway 7
Transportation	Screened Out Does not support the modelled traffic activity and EA Problem Statement	Provides dedicated facility for all modes, goods, and land uses.	Less direct and less effective traffic relief than North Bypass and Widening Options.	Relies on providing future capacity for all modes in mixed traffic through widening current Highway 7.
Cultural Environment	-	Avoids potential impacts to Cultural Heritage in Norval. Potential for archaeological impacts in Credit River Valley. <i>Further study required as part of future Class EA*</i>	Avoids potential impacts to Cultural Heritage in Norval. Potential for archaeological impacts in Credit River Valley. <i>Further study required as part of future Class EA*</i>	Impacts known Cultural Heritage in Norval. Potential for archaeological impacts in Norval. <i>Further study required as part of future Class EA*</i>
Natural Environment	-	Entirely within Greenbelt limits. High potential for impacts to the Credit Valley <i>Further study required as part of future Class EA*</i>	Entirely within Greenbelt limits. High potential for impacts to the Credit Valley <i>Further study required as part of future Class EA*</i>	Avoids impacting undisturbed areas of the Credit River Valley. <i>Further study required as part of future Class EA*</i>
Socio-Economic	-	Diverts traffic and related noise/air quality impacts to new route. High cost and geotechnical impacts associated with unique Credit River Valley crossing. <i>Further study required as part of future Class EA*</i>	Provides less effective diversion than North Bypass. High cost and geotechnical impacts associated with unique Credit River Valley crossing. <i>Further study required as part of future Class EA*</i>	Impacts Cultural Heritage properties in Norval. Directs traffic and related noise/air quality through Norval. Costs related to widening within existing Norval. Potential for moving, impacting or restoring existing land uses. <i>Further study required as part of future Class EA*</i>
Overall	Screened Out	No Recommendation – Further Study Required	No Recommendation – Further Study Required	No Recommendation – Further Study Required

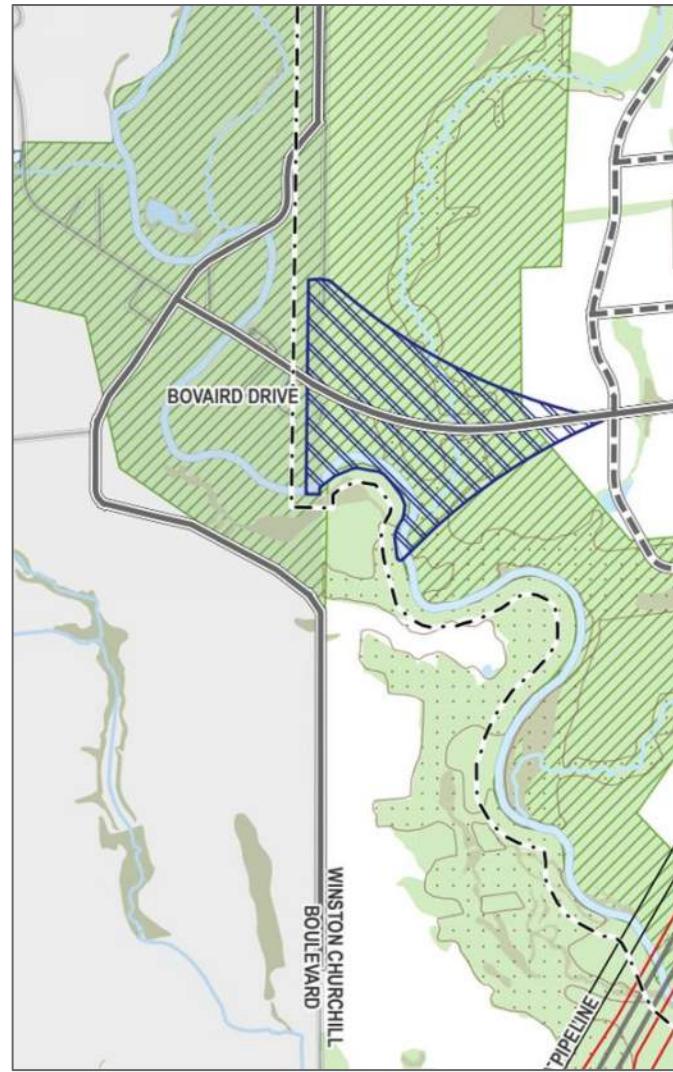
*Potential Cultural Environment, Natural Environment, and Socio-Economic Environment impacts reach outside of Secondary Plan Limits study area. EA evaluation shown for illustrative purposes.

Short List E: East-West Connection Focus Area (Inter-Regional)

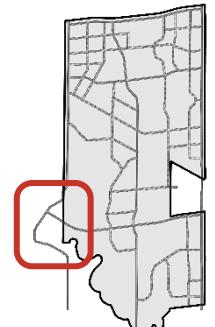
55

Recommendation:

- Consistent with the findings of 2010 HPBATS study, future modelled traffic re-confirms capacity constraints are recommended to be addressed through Norval.
- Further detailed study within Norval and the Credit Valley (within the Greenbelt) and west of the Secondary Plan limits is required to fully assess Cultural Environment, Natural Environment, and Socio-Economic Environment criteria to determine a preferred solution.
- The East-West Connection Focus Area is recommended to be protected for to allow a future Schedule C EA to determine a preferred solution.

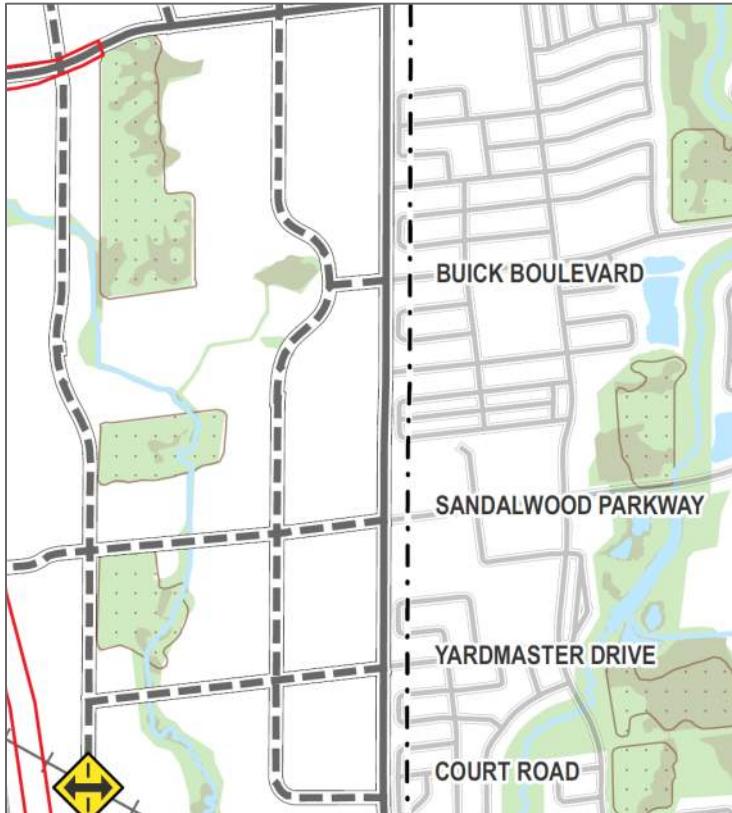


East-West Connection Focus Area



Short List F: Extension of Buick Boulevard (52-2)

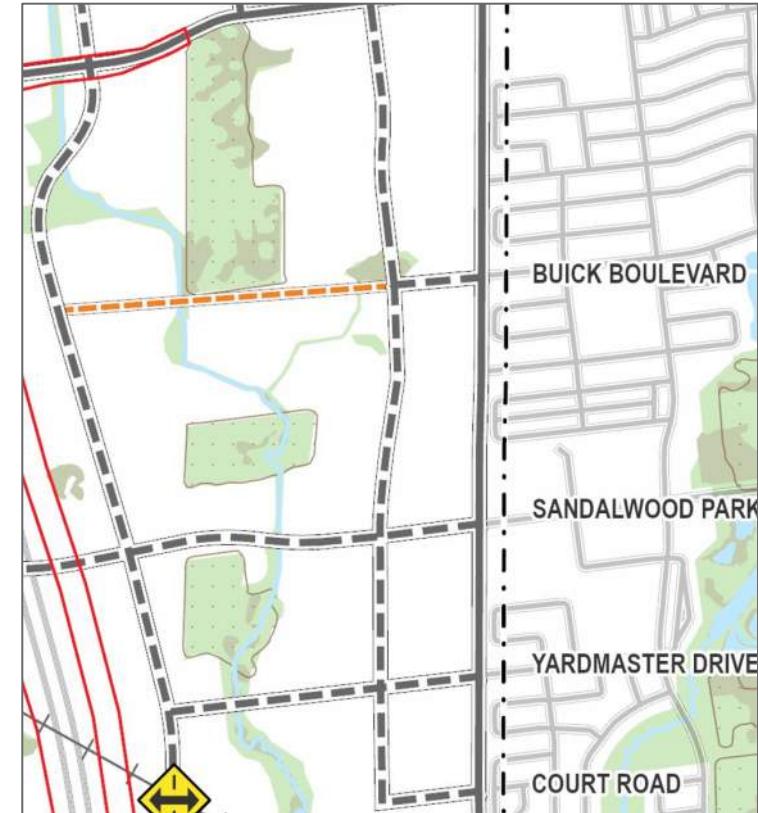
56



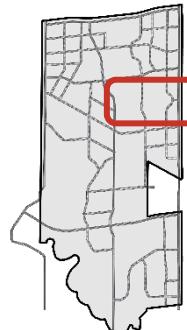
Option 1:
Secondary Plan Road Network
(Sandalwood + Yardmaster)



Option 2:
Secondary Plan + Buick



Option 3:
Secondary Plan +
Buick Active Transportation Only



Extension of Buick Boulevard (52-2)

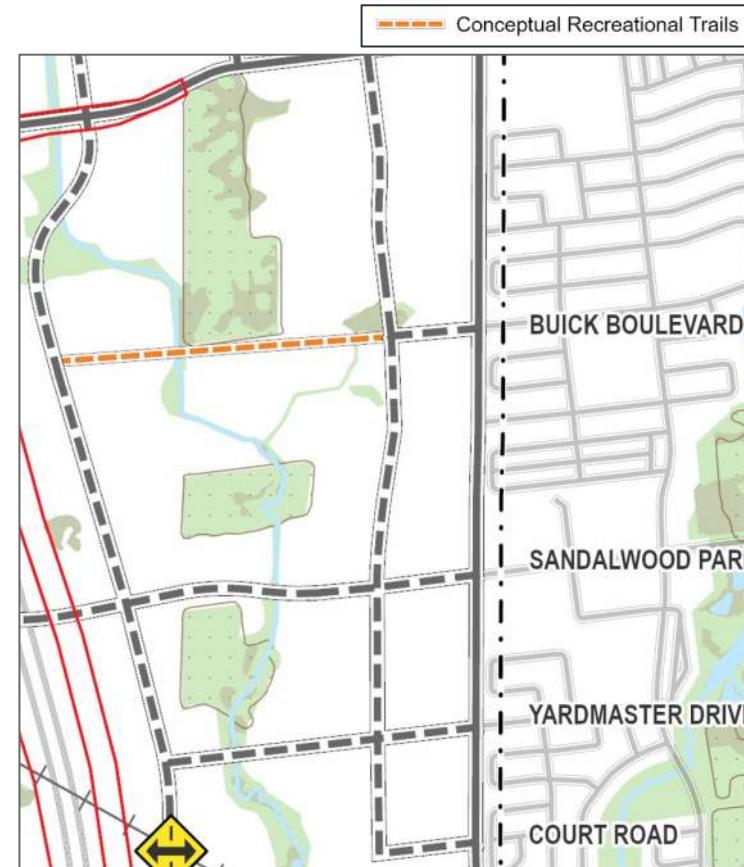
Categories	Option 1 – Secondary Plan Road Network (Sandalwood + Yardmaster)	Option 2 – Secondary Plan + Extension of Buick Boulevard	Option 3 – Secondary Plan + Buick Boulevard Active Transportation Only
Transportation	Less local east-west transportation options for all modes compared to other options.	Provides the finest grain connectivity for collector (transit-friendly) road network	Less east-west transit connectivity compared to Option 2. Provides fine-grained connectivity for walking and cycling.
Cultural Environment	Cultural and Built Heritage areas not identified within study area. Archaeology clearance to be assessed during detailed design.	Cultural and Built Heritage areas not identified within study area. Archaeology clearance to be assessed during detailed design.	Cultural and Built Heritage areas not identified within study area. Archaeology clearance to be assessed during detailed design.
Natural Environment	Avoids natural feature impacts related to extending Buick Boulevard.	Buick Boulevard would directly or indirectly impact: <ul style="list-style-type: none"> watercourse providing seasonal fish habitat and Terrestrial Crayfish Significant Wildlife Habitat. Provincially Significant Wetland (22) Significant woodland, significant wildlife, and species at risk habitat, North-south ecological linkage facilitating wildlife movement 	Avoids major natural feature impacts related to extending Buick Boulevard. <p>Opportunities to explore a variety of designs and crossing locations for active transportation that may avoid or mitigate impacts to property and Natural Heritage System features.</p>
Socio-Economic	Least kilometres and property impacts with some diversion of traffic on parallel corridors.	Would require the greatest overall property and cost to deliver road network, including three natural heritage bridge crossings in Precinct 52-2	Has the same kilometres of road network as Option 1, plus an active transportation corridor that advances transportation equity. Opportunities to explore a variety of designs and crossing locations for active transportation to mitigate both natural impacts and cost.
Overall			Recommended

Short List F: Extension of Buick Boulevard (52-2)

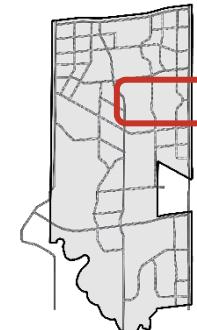
58

Recommendation:

- An active transportation crossing only along the Buick Boulevard alignment between the east and west sides of Precinct 52-2
- Provides fine-grained connectivity for walking and cycling between the east and west sides of Precinct 52-2
- Opportunities to explore a variety of designs and crossing locations for active transportation to mitigate both natural impacts and cost.



Option 3:
Sandalwood + Yardmaster +
Buick Active Transportation Only

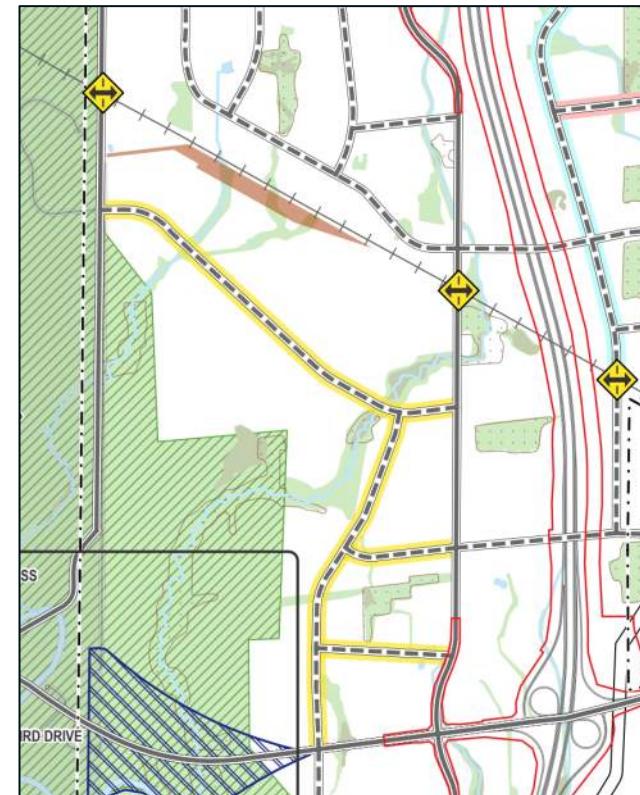


Short List G: North-South Rail Crossing (52-5 / 52-6) & Precinct 52-5 Road Network

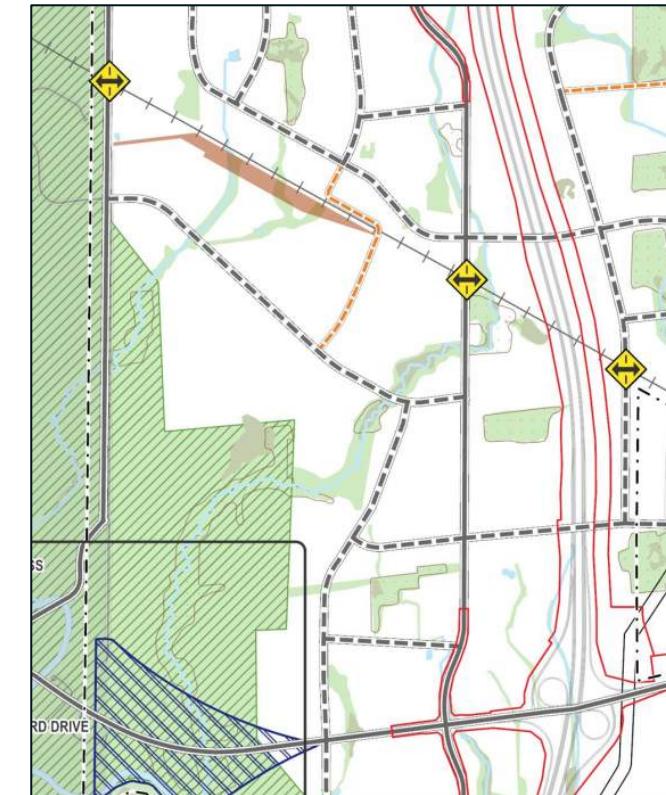
59



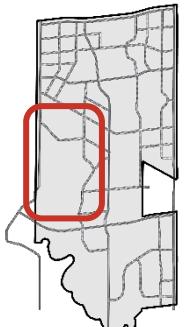
Option 1:
Secondary Plan Road Rail Crossing



Option 2:
Remove Rail Crossing



Option 3:
Active Transportation Rail
Crossing Only



North-South Rail Crossing (52-5 / 52-6) & Precinct 52-5 Road Network

60

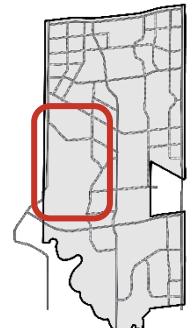
Categories	Option 1 – Secondary Plan Road Network	Option 2 – Road-Rail Crossing Removed	Option 3 – Active Transportation Rail Crossing Only
Transportation	Provides the finest grained connectivity for people by all modes.	Loss of local north-south routing options across all modes between Precinct 52-5 and Precinct 52-6.	Walking and cycling routing connectivity maintained for neighbourhood connectivity between Precinct 52-5 and Precinct 52-6.
Cultural Environment	Overpass/Underpass may impact McNichol's cemetery. Other archaeology findings may require mitigation/modification in coordination with road design.	No road over-under rail results in least risk of directly impacting existing Cultural Environment.	Smaller scale bridge affecting less surrounding area. Opportunities to explore a variety of designs and crossing locations.
Natural Environment	Impacts to Natural Heritage System with overpass/underpass. Headwater Drainage Feature with a floodplain near crossing location Low Point (underpass) would require pumping and other measures posing a challenge to mitigate for stormwater. Overpass is challenged by grading that has considerable impacts to future land uses.	Eliminates impacts from and costs related to building an overpass.	Smaller scale bridge affecting less of surrounding area. Opportunities to explore a variety of designs and crossing locations that may avoid or mitigate impacts to Natural Heritage System and grading/stormwater impacts.
Socio-Economic	Requires crossing of the Metrolinx Layover Facility. Grade separation would be required. Overpass restricts opportunities for local access and development areas in Precinct 52-5 and Precinct 52-6. Underpass would mitigate overpass impacts, with increased complexity/cost and would require groundwater pumping (e.g. low-point). Complex structure and high cost of options.	Greater vehicular activity on parallel corridors (Heritage Road and Winston Churchill Boulevard). Eliminates impacts from and costs related to building an overpass.	Improved walking and cycling experience between Precinct 52-5 and Precinct 52-6 compared to no bridge. Opportunities to explore a variety of designs and crossing locations that may mitigate phasing and geotechnical impacts.
Overall			Recommended

Recommendation:

- An active transportation crossing only crossing the railway between Precinct 52-5 and 52-6
- Provides fine-grained connectivity for walking and cycling between the north and south sides of the railway
- Align collector road network in Precinct 52-5 recognizing through corridor between Winston Churchill Boulevard and Heritage Road
- Potential 4-lane east-west collector in Precinct 52-5 to be refined as part of the Precinct Plan Transportation Study



Option 3:
Active Transportation Rail
Crossing Only



- The Preliminary Preferred Transportation Network includes four schedules:
 - Alignments and Widening
 - Road Classifications
 - Transit Network
 - Active Transportation Network

The **Transit** and **Active Transportation** networks fully integrate with the street network to support a complete street approach.

- The project team collaborated closely with Brampton Transit and Dillon Consulting to identify rapid transit and support corridors in the Heritage Heights area.
- These corridors will both help move people within Heritage Heights and provide connections to other rapid transit routes in Brampton, to GO Bus routes, to the Mount Pleasant GO Station, and to the proposed Highway 413 Transitway stations.
- Local transit routes, to be identified during precinct planning, will provide neighbourhood-level service in the Heritage Heights area.
- To support the proposed transit service and the development of complete streets, all arterial and collector roads will be designed to be transit-friendly.



Active Transportation

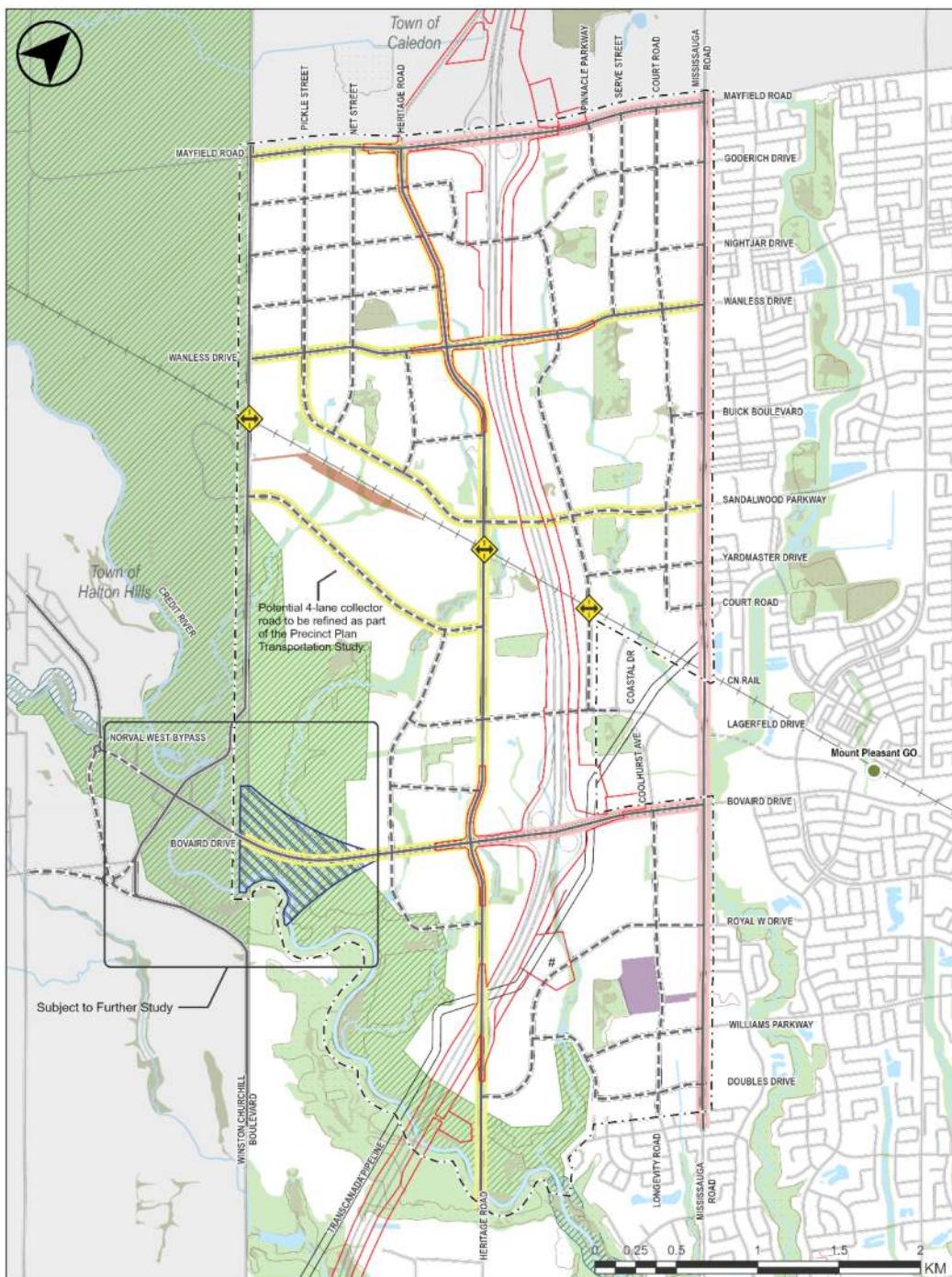
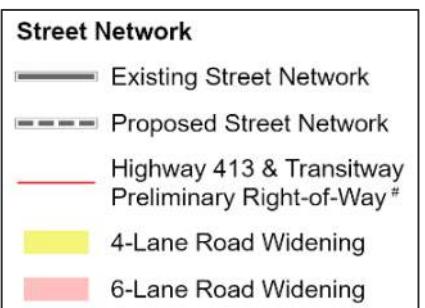
- All arterial and collector roads are recommended to have dedicated active transportation facilities, linking pedestrians and cyclists to key destinations.
- The project team collaborated closely with the City's Active Transportation team and other stakeholders to identify key cycling and trail connection opportunities, building on the network outlined on the Pedestrian and Cycling Network Schedule included in the Heritage Heights Secondary Plan.
- The proposed Recreational Trails and Active Transportation facilities will help connect new communities and destinations while enhancing access to the considerable natural features found in Heritage Heights.



Preliminary Preferred Network

Alignment and Widenings

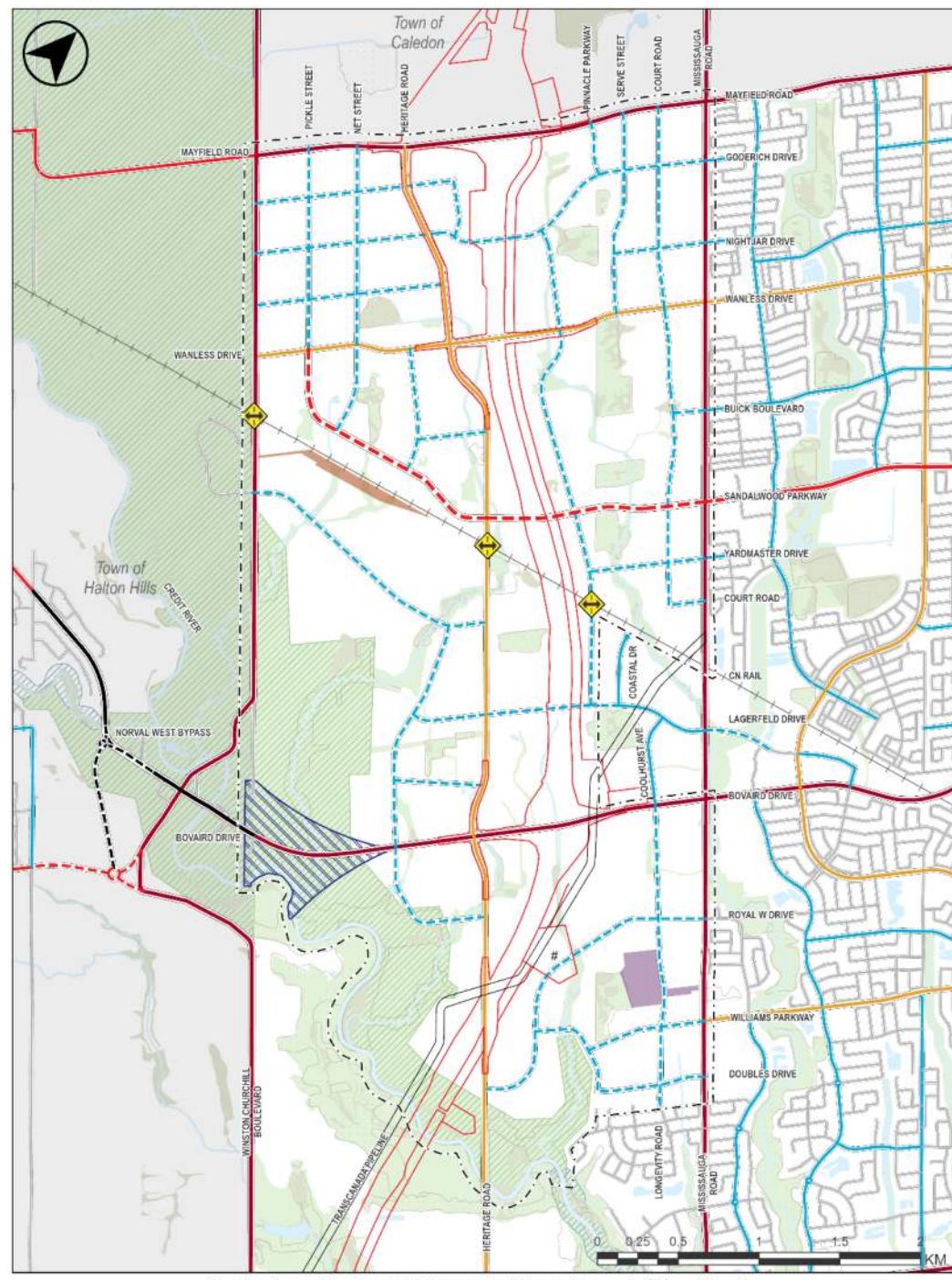
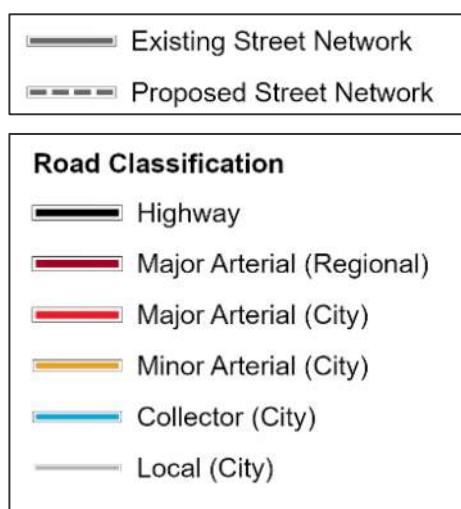
- The preliminary preferred street network reflects the recommended road alignments from the Long List as well as those evaluated in the Short List.
 - 4-Lane Road Widening**
 - Heritage Road, Wanless Drive, Sandalwood Parkway, Precinct 52-5 collector road, Mayfield Road and Bovaird Drive west of Heritage Road.
 - 6-Lane Road Widening**
 - Mayfield Road and Bovaird Drive east of Heritage Road, Mississauga Road.



Preliminary Preferred Network

Road Network Classification

- The preliminary preferred street network maintains consistency with the existing City of Brampton road classification system.
- Sandalwood Parkway is extended through the Secondary Plan area as a **Major Arterial (City)** road.
- The majority of proposed streets within the Secondary Plan area are classified as **Collector** roads.
- **Local** roads will be determined through Draft Plan of Subdivision applications and built within individual precincts



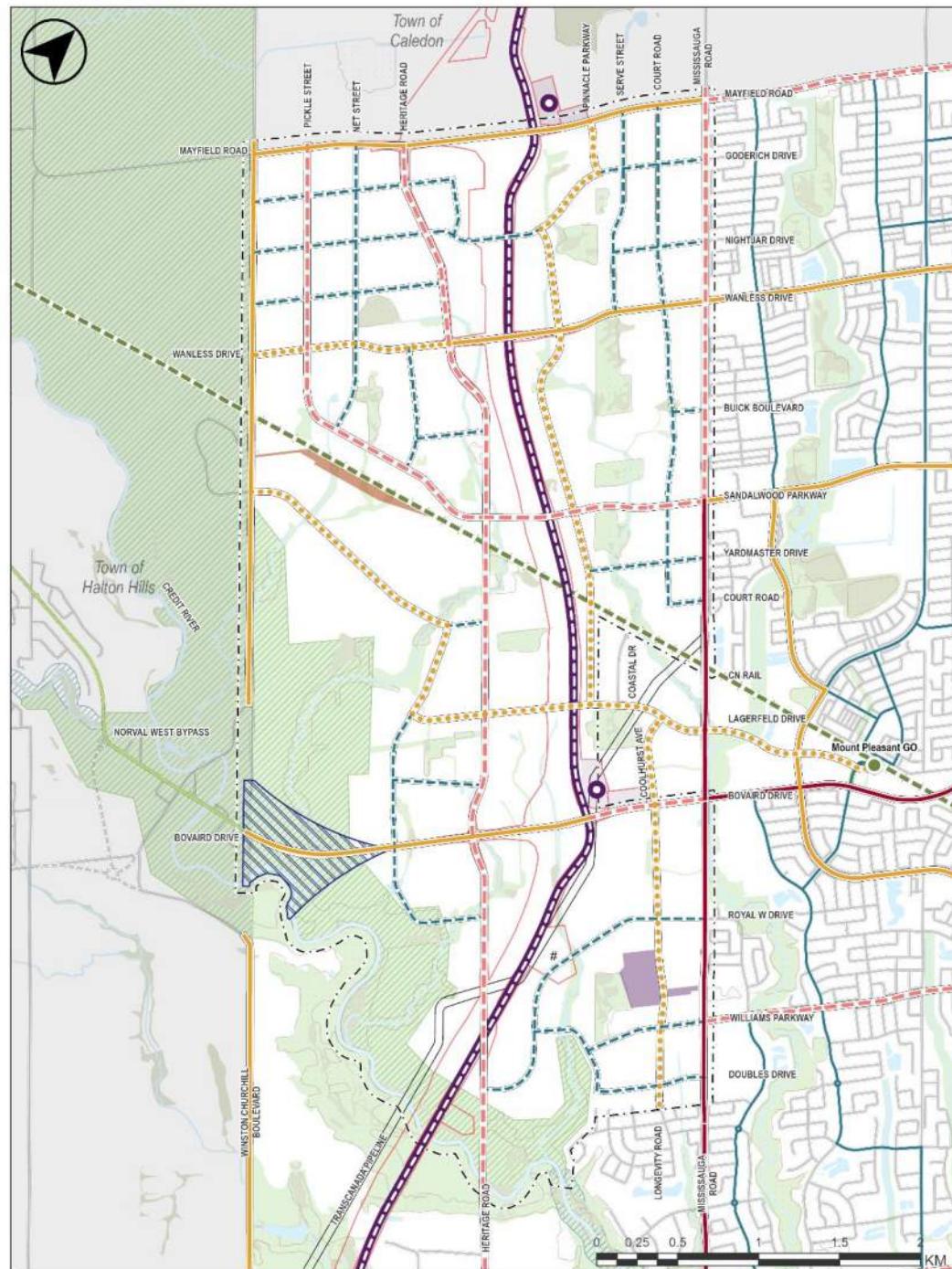
Preliminary Preferred Network

Transit Network

- The conceptual transit network is based on the **Brampton Official Plan Schedule 3B – Transit Network to 2051** and has been realigned to reflect the preliminary preferred street network.
- A **dedicated transitway** is planned along the Highway 413 corridor.
- Opportunities for expanded transit service has been identified as:
 - **Proposed rapid transit service** on Heritage Road and Sandalwood Parkway.
 - **Suggested Support Corridors** along Longevity Road, Lagerfeld Drive, Pinnacle Parkway, and Wanless Drive.
 - **Potential Local Service** along proposed Collector roads.



See [Heritage Heights Secondary Plan Policy 2.4.1 k](#) related to exploration of a potential GO station in Heritage Heights.



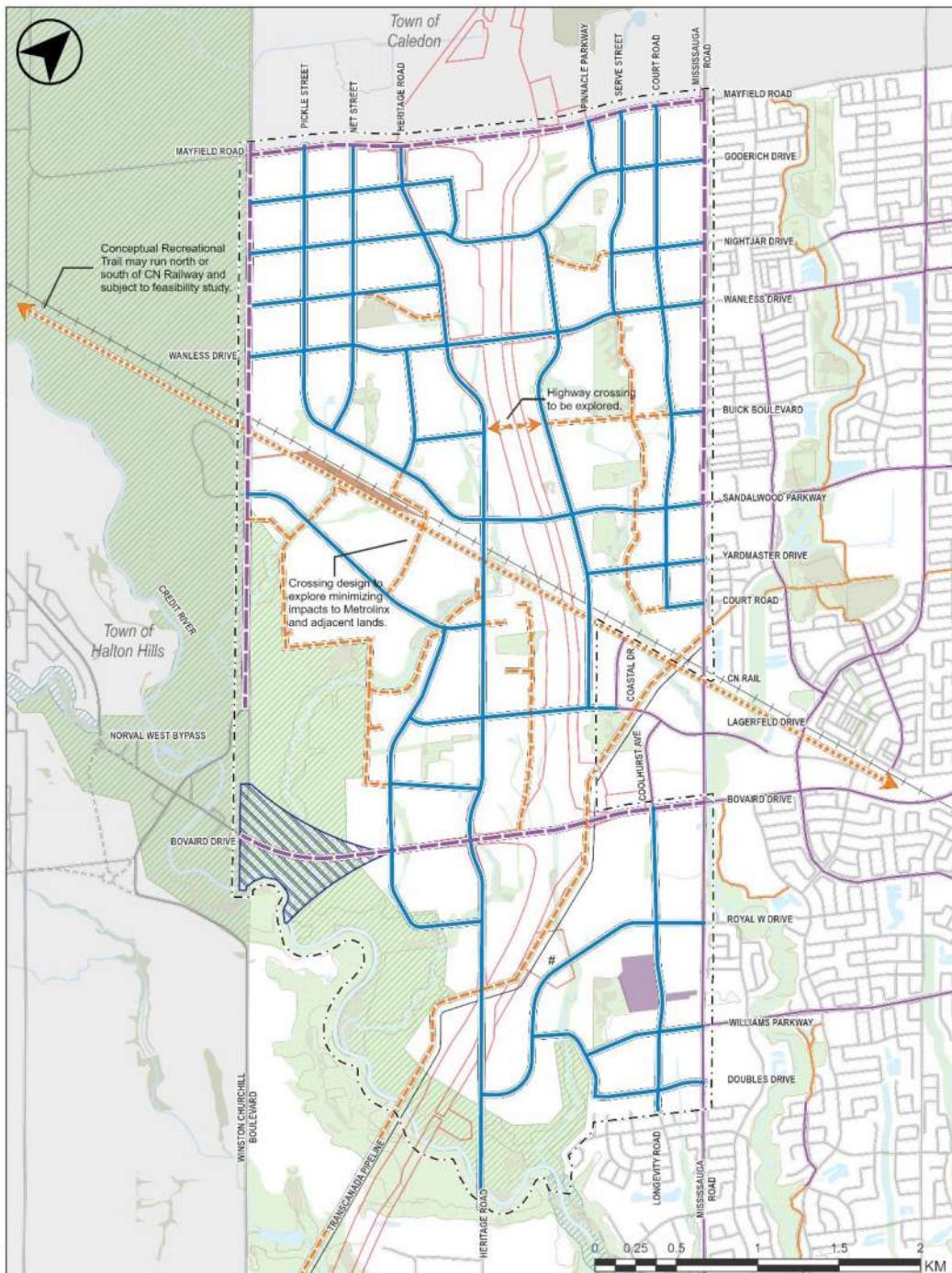
Preliminary Preferred Network

Active Transportation Network

- The active transportation network has been updated from the Secondary Plan to align with the preliminary proposed street network.
- Select streets will be planned to accommodate **on-road active transportation infrastructure**.
- Conceptual **recreational trail connections** are proposed to supplement road infrastructure to ensure continuous pedestrian and cyclist connectivity.
- All proposed active transportation routes are conceptual and subject to change as planning advances.



[†] Existing active transportation infrastructure includes those under construction.
All future / conceptual active transportation infrastructure is subject to change.



1. We want to hear from you!

Please submit any additional comments on the PIC material or further project requests by email to heritageheights@brampton.ca by **September 3, 2025**. Any input received by that date will be taken into consideration as part of the TMP report, which will be available for public comment when the TMP is completed.

1. We want to hear from you!
2. Confirmation of the preferred design based on feedback from the public, Indigenous communities, and stakeholders.
3. Preparation of the final Transportation Master Plan and Environmental Study Report.
4. Place report on public record and notify stakeholders of completion of the study.

Heritage Heights Transportation Master Plan Project Management:

Vikram Hardatt, RPP MCIP

Emily Ecker, P.Eng.

Principal Planner

Senior Associate

City of Brampton

BA Consulting Group Ltd.

Project Email: heritageheights@brampton.ca



MOVEMENT IN URBAN ENVIRONMENTS

5.0 ENGAGEMENT WITH INDIGENOUS COMMUNITIES

Engagement with Indigenous Communities was led by City of Brampton.

The following communities were notified:

- Haudenosaunee Development Institute
- Huron-Wendat Nation
- Mississaugas of the Credit First Nation
 - Feedback is attached below.
- Six Nations of the Grand River First Nation
- The Indigenous Network





July 16, 2025

Project Name: Heritage Heights Transportation Master Plan

DOCA Project Number: 2025-0765

Agent: City of Brampton

Dear Paul Brioux,

This letter is to confirm receipt of the project-related correspondence sent by yourself on behalf of the City of Brampton, on July 15, 2025, regarding the Heritage Heights Transportation Master Plan

The Mississaugas of the Credit First Nation (MCFN) are the Treaty Holders of the land on which the City of Brampton is located – specifically, Ajetance Treaty, No. 19 (1818). The MCFN holds Indigenous and Treaty Rights specific to the project location and its environs, which may be adversely impacted by it. The Department of Consultation and Accommodation (DOCA) is designated by MCFN to handle consultation matters on its behalf.

The DOCA consultation team has reviewed the project-related correspondence identified above. **This project has been flagged for review.**

The following items will be subject to technical review by the following DOCA Units:

- CONSULTATION
 - All relevant documents and reports
- ARCHAEOLOGY
 - All relevant documents and reports
- ENVIRONMENT
 - All relevant documents and reports
- HERITAGE AND CULTURE
 - All relevant documents and proposals

This review will evaluate the above-referenced project-related correspondence and associated documentation in the context of the MCFN's Indigenous and Treaty Rights and will provide feedback regarding any questions, concerns, and/or interests identified by DOCA during the review. Completion of this review is only made possible through the provision of capacity funding for our technical experts. Therefore, I have attached a Technical Review Agreement for your review and execution. When the signed agreement



Mississaugas of the Credit First Nation



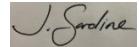
has been returned to DOCA, we will begin our review, and comments will be available in four to six weeks' time.

At this time, the MCFN asserts that Heritage Heights Transportation Master Plan requires further review by DOCA. Until that review has been completed and any questions, concerns, and/or interests have been satisfactorily addressed, the project must not receive approval or proceed with any ground-altering activities.

DOCA expects to be notified of any and all future project updates and/or changes.

If you have any questions for the DOCA consultation team, please feel free to contact us.

Thank you,



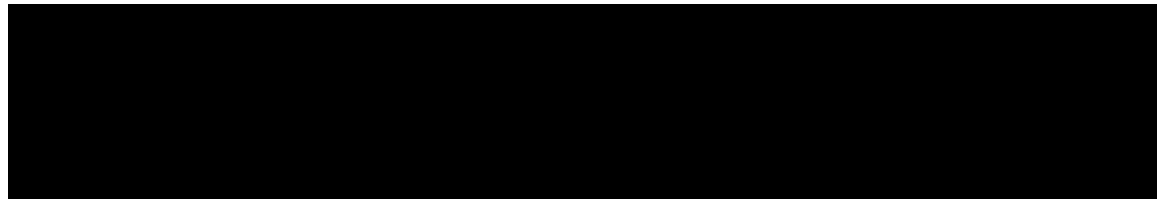
Jeremy Sardine (he/him)

Consultation Specialist

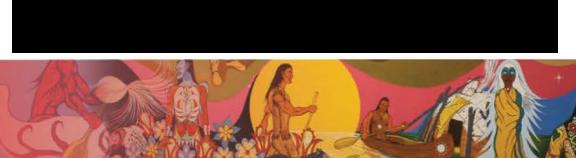
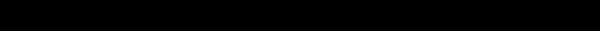
Department of Consultation and Accommodation



Mississaugas of the Credit First Nation

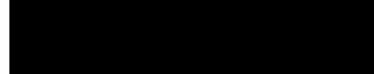


Mississaugas of the Credit First Nation





Mississaugas of the Credit First Nation



6.0 PUBLIC CONSULTATION EVENT

A virtual Public Information Centre (PIC) was held during the evening on August 20, 2025 to inform the public of the Heritage Heights Transportation Master Plan (HHTMP) by presenting alternatives and a Preliminary Preferred Network Alternative. The meeting consisted of two parts: a live presentation of the HHTMP and a question and answer period whereby residents and interest groups could ask questions of the city staff and the project team.

Approximately 25 members of the public attended the virtual PIC meeting; a recording (including the question and answer period) is provided at this link:

[Public Information Centre Recording](#)

Following the virtual meeting, a recording of the presentation was posted on the City's project website and participants at the meeting were informed that they would be able to continue to provide feedback until September 5, 2025.

In this section, public consultation materials are provided including:

- Presentation slides; and
- Feedback formally submitted within the allotted timeframe.





HERITAGE HEIGHTS TRANSPORTATION MASTER PLAN

Prepared For: Public Information Centre (PIC)

August 20, 2025



BRAMPTON

*Heritage Heights
Landowners Group*



BA Group

The City of Brampton is located on the traditional territories of the Mississaugas of the Credit, Haudenosaunee, and Wendat Nations who have called this land home since time immemorial. We acknowledge the agreements made in Treaty 19 – the Ajetance Purchase of 1818 – and are committed to our ongoing role in reconciliation through meaningful action rooted in truth, justice, and respect. We are grateful to the original caretakers of this land who have ensured we are able to work, play, and live in Brampton now and in the future.

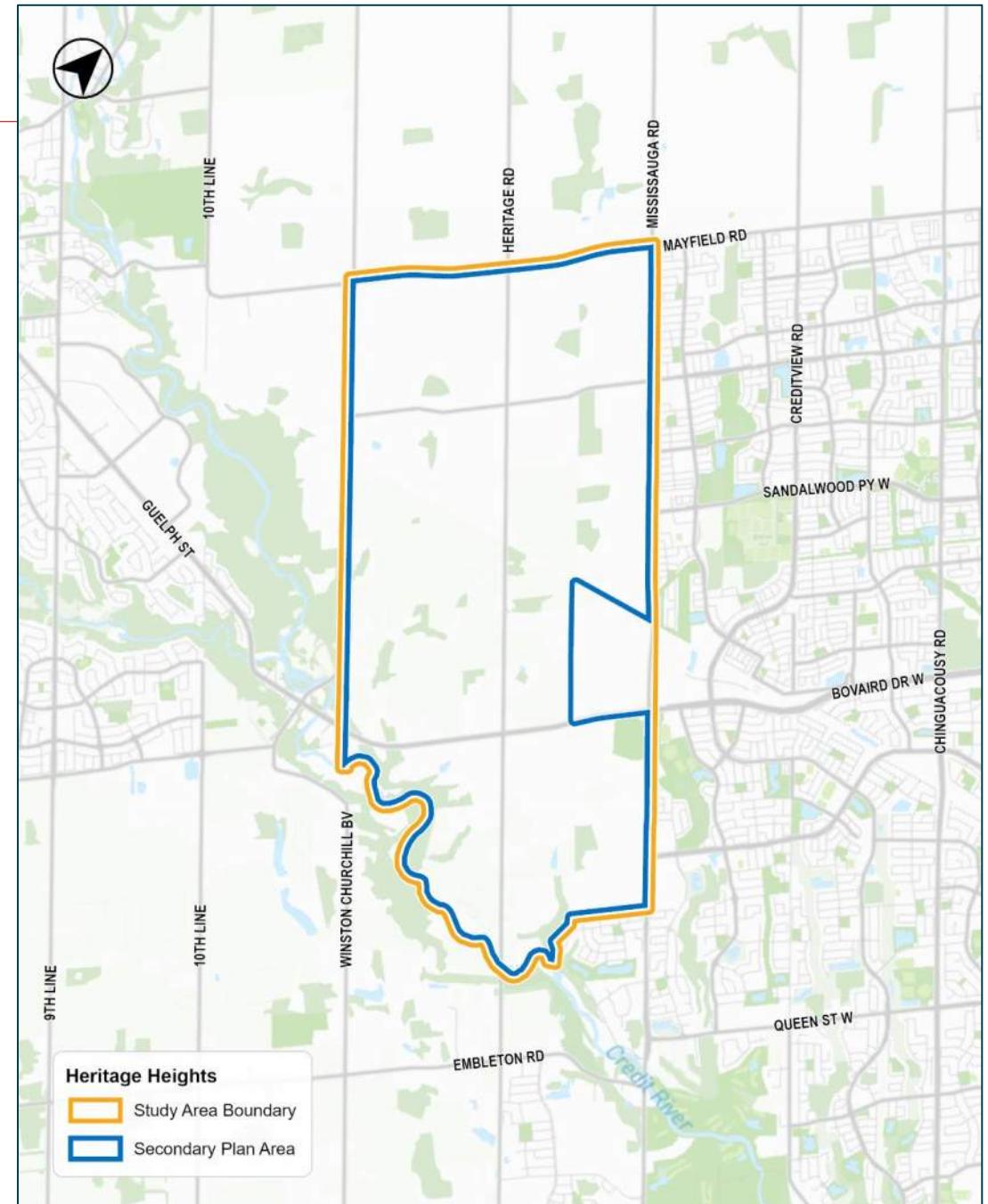
Transportation Master Plan

The Study

The City of Brampton is experiencing significant growth, including within the Heritage Heights Secondary Plan Area.

The City of Brampton and the Heritage Heights Landowners Group are working together as co-proponents to undertake a Transportation Master Plan (TMP) to identify the transportation infrastructure required to support the growth envisioned for the Heritage Heights Secondary Plan Area.

The TMP will provide the long-range planning necessary to identify a transportation network that supports the land use plan and the forecasted travel demand while encouraging more sustainable travel choices and behaviours.

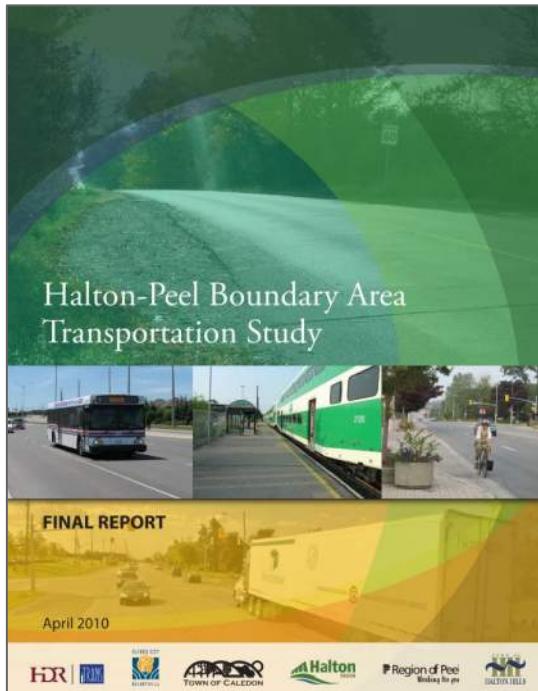


History

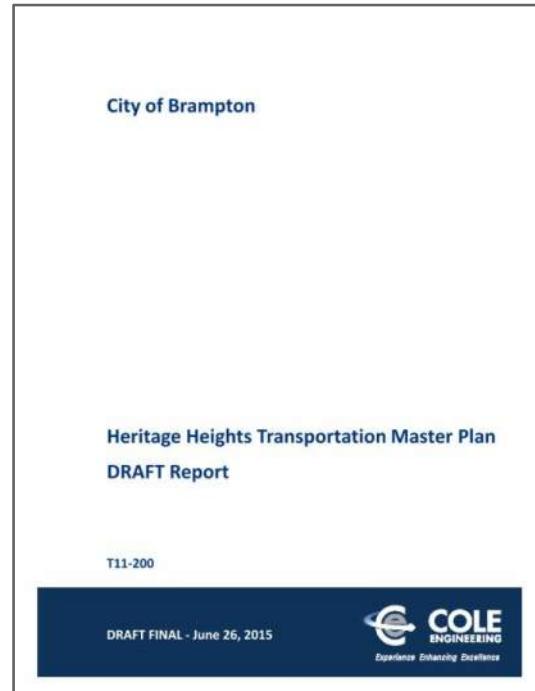
- **2006:** Peel Council extended Regional Urban Boundary to include “North West Brampton Urban Development Area”
- **2009:** Brampton Council initiated secondary plan planning for the Heritage Heights Community
- **2014:** Proposed Land Use Plan Approved for Public Consultation
- **2015:** Council directed staff to revisit the plan and incorporate broader stakeholder engagement which would better inform the plan.
- **2019:** Design charrettes with landowners, staff, agencies, and the public shaped a new vision and land use framework guided by Brampton 2040 Vision.
- **2020:** Council endorsed a new Conceptual Land Use Plan and directed further technical evaluation and policy development.
- **2022:** Heritage Heights Secondary Plan was formally adopted through an Official Plan Amendment.
- **2024:** After appeals, the Ontario Land Tribunal Decision was published, and the Secondary Plan’s details were **finalized and approved on August 21st 2024**.

Former Studies

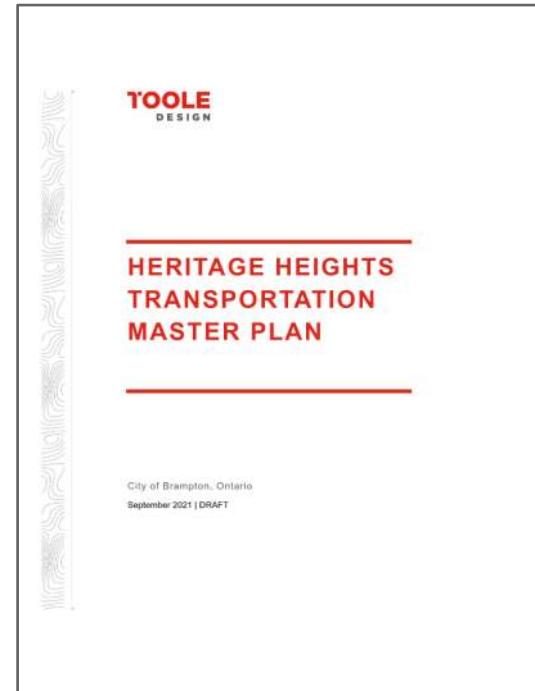
2010 Halton-Peel Boundary Area Transportation Study (HPBATS)



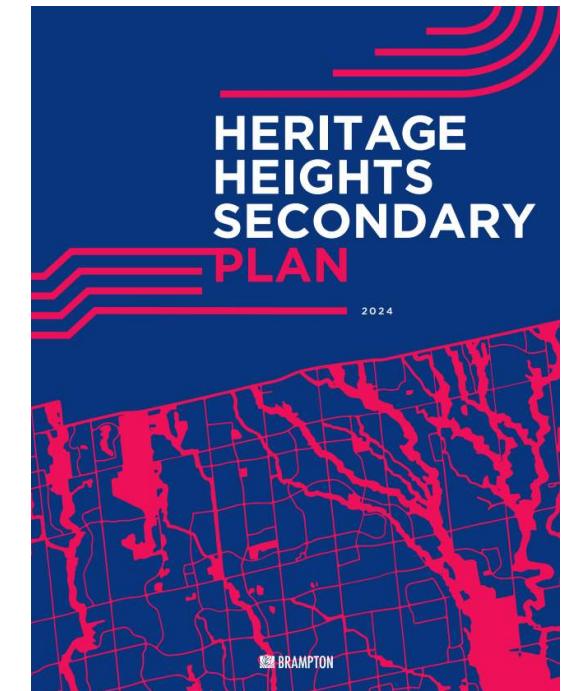
2015 Heritage Heights Transportation Master Plan (draft)



2022 Heritage Heights Transportation Master Plan



2024 Heritage Heights Secondary Plan (approved by the OLT)



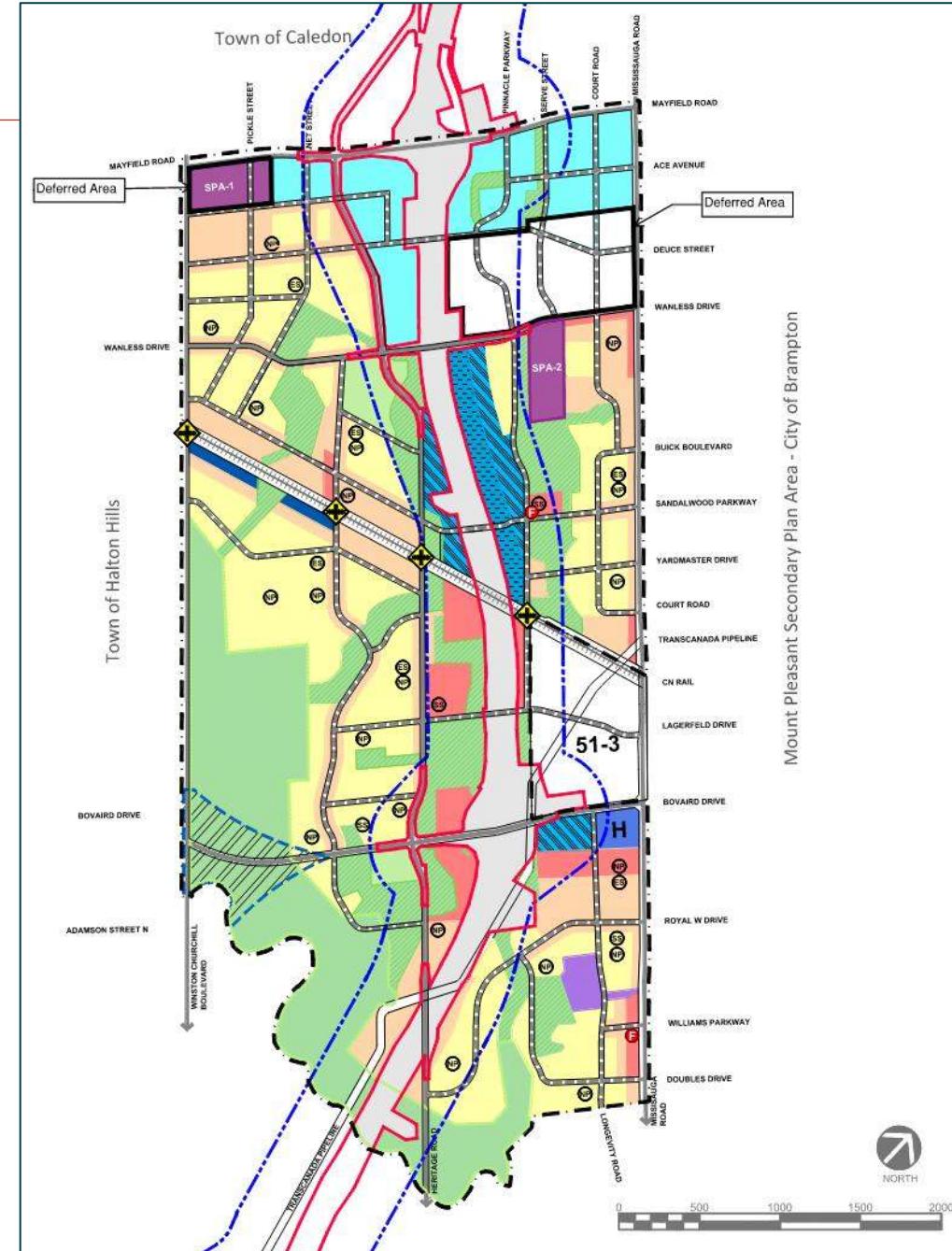
Heritage Heights Secondary Plan

The Secondary Plan

- The revised and approved Heritage Heights Secondary Plan (HHSP) represents a significant re-envisioning of the area that includes the Province's identified provincial transportation corridor.

Supporting Studies Underway

- Infrastructure Servicing Study (ISS)
- Subwatershed Study (SWS)
- Transportation Master Plan (TMP)



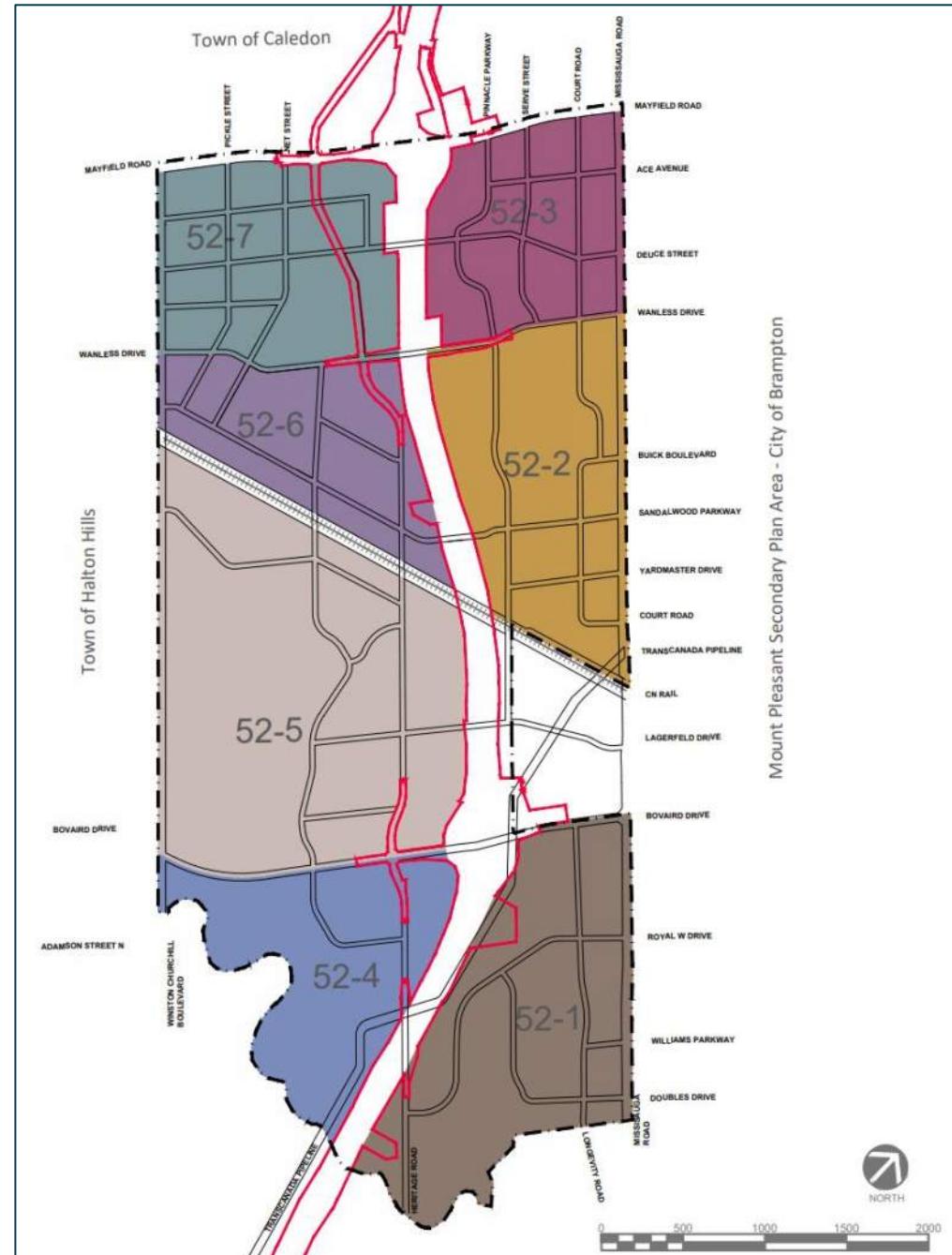
Secondary Plan Precinct Areas

“Precinct Plans will be developed incrementally over time for the Precinct Areas defined in this Secondary Plan. Precinct Plans are intended to outline development principles and guidelines at a more detailed level. Precinct Plans will demonstrate how lands can be developed to meet the vision and policies in this Secondary Plan.”

“Precinct Plans will a) Establish the location, scale and character of streets in addition to those already identified on Schedule 52-8 – Street Network; c) Identify infrastructure requirements to support development; f) Develop implementation strategies for ... infrastructure requirements to support growth in the Precinct;”

“Schedule 52-2 – Precinct Areas identifies the boundaries of seven (7) Precinct Areas, each of which will have a unique Precinct Plan.”

Section 3, “Precinct Area Structure” –
Heritage Heights Secondary Plan



Vision

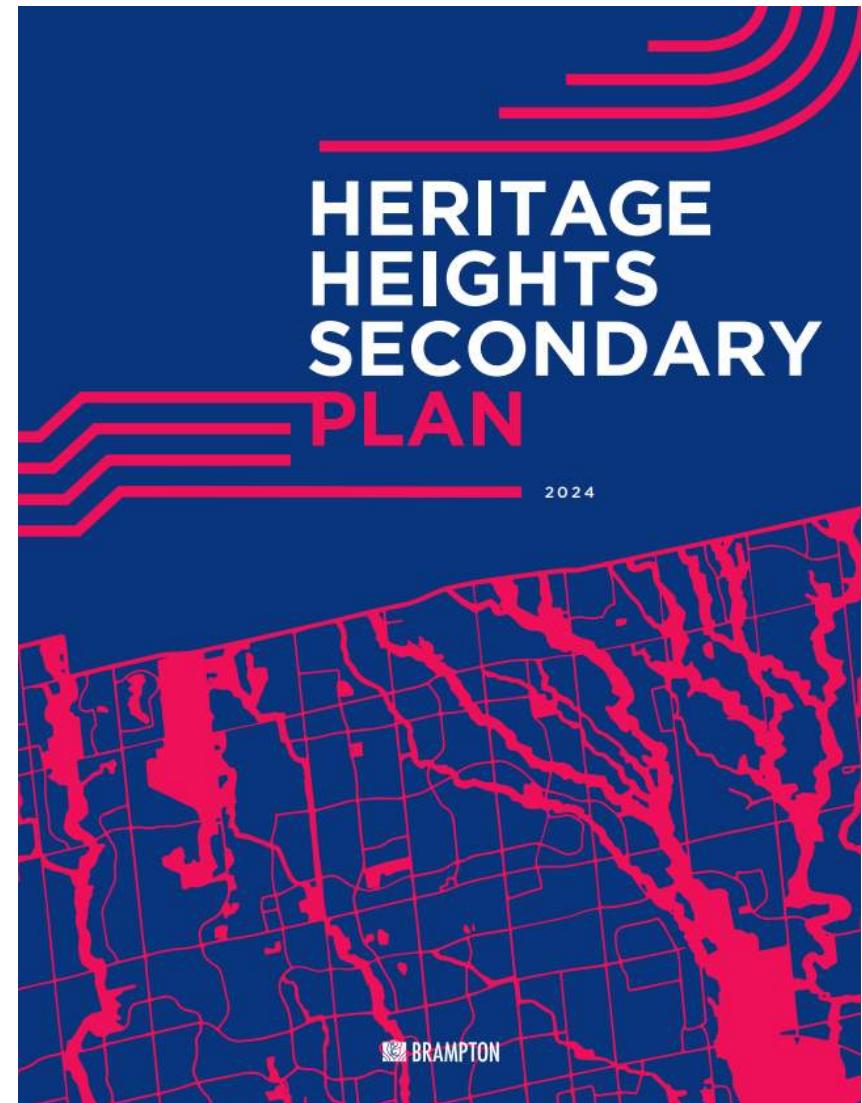
The following principles were created to guide the future design and growth policies in Heritage Heights and continue to be reflected in the revised Heritage Heights Secondary Plan:

1. Create **walkable communities** for people to gather, recreate, work and live.
2. Development should be **compact and diverse** to achieve walkable, affordable, and active neighbourhoods.
3. Implement **sustainable and resilient plans**, technologies and design approaches.
4. Include **arts and cultural uses** to leverage Brampton's diversity and attract investment.
5. Conserve the **natural and cultural heritage** of the area, creating a destination for local and regional visitors.
6. Foster a competitive environment for **employment and economic development**.
7. Plan for well-being - physical, mental and social - through the design of **people-centric spaces that are safe and age-friendly**.
8. Integrate and **connect green and open spaces** into the design of neighbourhoods **while being sensitive to existing ecological systems**.

“The Secondary Plan Area will be premised on strategies that promote a balanced approach to transportation and land uses, which leverages investment in transit and enhancements in the pedestrian and cycling environment to provide a range of travel choices and encourage more sustainable travel behaviours.

The Heritage Heights Secondary Plan will be planned to encourage a safe and attractive walking and cycling environment, public transit, and new streets and connections through precincts by providing pedestrian and cycling facilities, enhancing the public realm, and ensuring midblock connections are introduced with development to enhance connectivity and circulation.”

Section 10, “Mobility” – Heritage Heights Secondary Plan



Process

- The TMP is being undertaken in accordance with **Approach #2** of the Master Planning Process, as outlined in the Municipal Class Environmental Assessment (MCEA) (February 2024).
- The TMP will address Phases 1 and 2 of the MCEA process for any Schedule B projects and will form the basis for the recommended Schedule C transportation infrastructure projects identified within the TMP report.

4.5 Master Planning Process

The master planning process is discussed in Section A.2.7. A summary of the various approaches is provided below.

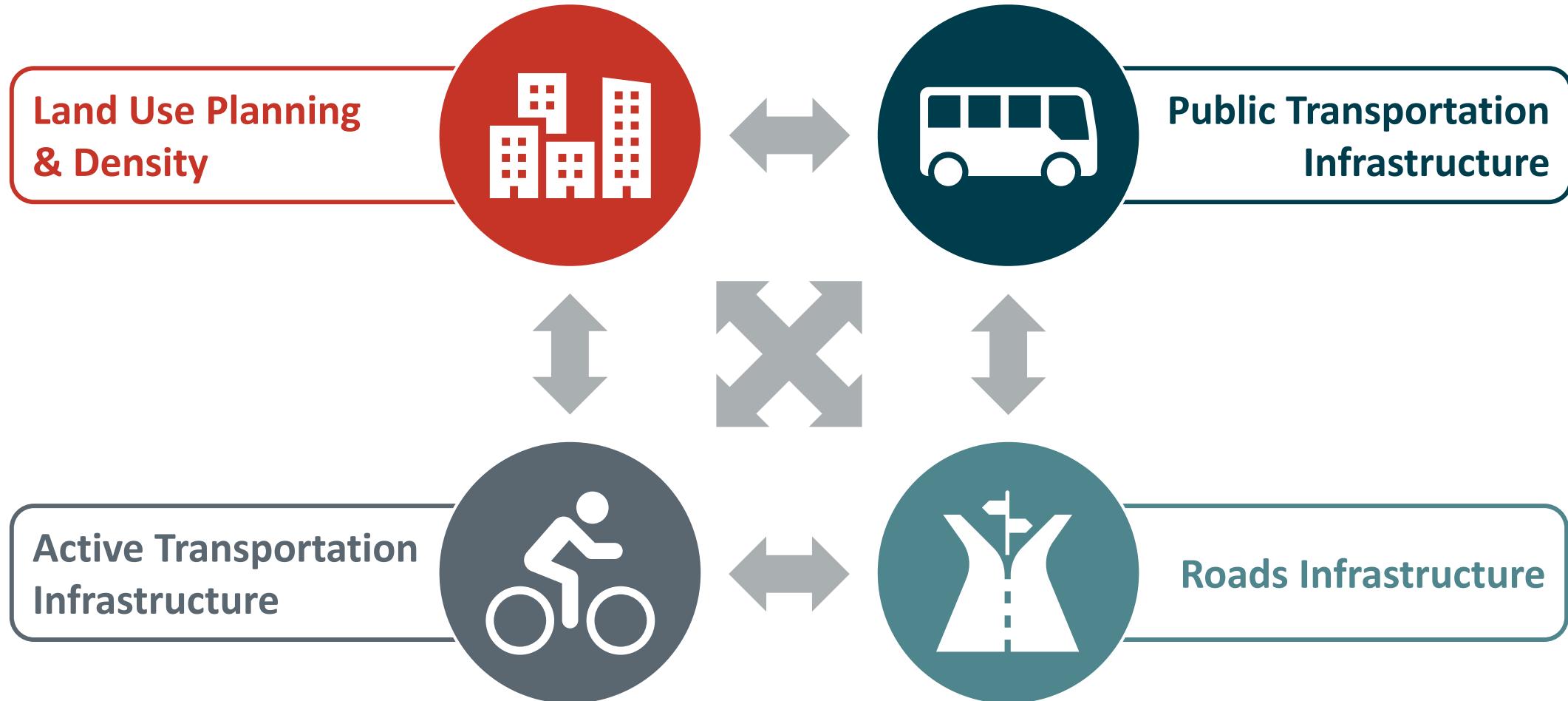
	Approach 1	Approach 2*	Approach 3*
Level of Detail	Broad (project specific information is minimal) Used as support for subsequent Schedule B and C project specific studies	Sufficient to fulfil requirements for Schedule B projects (more detailed project specific information is included) Used as support for subsequent Phase 3 and 4 for Schedule C project specific studies	Sufficient to fulfil requirements for Schedule B and C projects (more detailed project specific information is included)
Process	Preliminary Phase 1 and 2	Phase 1 and 2	Phase 1 to 4
Final Notice	Notice of Master Plan *This should not be called a Notice of Completion	Final notice becomes Notice of Completion for Schedule B projects	Final notice becomes Notice of Completion for Schedule B and C projects
Integration with Planning Act*	*Official Plan and Secondary Plan	*Official Plan and Secondary Plan, Plan of Subdivision, etc.	*Official Plan, Secondary Plan, Plan of Subdivision, etc.

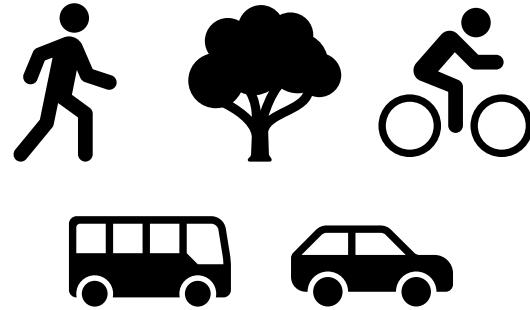
* The various master planning approaches provide proponents with flexibility to customize their master plans to suit their needs. See section A.2.7 for information on modified approaches 2 and 3.

Why a Transportation Master Plan?

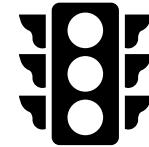
1. **Ground Truth the Land Use Plan and the Transportation Network in the Secondary Plan**
2. **Test Options**
3. **Identify a Transportation Network** (roads, transit, active transportation) that supports the Land Use Plan.
4. **A Collaborative Approach.** Iterative Process and Benefits to future Planning Application processes.







Complete
Street Design

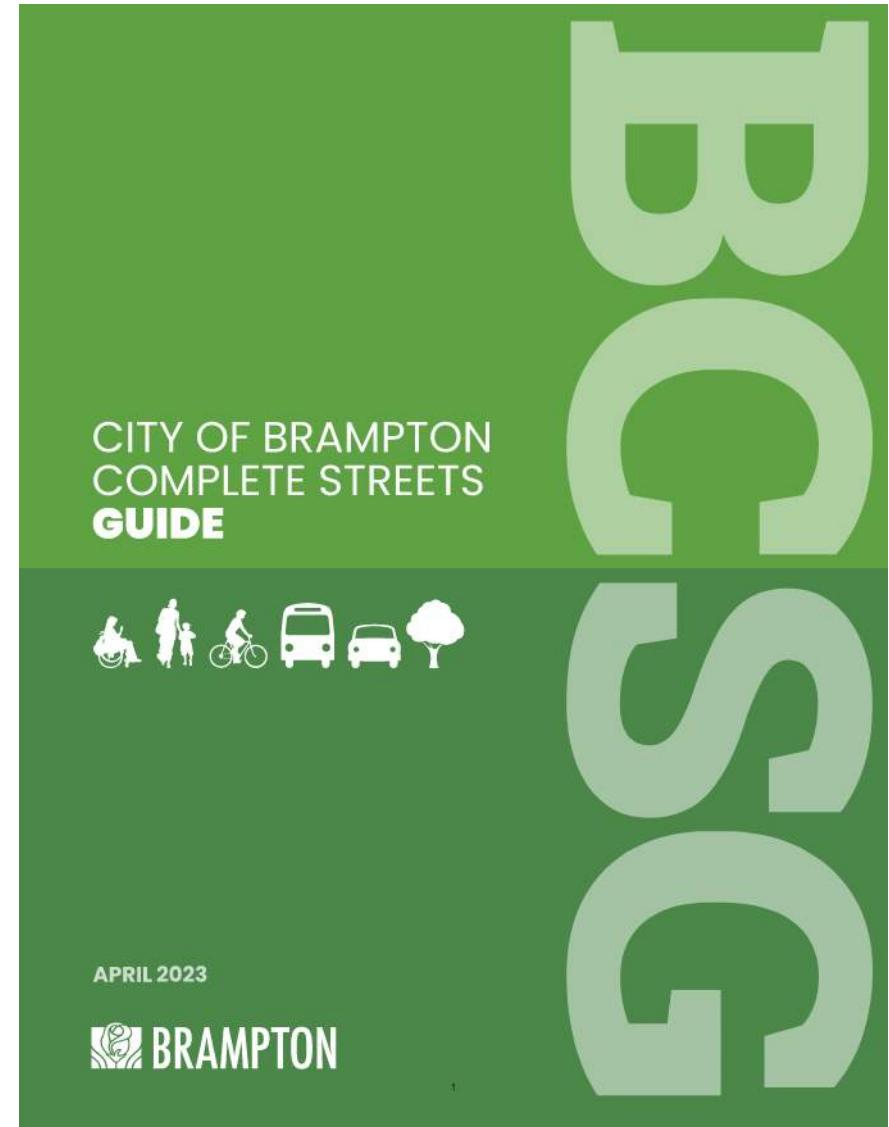


Future Mobility
Conditions
(Transportation
Forecasting Model)



Environmental
Assessment
Evaluation

- As reflected in municipal policy, **the City of Brampton**:
 - “**promotes a multi-modal transportation system**, with the objective of designing, building, and maintaining streets (including multi-use paths) that **safely and comfortably accommodate all users**, including motorists, motorcyclists, bicyclists, pedestrians, individuals with disabilities, transit and school bus riders, delivery and service personnel, freight haulers, and emergency responders.”
- Streets to be located in the Heritage Heights Secondary Plan area of all classification types – Arterial, Collector, Local – were studied for their street characteristics. A collaborative workshop series involving local landowners and City of Brampton staff was undertaken to establish street design principles. **New cross-sections are developed to be located within the Heritage Heights Secondary Plan area.**

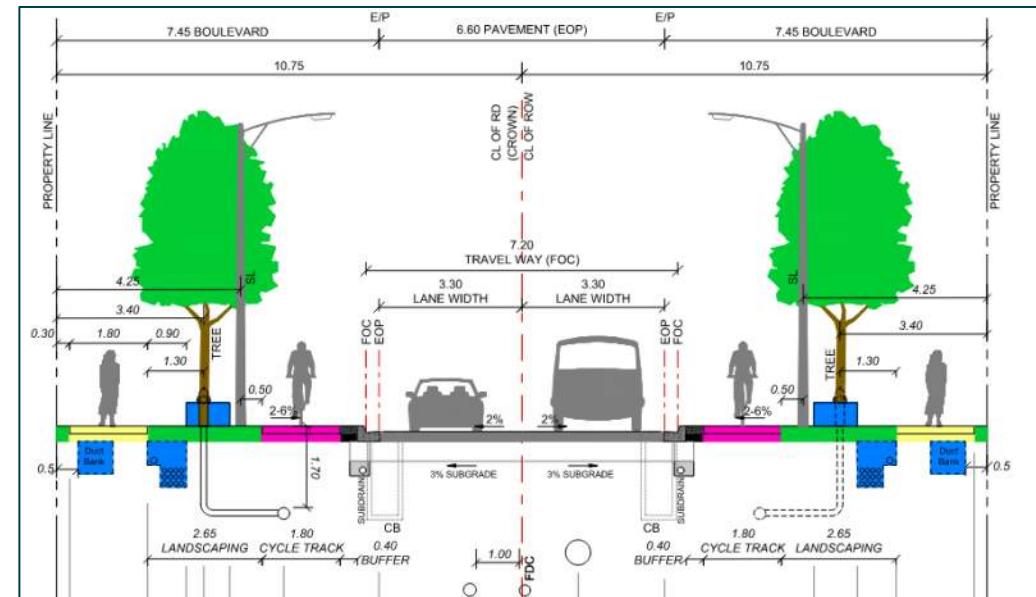
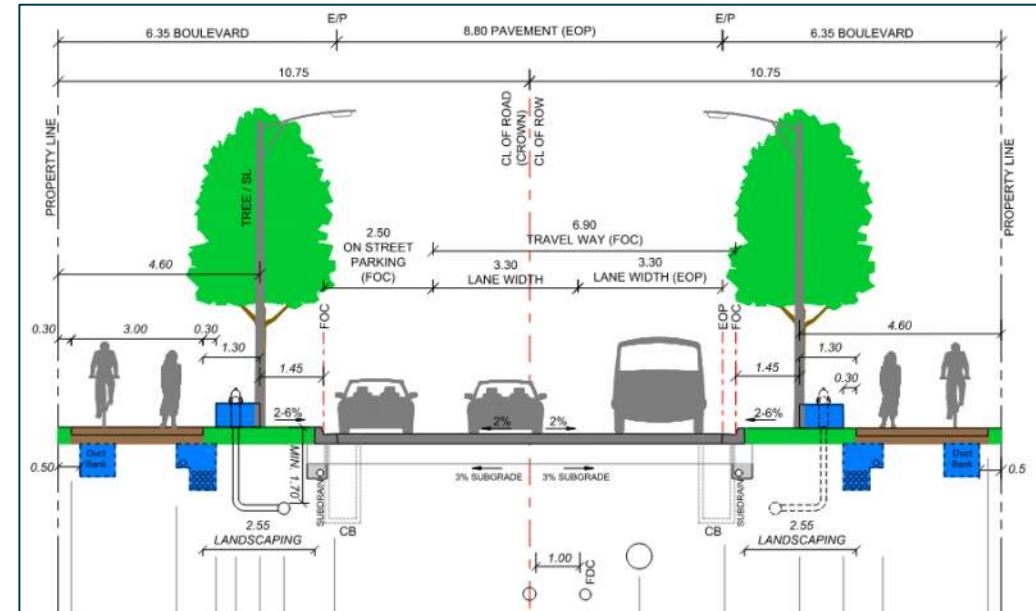


Heritage Heights Street Design

15

- Streets in the Heritage Heights Secondary Plan will look different than the rest of the City.
- Heritage Heights has built on the City's Complete Streets Guide to develop cross-sections unique to the Secondary Plan area that balance:
 - Cycling Facilities
 - Sidewalks
 - On-Street Parking
 - Transit-Friendly Travel Lanes
 - Landscaping
 - Aboveground-Underground Services
 - Compact Urban Design

Draft Street Section Examples



Future Mobility Conditions adopts a standard multi-resolution modelling approach (macroscopic-to-mesoscopic). Tools used include:

1. City of Brampton's Macroscopic Regional Demand Model

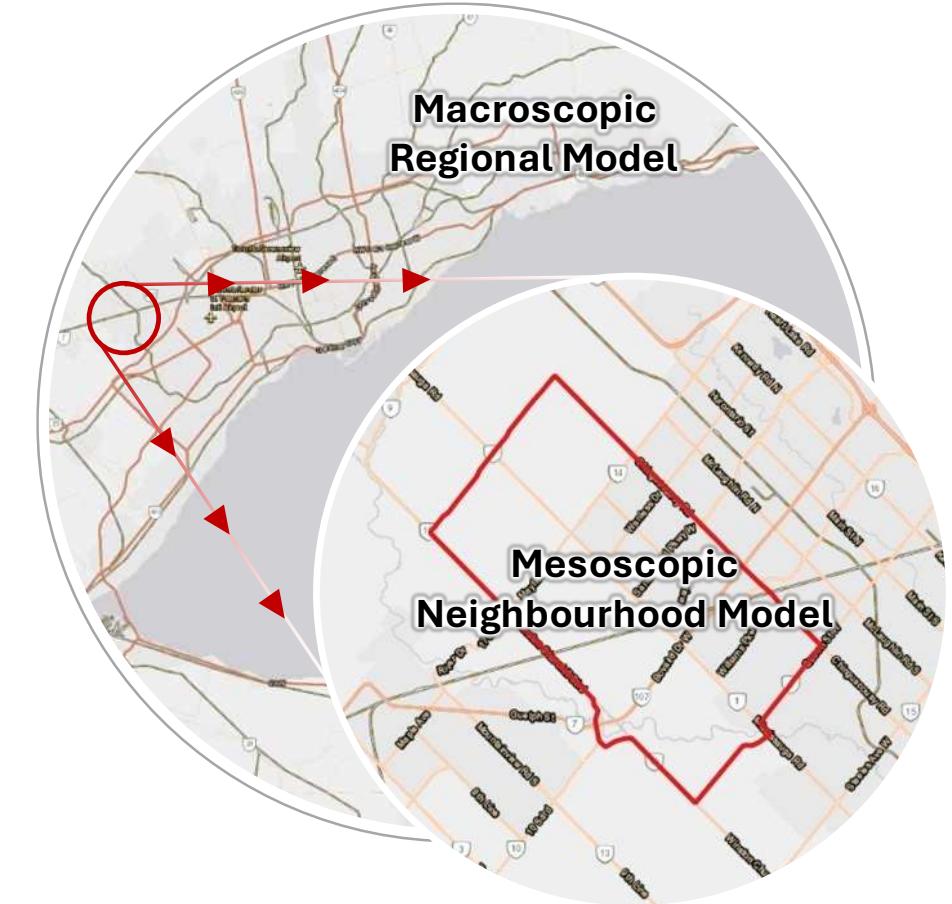
Forecasts transportation demand throughout the Greater Toronto-Hamilton Area

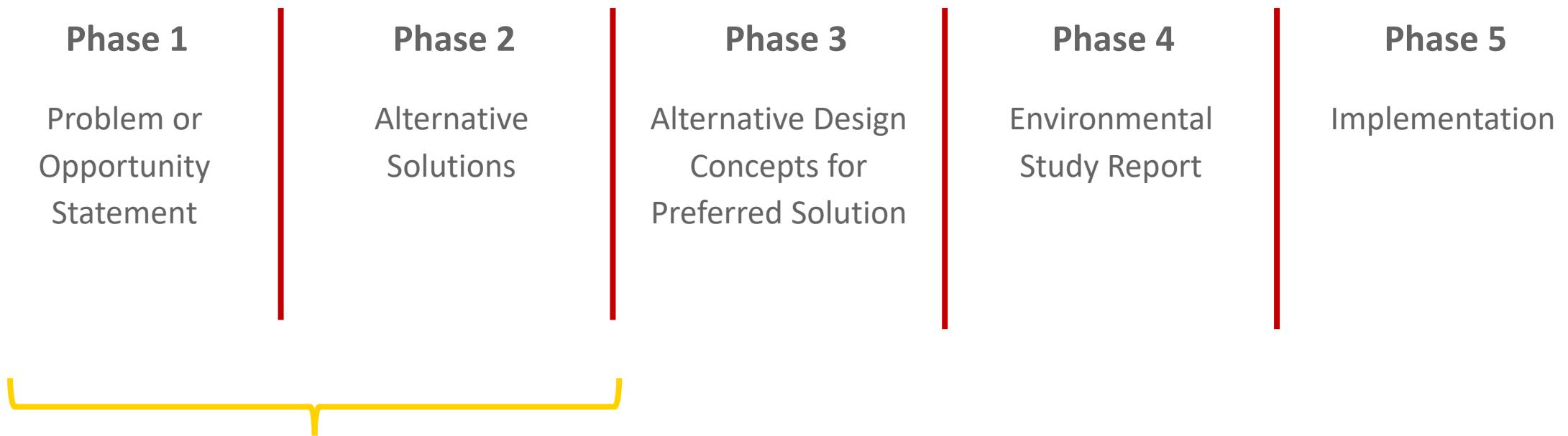
2. Heritage Heights Mesoscopic (“Neighbourhood”) Model

Assesses transportation network operations in and around the Heritage Heights Secondary Plan Area. Model boundaries:

- **Old School Rd to the North, Chinguacousy Rd to the East, Embleton Rd / Queen St W to the South, Winston Churchill Blvd to the West**

Macro-model outputs are used as meso-model inputs – allows for detailed and reliable corridor-level traffic volume projections that account for future planned regional and local growth and transportation infrastructure improvements.





The Heritage Heights Transportation Master Plan follows **Phases 1 and 2**

Alternative Solutions (Phase 2) in the Heritage Heights Transportation Master Plan consider road alignment alternatives and road crossings within the Master Plan road framework.



Street Network

- Road Network and Classification
- Transit Network
- Active Transportation Infrastructure
- Highway 413 Protected Corridor



Traffic Operations

- Weekday Morning and Afternoon Peak Hour Volumes and Capacity



Environmental Features

- Natural Heritage System, Greenbelt, Watercourses
- Terrestrial
- Archaeology Studies*

**Archaeology not required in Phase 1 and 2. Archaeology required prior to road construction.*



Constraints

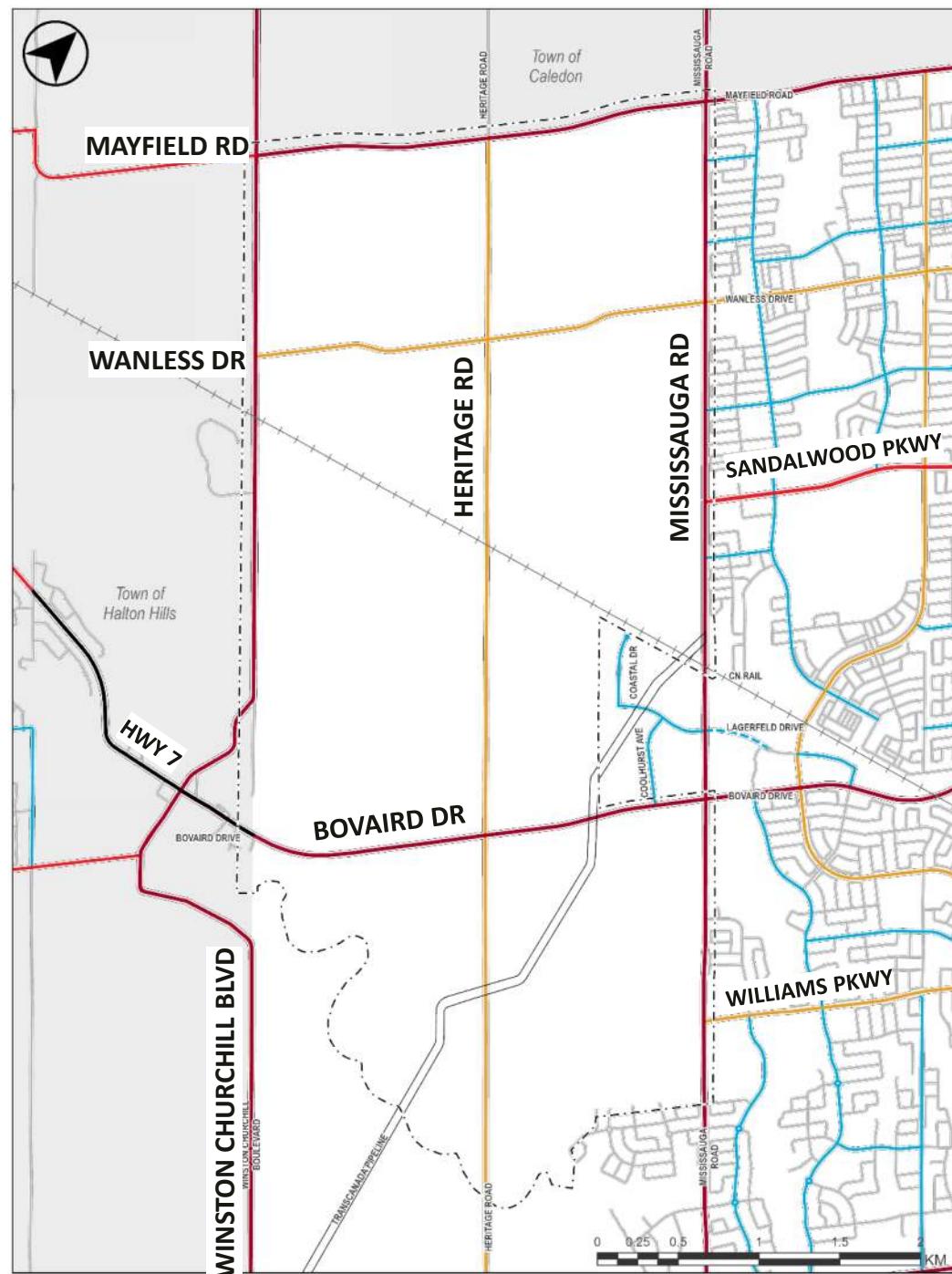
- Cultural Heritage Land and Built Heritage Resources
- Built and Planned Constraints

Existing Roads

- Major Arterial Roads (Regional)
 - Mayfield Road, Bovaird Drive*, Winston Churchill Boulevard, Mississauga Road
- Minor Arterial Roads (City)
 - Wanless Drive, Heritage Road

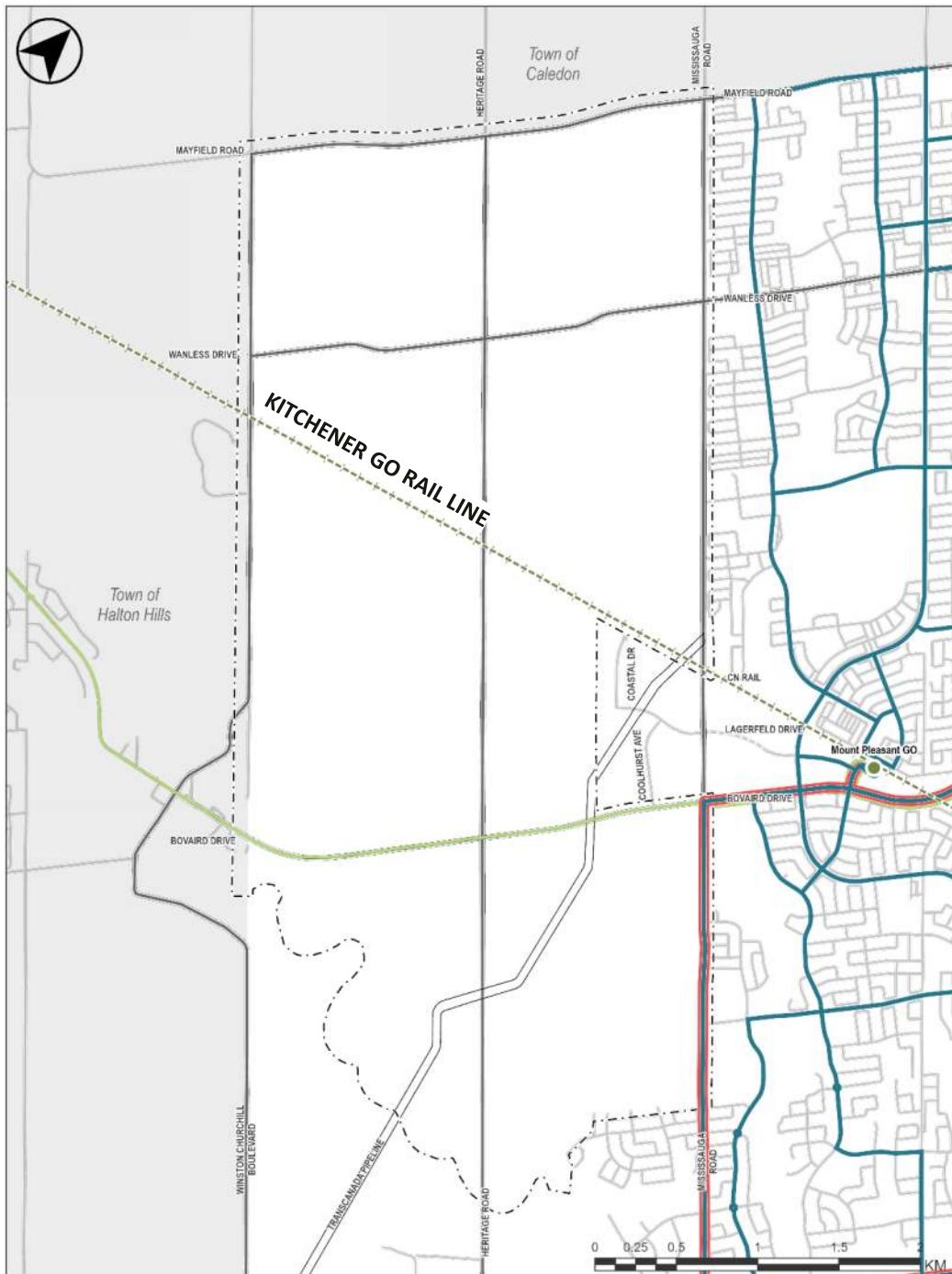
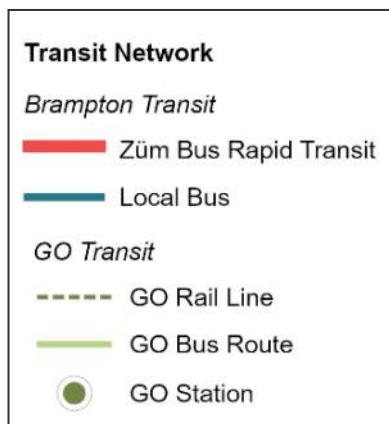
Road Classification	
■	Highway
■	Major Arterial (Regional)
■	Major Arterial (City)
■	Minor Arterial (City)
■	Collector (City)
■	Local (City)

*Bovaird Drive transitions into Highway 7 (Provincial Highway) in Norval (Halton Hills)



Existing Transit

- **Brampton Transit** local bus routes operate east of the Secondary Plan area in Mount Pleasant.
 - Züm bus and local bus routes are provided along Mississauga Road and Bovaird Drive.
- The **GO Transit Kitchener Line** runs along the CN Rail corridor through the Secondary Plan area.
- **Mount Pleasant GO Station** is located along the CN Rail corridor, approximately 1.25-km east of the Secondary Plan area boundary.



Existing Active Transportation

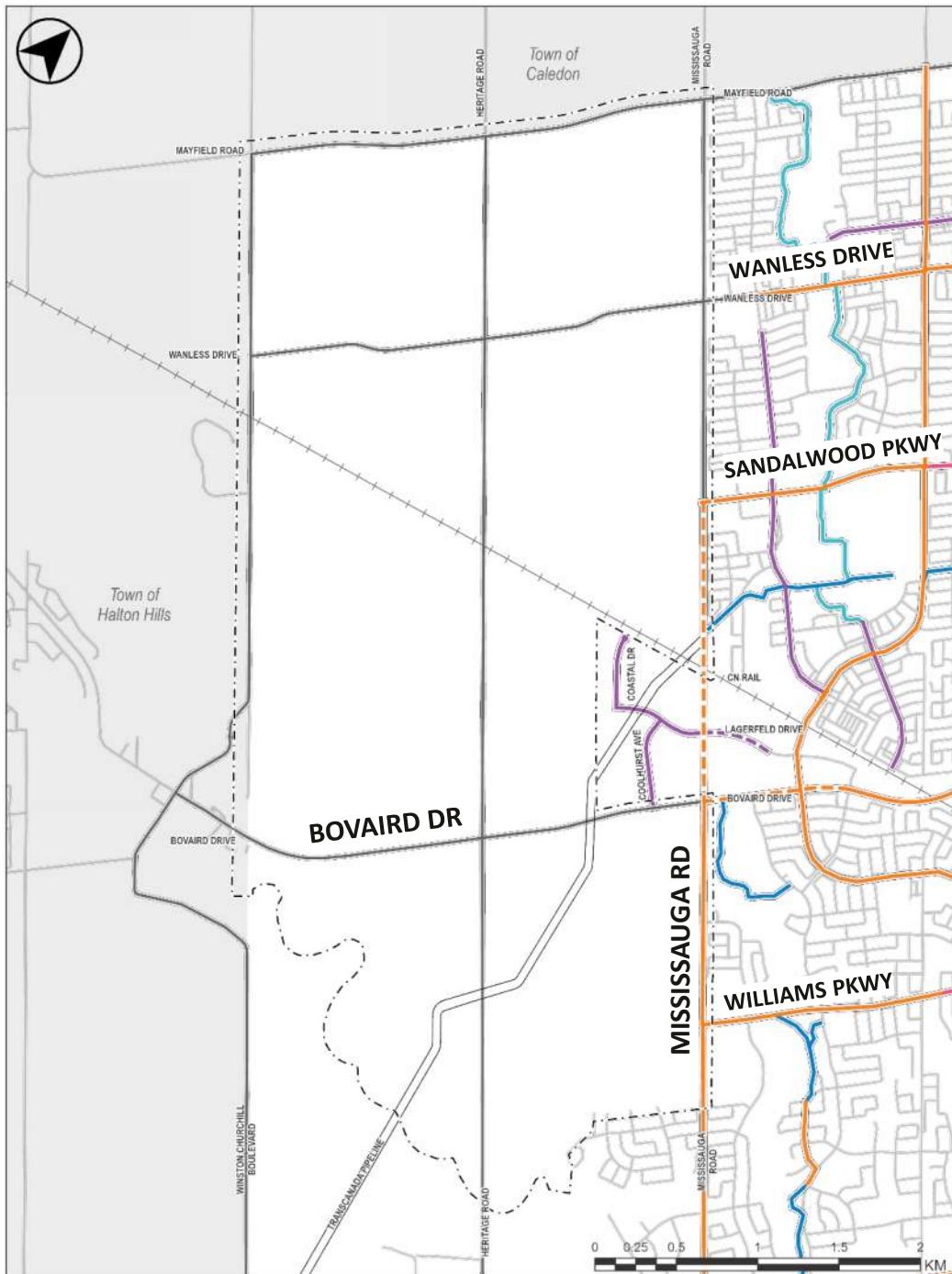
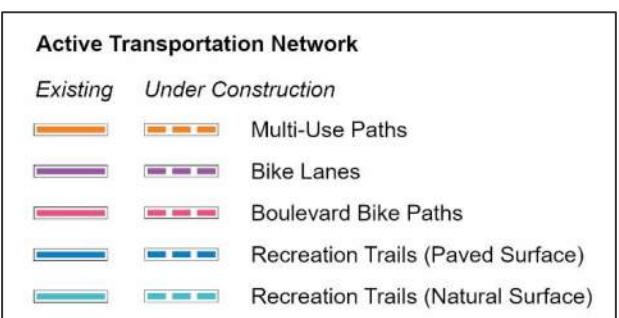
- Active Transportation infrastructure is provided east of the Secondary Plan area.

- **Multi-Use Paths**

- Mississauga Road
- Bovaird Drive
- Sandalwood Parkway
- Williams Parkway
- Wanless Drive

- **Bike Lanes**

- Lagerfeld Drive
- Coolhurst Avenue
- Coastal Drive

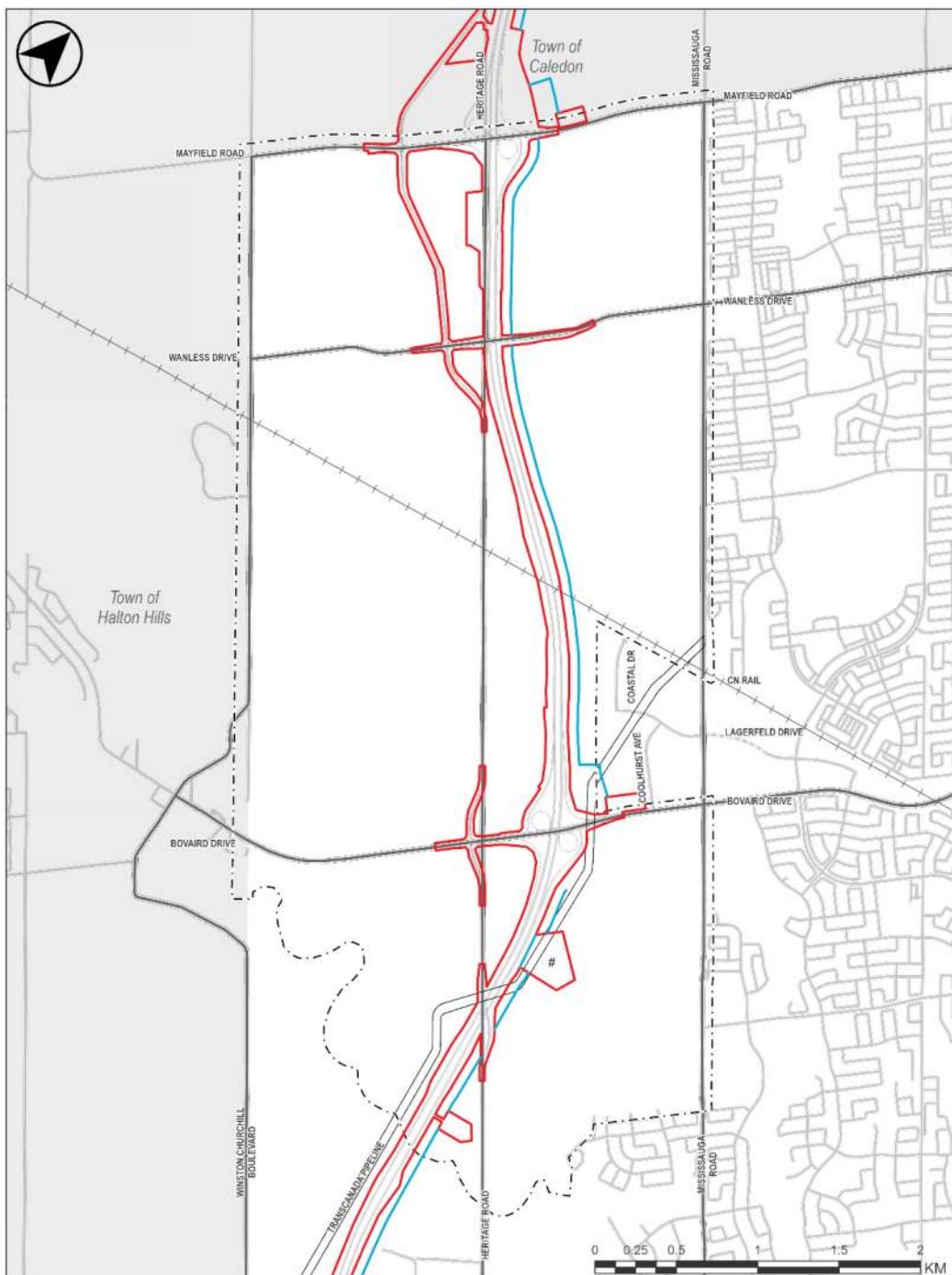


Highway 413 Protected Corridor

- Highway 413 is a:
 - proposed provincial highway connecting Halton, Peel, and York Regions.
 - protected corridor currently at 90% design and is subject to change as planning progresses.
- Highway 413 corridor includes provisions for highway and transitway incorporated on road crossings of the corridor.



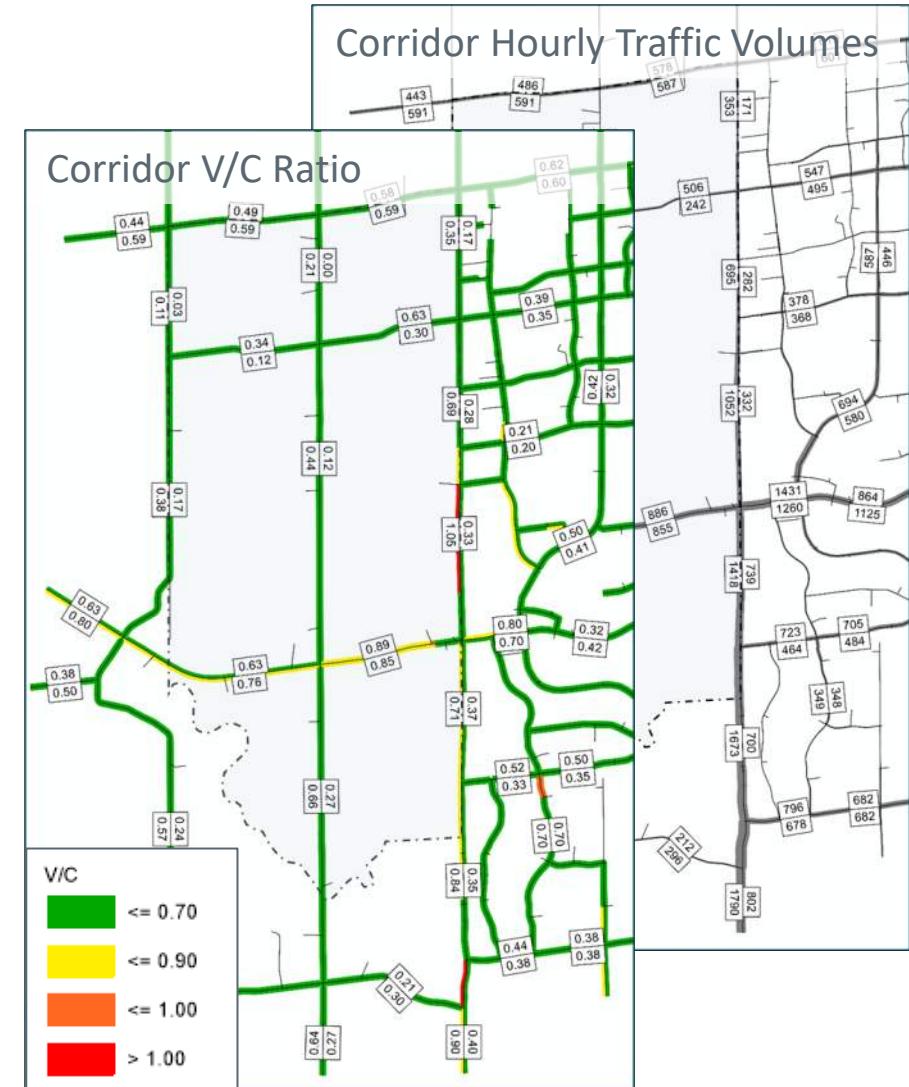
Highway 413 Corridor at 90% preliminary design, subject to change.
SWM pond in Precinct 52-1 to be relocated in consultation with Province.



Traffic Operations – Metrics

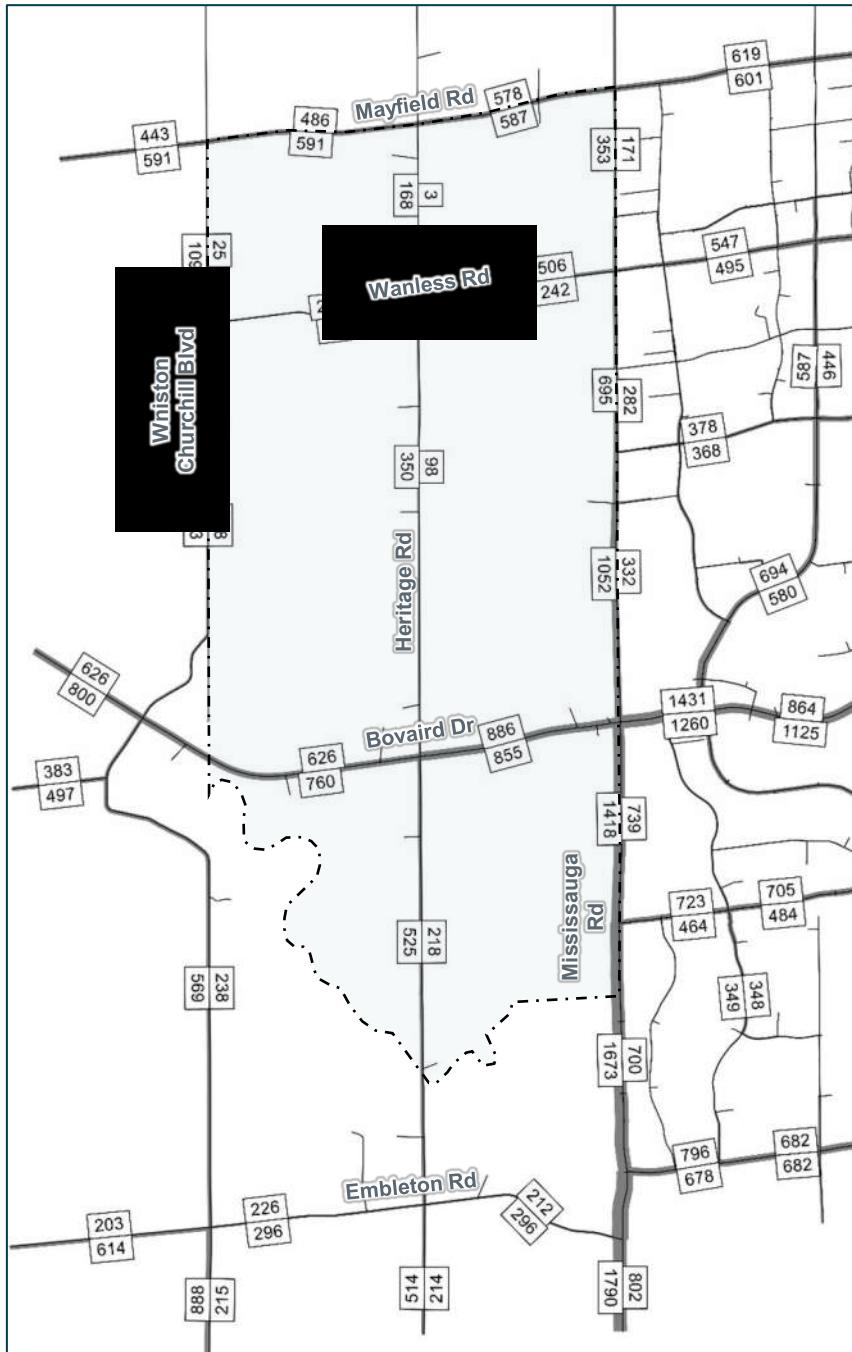
Output Samples

- Traffic operations outputs/metrics include **corridor hourly traffic volumes** and **volume-to-capacity (V/C) ratios**
- V/C ratio is a metric that measures the level of service on a given corridor segment.
- **V/C Ratio = Hourly Traffic Volume ÷ Theoretical Hourly Carrying Capacity**
 - **Green (V/C ≤ 0.70):** not busy, well within capacity
 - **Yellow (V/C ≤ 0.90):** moderately busy, still within capacity
 - **Orange (V/C ≤ 1.00):** very busy, nearing capacity
 - **Red (V/C > 1.00):** over capacity, congested with significant delays

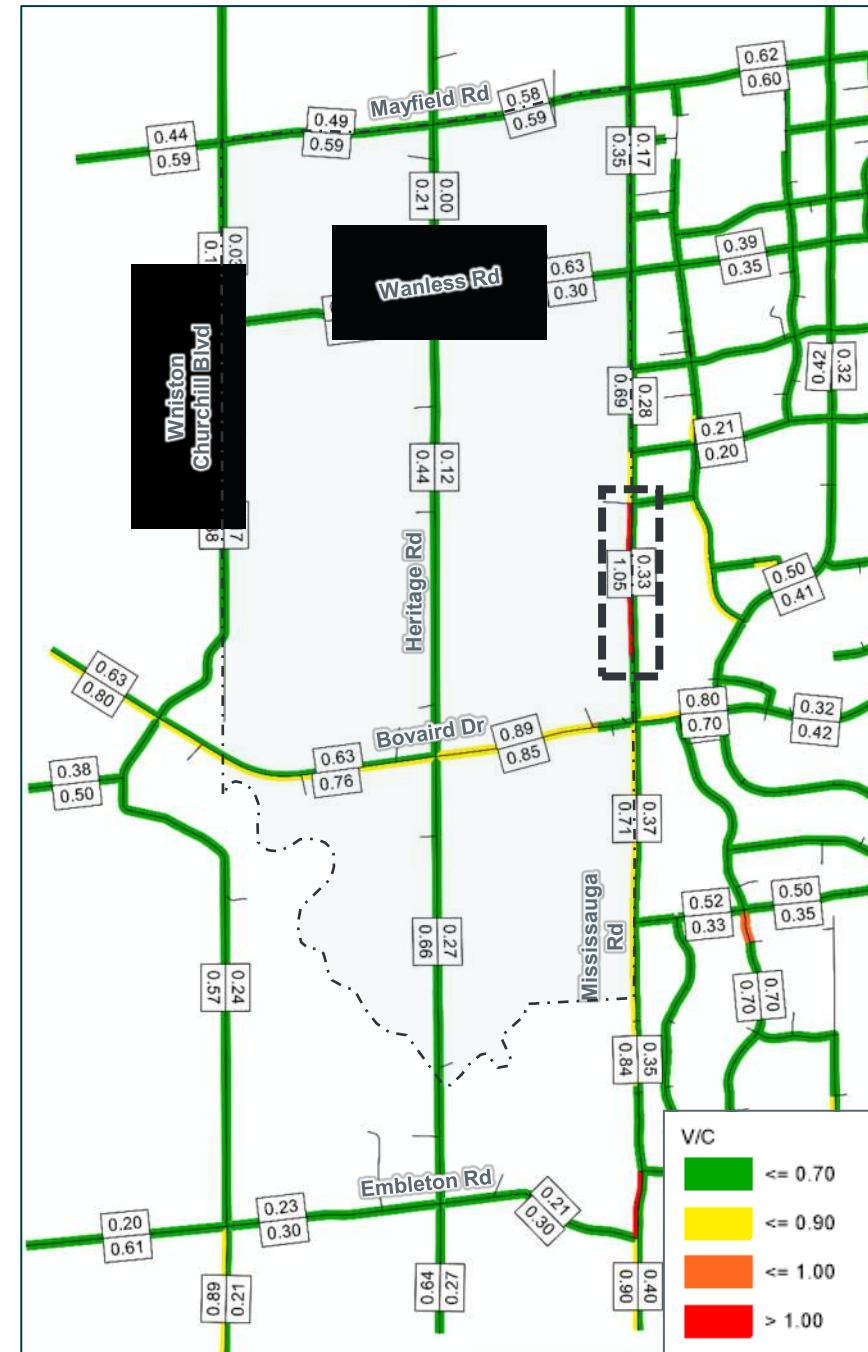


Existing Traffic Operations Weekday AM Peak Hour

- Traffic operations during the **weekday morning peak hour** in the Heritage Heights SPA are generally well within acceptable limits
- Mississauga Road in the southbound direction north of Bovaird Dr is over capacity.



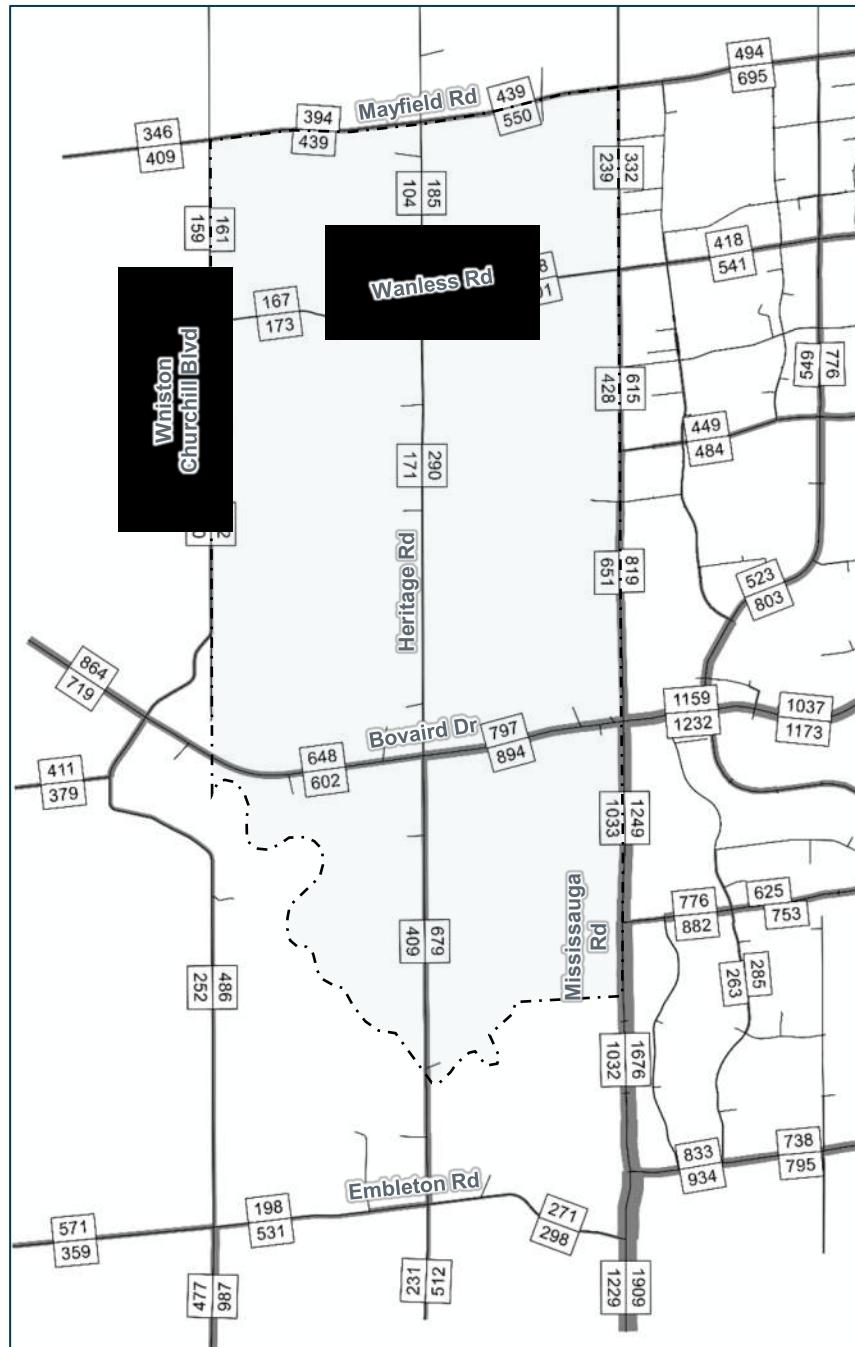
Existing Weekday Morning Peak Hour - Corridor Traffic Volumes (per hour)



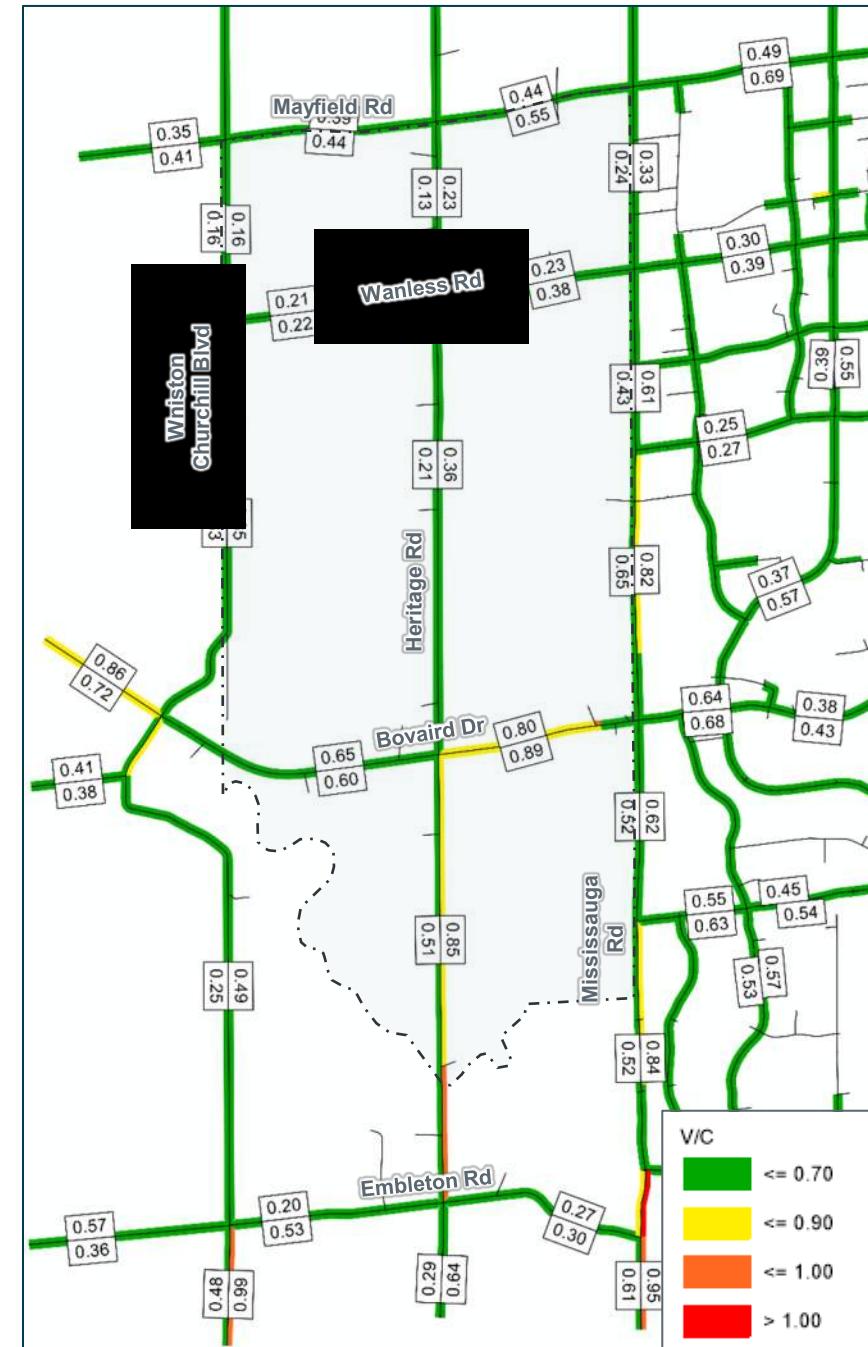
Existing Weekday Morning Peak Hour - Corridor Volume-to-Capacity Ratios

Existing Traffic Operations Weekday PM Peak Hour

- Traffic operations during the **weekday afternoon peak hour** in the Heritage Heights SPA are generally well within acceptable limits



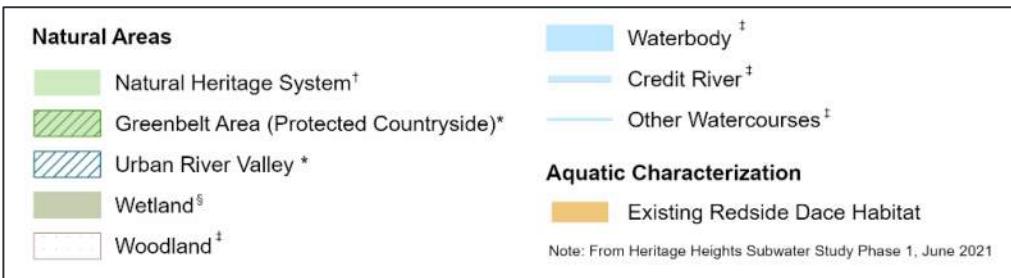
Existing Weekday Afternoon Peak Hour - Corridor Traffic Volumes (per hour)



Existing Weekday Afternoon Peak Hour - Corridor Volume-to-Capacity Ratios

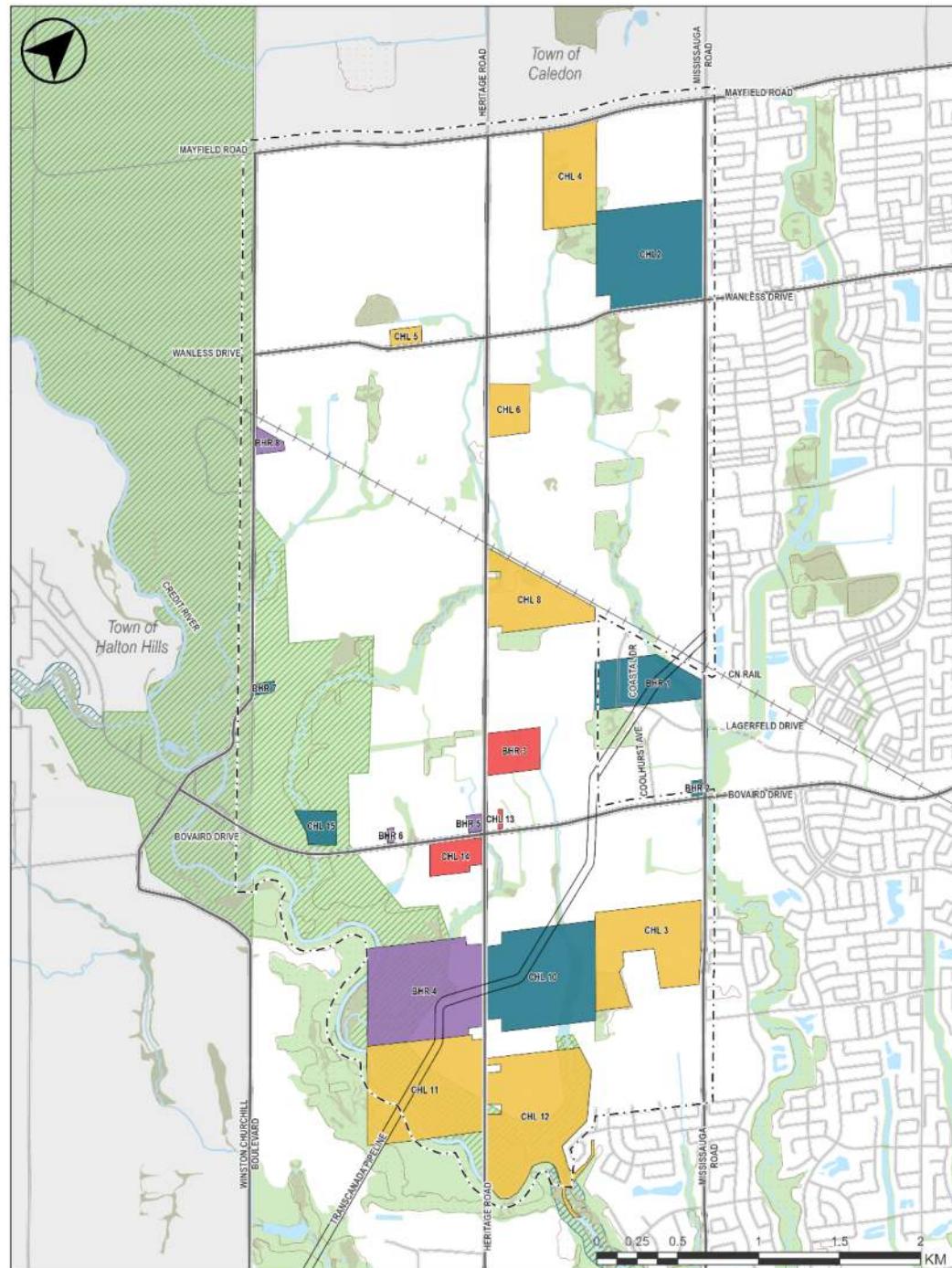
Existing Natural Areas

- Natural Heritage System
- Greenbelt Area (Protected Countryside)
 - Urban River Valley
- Watercourses & Waterbodies
 - Credit River and Credit River Valley
- Woodland & Wetlands
- Redside Dace Habitat



Existing Cultural and Built Heritage Resources

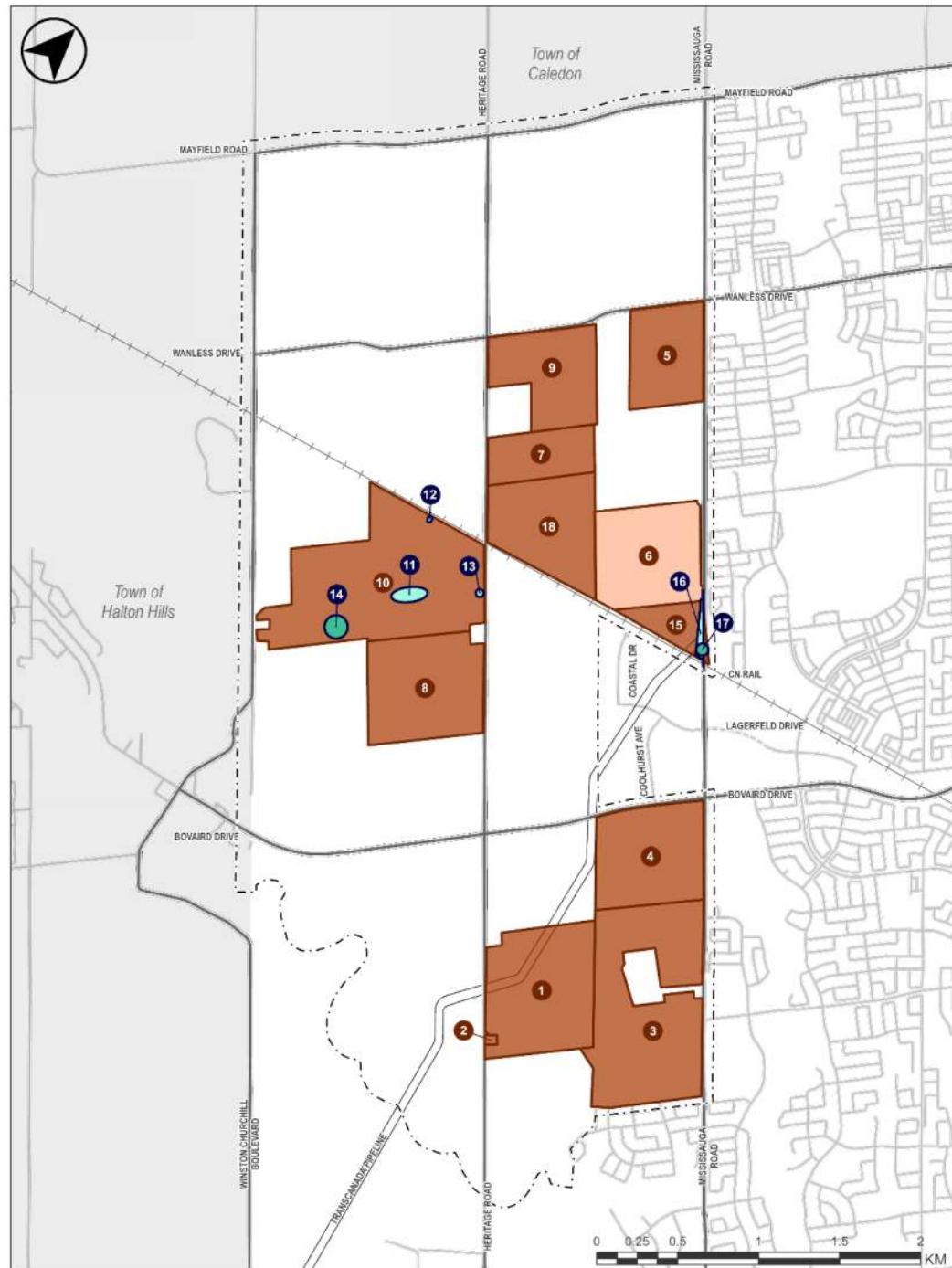
- ASI completed a Cultural Heritage Study for Heritage Heights Community study area in October 2020
- Area has a rural land use history dating back to early nineteenth century with 21 active resources (13 farmscapes, 6 rural residential properties, 1 place of worship and 1 cemetery) as shown in the Figure
- Some of the resources are potentially impacted by road alignments and these could consider implementation of management or mitigation strategies to minimize impacts



Existing Archaeology Studies

- Archaeological Assessments (AA) (Stages 1 and 2) have been completed or are underway in most of the study area and provides an understanding of potential archaeological impacts from road construction
- Most of the Stage 2 work has resulted in clearance under the Heritage Act however there are a few areas that require additional Stages 3 and 4 AA and these have been or will be completed during detailed design and prior to road construction
- Any areas where Stages 1-2 have not been completed will be assessed during detailed design to confirm road alignments
- While AA are not required for a Transportation Master Plan/Phases 1 and 2 of the Class EA process these must be completed prior to initiation of road construction activities in an area

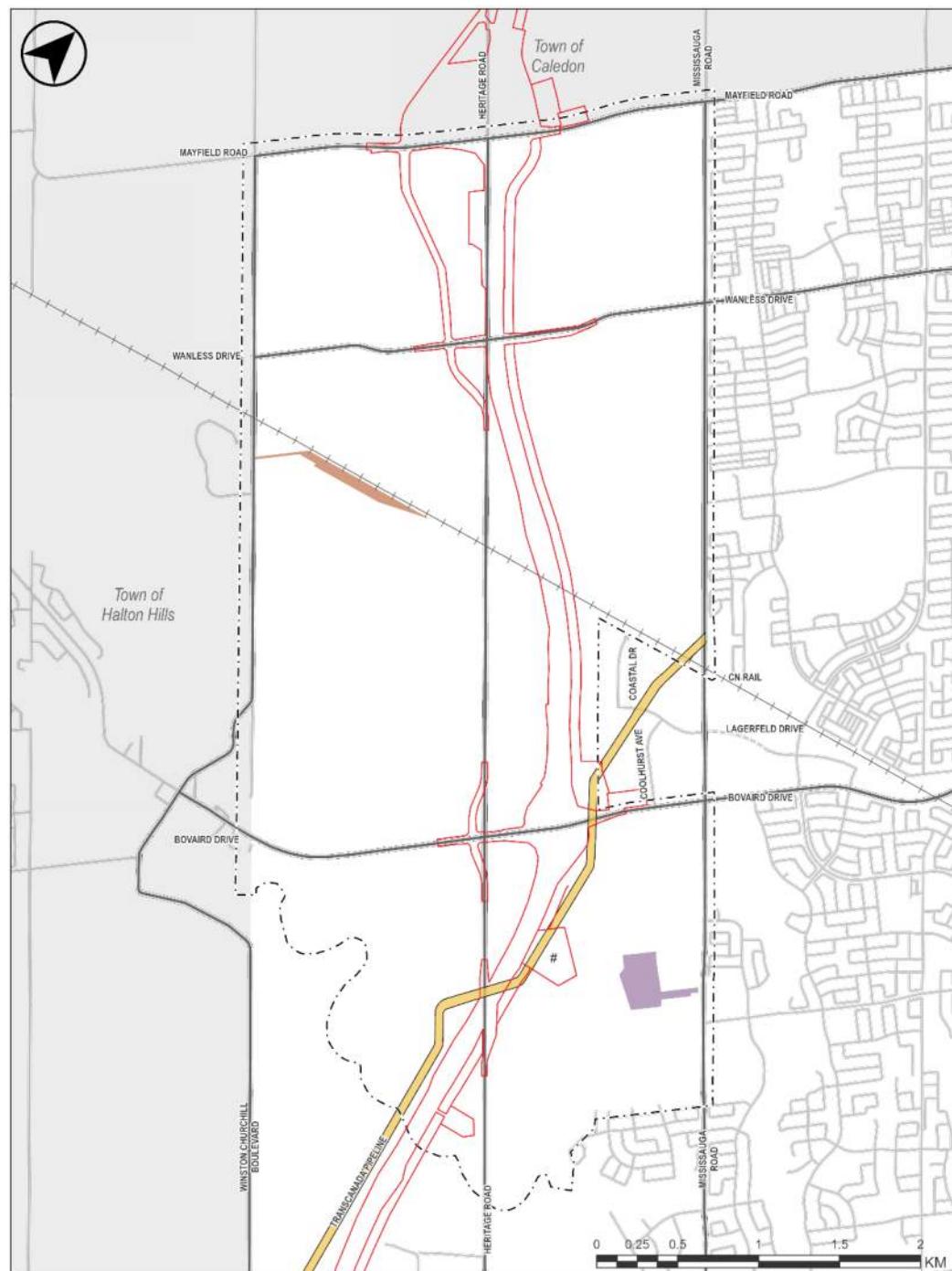
Archaeological Assessment Stage	Completed Archaeology Studies
Stage 1-2	(9) TFP Heritage Wanless Inc.
Stage 1-3	(1) Argo TFP III (2) Argo TFP V (3) DG Lands (4) 2055 Bovaird Drive W (5) Lot 15, Concession 5 (6) Lot 13, Concession 5 (7) 1761540 Ontario Limited Lands (8) 10294 & 10378 Heritage Road
Stage 3	(10) 10510 Heritage Road (11) Cameron Site (12) McNichol's Cemetery (13) 10510 H5 Site (14) McNichol Site (15) Lot 12, Concession 5 (16) McDowell Site - Stage 3 (17) McDowell Site - Stage 4
Stage 4	(18) Lot 13 & 14, Concession 5



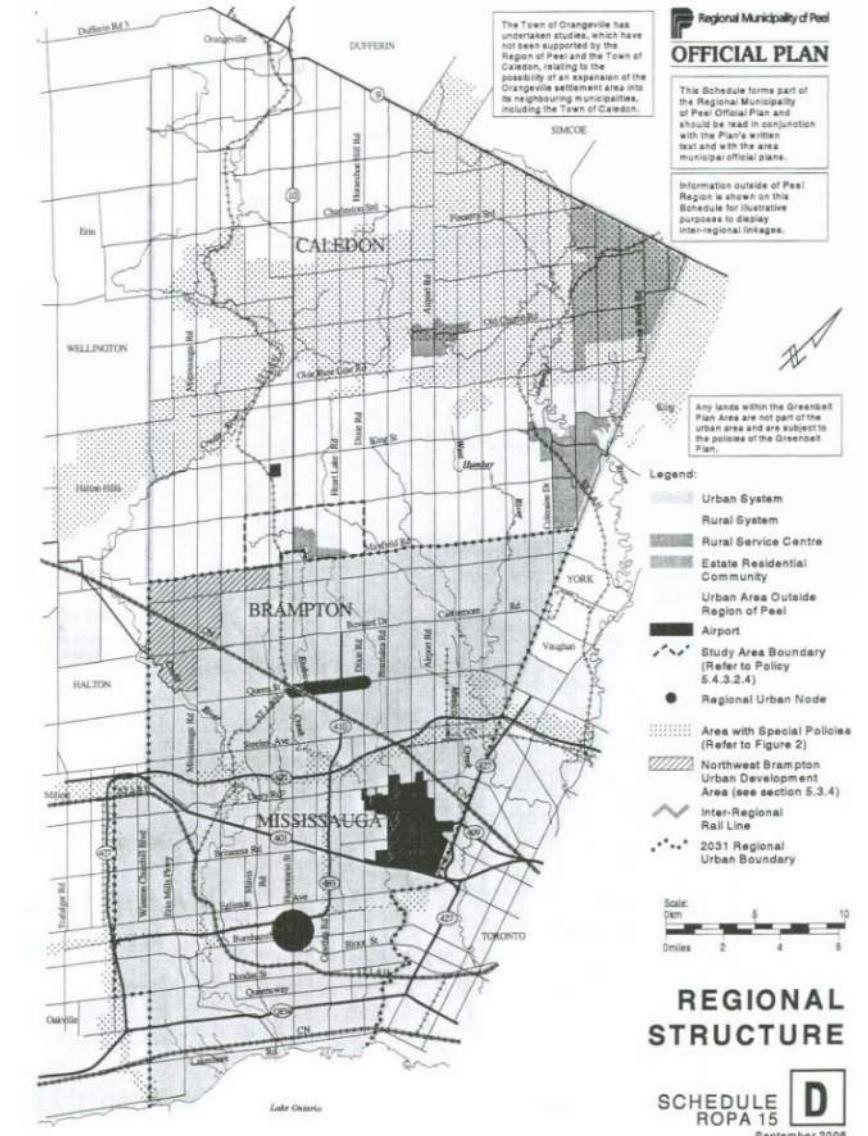
Existing Built and Planned Constraints

- Highway 413 Transportation Corridor (Planned)
- TransCanada Pipeline
- CN Rail Line (Kitchener GO Line)
- Metrolinx Heritage Road Layover Facility
- Peel Region Pumping Station

Constraints
— Highway 413 & Transitway Preliminary Right-of-Way #
—+— CN Rail Line
— TransCanada Pipeline
— Metrolinx Heritage Road Layover Facility
— Peel Region Pumping Station



- Much of the City of Brampton is comprised of existing neighbourhoods that are largely built out. Aside from intensification in Strategic Growth Areas, new development will occur in the City's remaining greenfield areas, the largest of which is the Heritage Heights Secondary Plan area.
- In the **Region of Peel Official Plan, the entirety of the Heritage Heights Secondary Plan area – with the exception of the Greenbelt lands – is designated as “Urban System” in Schedule E-1 (Regional Structure).**
 - The “Urban System” designation has been in effect since 2006, originally legislated by City of Brampton Official Plan Amendment OP93-245 and Peel Region Official Plan Amendment 15 (ROPA 15), and subsequently approved by the Ontario Municipal Board (OMB).

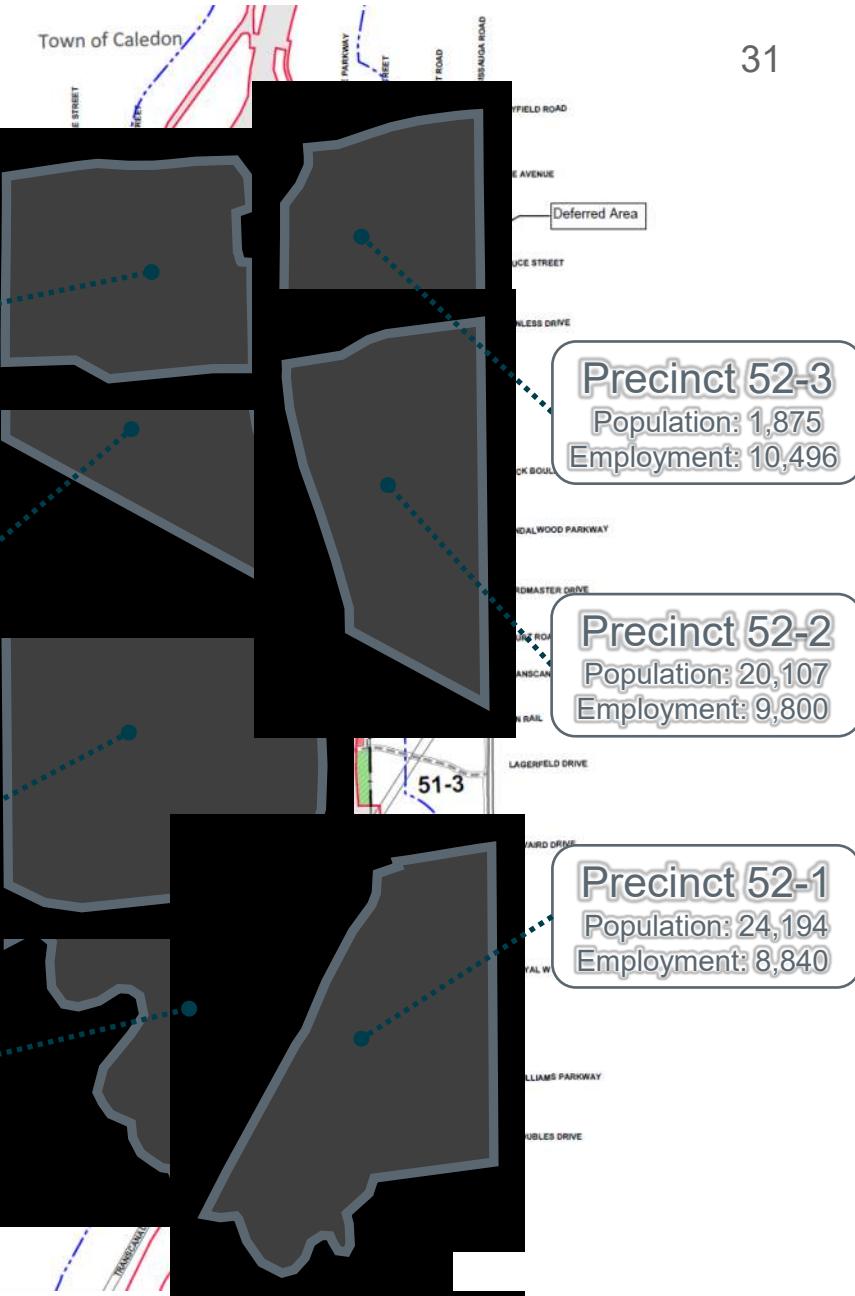
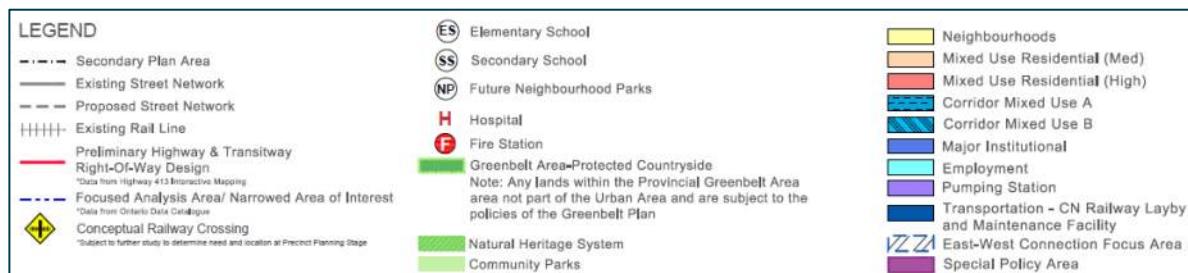


What's Planned for Heritage Heights?

“Seven distinct Precincts, which will accommodate both people and jobs through a full range of uses including employment, mixed-use, institutional and civic, and various housing forms, tenures and types” – Section 2, “Vision and Guiding Principles” - Heritage Heights Secondary Plan

Heritage Heights Secondary Plan Area is planned to accommodate **132,188 people** and **54,307 jobs**

“A connected system of green corridors and pedestrian, cycling, and street networks will promote walkability, accessibility, and connections to transit, and local and regional destinations” – Section 2, “Vision and Guiding Principles” - Heritage Heights Secondary Plan



Testing the Proposed Secondary Plan Road Network

- The Proposed Secondary Plan road network was tested based on planned growth in population and employment to identify:
 - **Critical constraints in transportation capacity** that call for new infrastructure or infrastructure improvements, and
 - **Opportunities to respond to known environmental, built, and planned features** through:
 - Road re-alignments and
 - Re-evaluation of Road, Rail, and Environmental Crossings

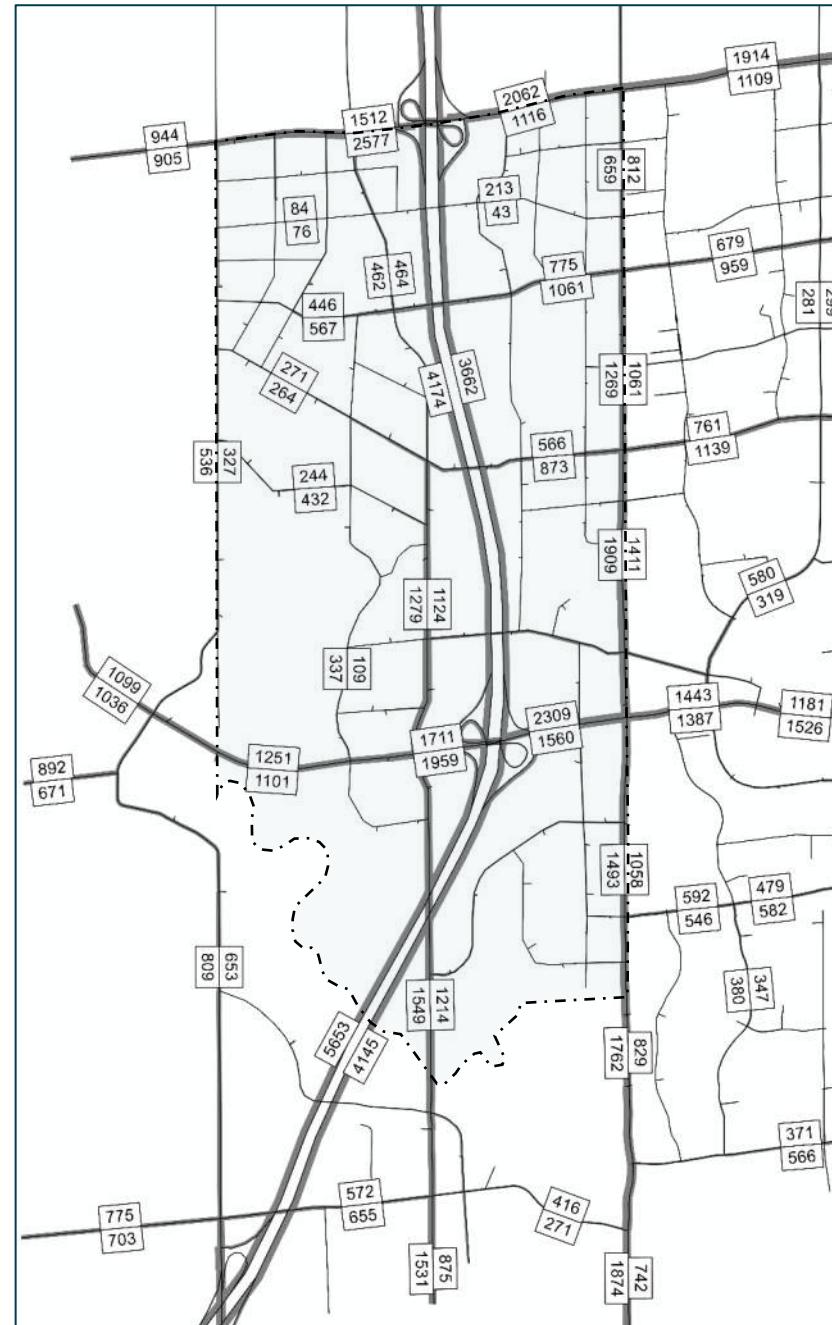


Future Traffic Operations 2051, Morning Peak Hour

Forecasted traffic operations during the **weekday morning peak hour** in the Secondary Plan area are generally within acceptable modelling limits

Over-capacity road segments:

- Mayfield Road at Highway 413 (between Pinnacle Parkway and Heritage Road)
- Bovaird Drive between Highway 413 and Heritage Road
- Bovaird Drive/Highway 7 west of Halton-Peel Boundary

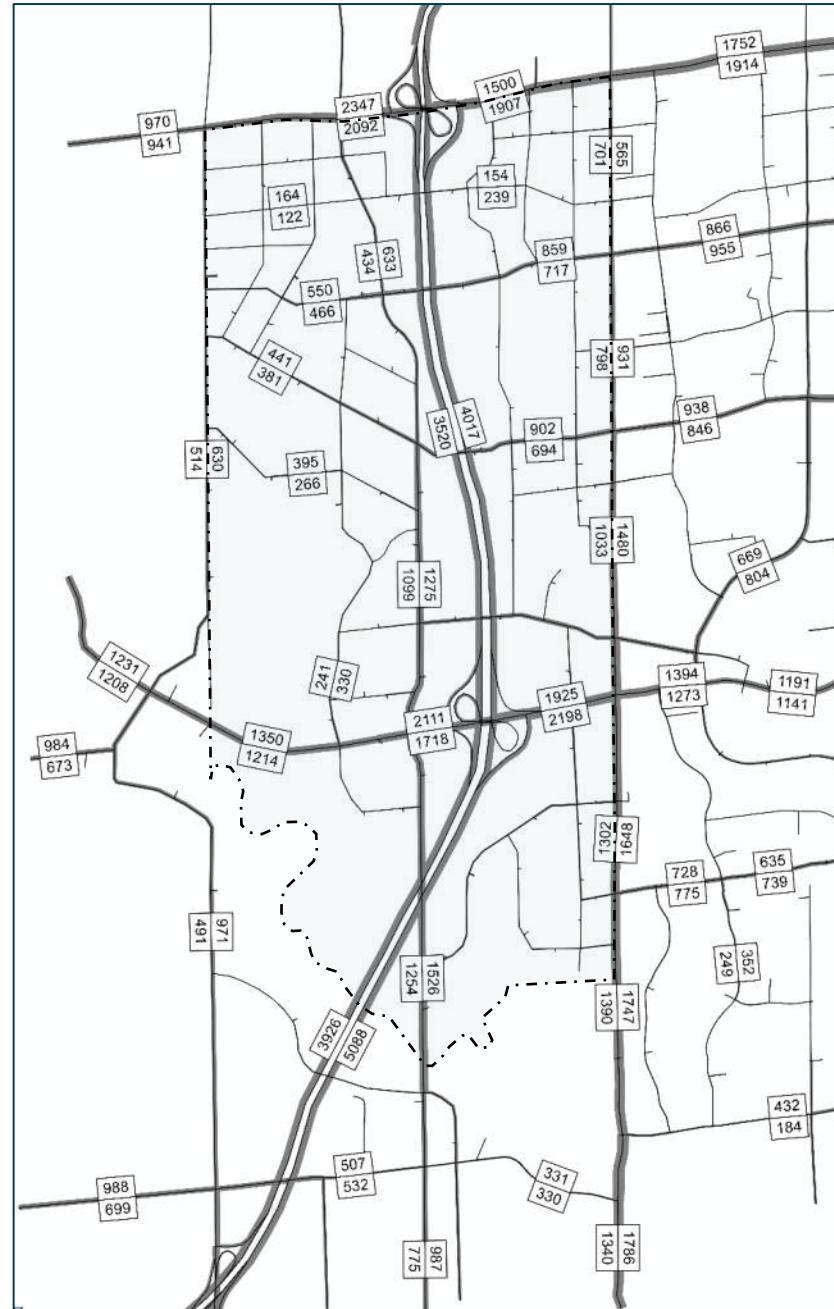


Future Traffic Operations 2051, Afternoon Peak Hour

Forecasted traffic operations during the **weekday afternoon peak hour** in the Secondary Plan area are generally within acceptable modelling limits

Over-capacity road segments:

- Mayfield Road at Highway 413 (between Pinnacle Parkway and Heritage Road)
- Bovaird Drive between Highway 413 and Heritage Road
- Bovaird Drive/Highway 7 west of Halton-Peel Boundary

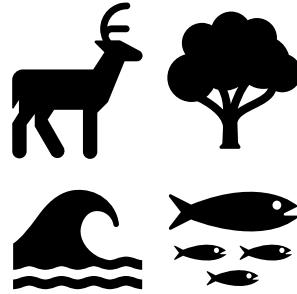


Future Weekday Afternoon Peak Hour - Corridor Traffic Volumes (per hour)

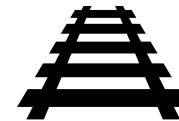


Future Weekday Afternoon Peak Hour - Corridor Volume-to-Capacity Ratios

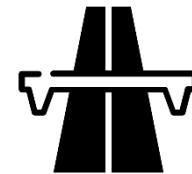
- There are three (3) types of major physical opportunities and constraints that influence changes in the Secondary Plan road network alignment:



**Crossing Natural Heritage
Systems: Watercourses,
Wetlands, Terrestrial Habitats**



**Crossing the Railway and
the TransCanada Pipeline**

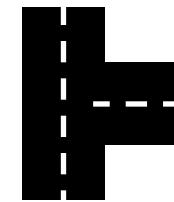


**Crossing Future
Highway 413**

- Alternatives to the Secondary Plan road network to address traffic capacity and environmental considerations were categorized as:



Road Widening



**Road Alignment
Alternatives**



**Refinements to Road
Crossings (of natural and
built environment features)**

Network Alternatives

Road Widenings

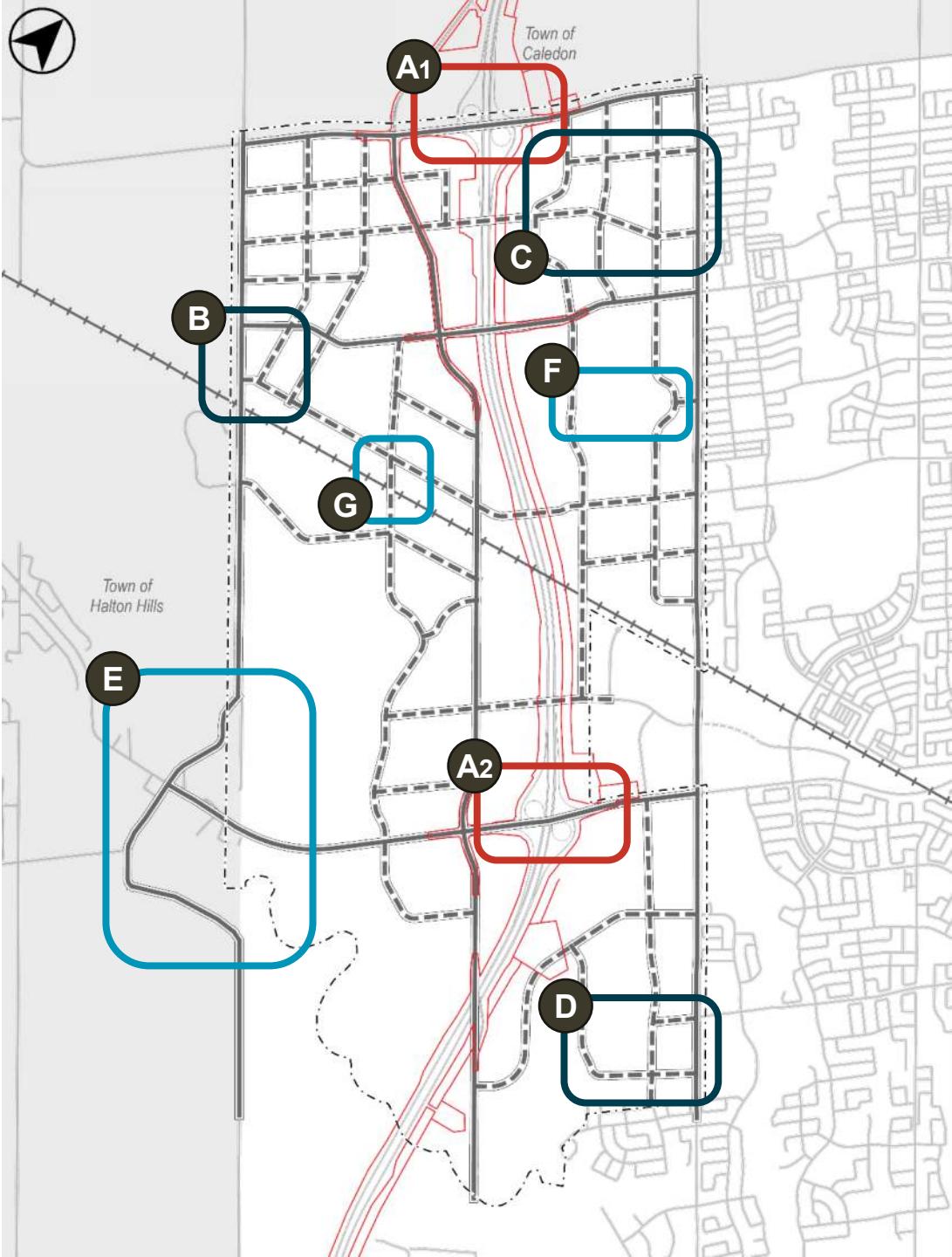
- A. Bovaird Drive & Mayfield Road Lane Widening

Road Alignments

- B. Sandalwood Parkway & Winston Churchill Boulevard (52-6 / 52-7)
- C. Precinct 52-3 Road Network
- D. Williams Parkway & Doubles Drive (52-1)

Crossings of Existing or Planned Constraints

- E. East-West Connection Focus Area (Inter-Regional)
- F. Buick Boulevard (52-2)
- G. North-South Rail Crossing (52-5 / 52-6)



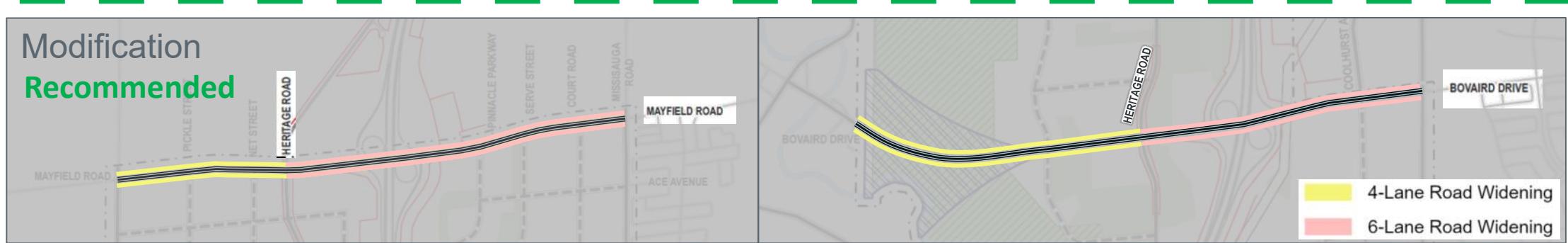
The **Long List** of transportation network modifications to the Secondary Plan has been reviewed and screened as:

- **Recommended:** Addresses Transportation Master Plan Problem Statement with similar or less impacts to the Secondary Plan transportation network, can be adopted and will not require further evaluation
- **Carried Forward for Further Evaluation:** Addresses the Transportation Master Plan Problem Statement but requires further Environmental Evaluation
- **Not Recommended:** Does not address the Transportation Master Plan Problem Statement or improve on the Secondary Plan transportation network

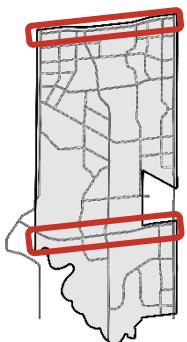
A **Short List** of transportation modifications screened as **Carried Forward for Further Evaluation** has been developed for evaluation through the Municipal Class Environmental Assessment process.

Long List Alternative A: Bovaird Drive and Mayfield Road (Regional Roads)

39



- Description of Modification:** Widenings to 6-lanes along Mayfield Road and Bovaird Drive extended westward to Heritage Road
- Description of Need/Impacts:**
Required to accommodate high east-west volumes travelling to/from to the Highway 413 interchanges. Widenings in this area are also under consideration by the Region's Long Range Transportation Plan, Completed, and Ongoing Municipal Class Environmental Assessment **Widening Recommended**

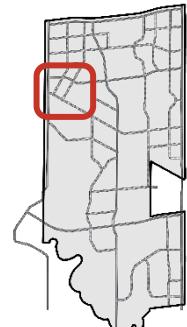


Long List Alternative B: Sandalwood Parkway & Winston Churchill Boulevard

40



- **Description of Modification:**
Re-alignment of the planned Sandalwood Parkway terminating at Wanless Drive.
- **Description of Need/Impacts:**
Grading, Fill, environmental impacts related to Sandalwood Parkway intersecting future elevated Winston Churchill Boulevard (CN Railway crossing).
- Negligible impact to traffic operations.
- Precinct 52-6 and 52-7 roads also aligned to normalized intersection angles and curvature.
- **Realignment Modification Recommended**



Long List Alternative C: Precinct 52-3 Road Network

41

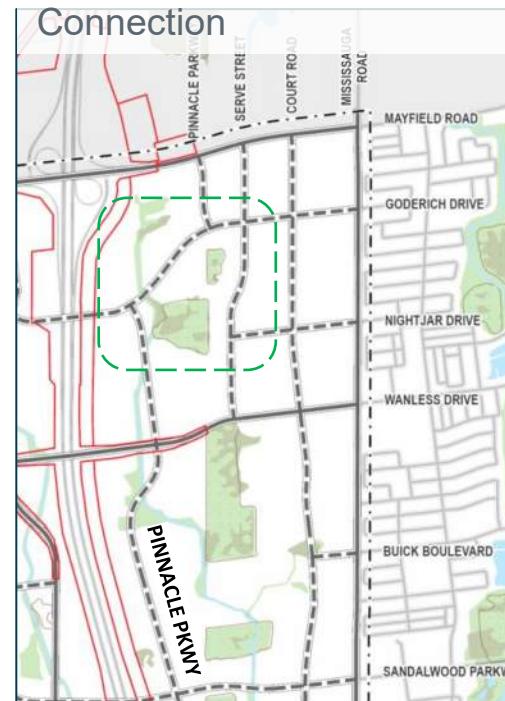
Secondary Plan



Option 2 – Continuous NS Connection

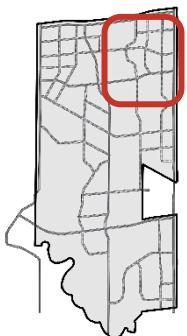


Option 3 – Continuous EW Connection

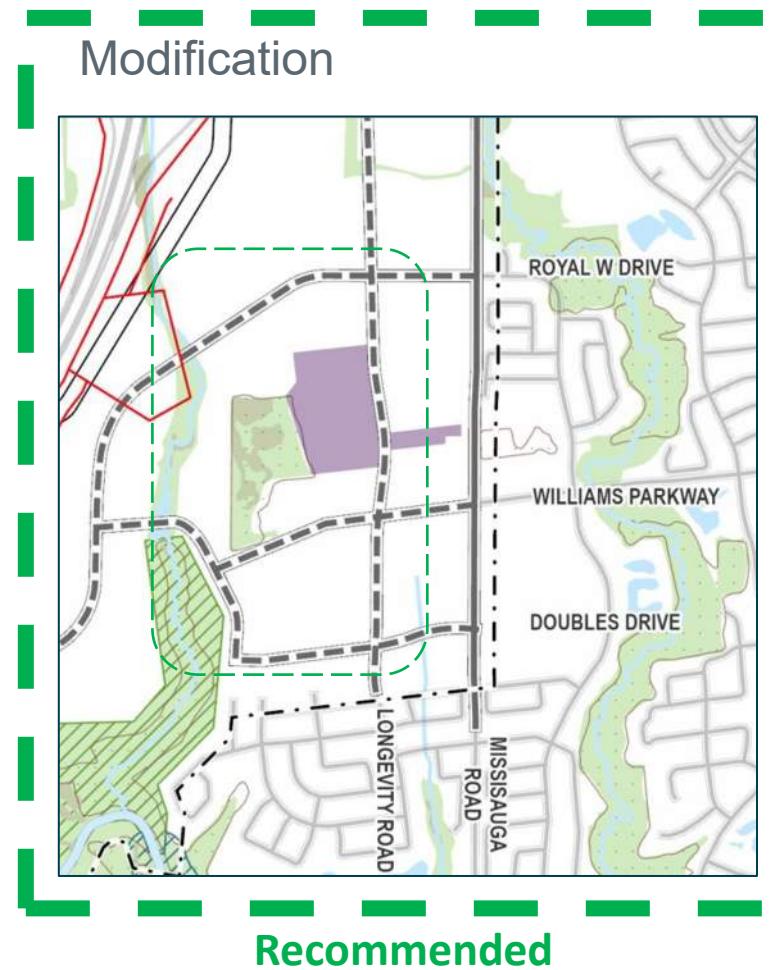


All Options
Carried
Forward for
Further
Evaluation

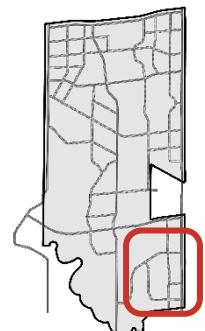
- Description of Modification:** Realignment of Nightjar Drive (Deuce Street), extension of Goderich Drive (Ace Avenue), and realignment of Pinnacle Parkway to avoid wetland features.
- Description of Need and/or Impacts:** Need to maintain Pinnacle Parkway and Court Road north-south connectivity to Mayfield Road and east-west employment connectivity across Highway 413. Continuous Grid Network compromised by realignment although environmental features are avoided.
- All Options Carried Forward for Further Evaluation** given natural heritage impacts and connectivity needs.



Secondary Plan

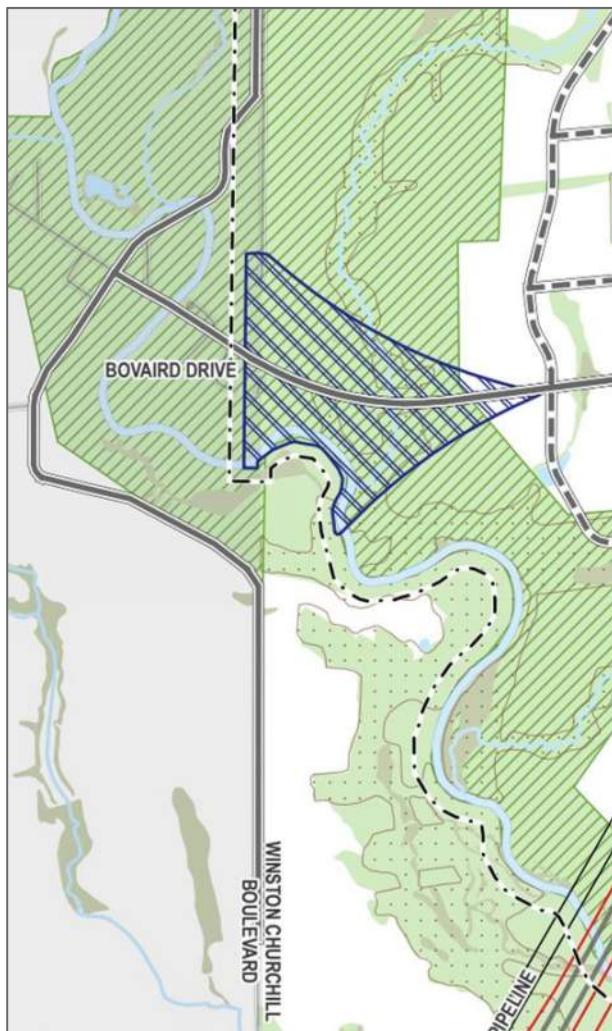


- **Description of Modification:**
Realignment of Doubles Drive and Williams Parkway reflecting emerging Precinct Planning
- **Description of Need/Impacts:**
Negligible impact to traffic operations. Increased connectivity to west Precinct.
- Similar environmental impacts to Secondary Plan road network.
Avoids road impacts next to woodland.
- **Realignment Modification Recommended**



Long List Alternative E: East-West Connection Focus Area (Inter-Regional)

43



Secondary Plan

Widening, By-Pass or Do Nothing Carried Forward for Further Evaluation

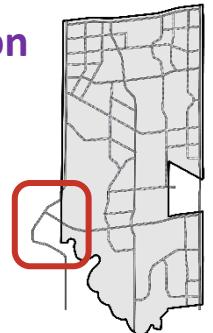
Table 7-21: East-West Options Evaluation

Indicators/Options	Option 1	Option 2	Option 3	Option 4
DO NOTHING – No additional East-West Connection	Not Recommended	New Link South of Norval from 10 Sideroad to Bovaird	New Link to from 10 Sideroad to Williams Parkway	New Link North of Norval to Bovaird
Transportation Service Impacts	Circle	Circle	Circle	Circle
Impacts on Natural Environment	Circle	Circle	Circle	Circle
Community Impacts	Circle	Circle	Circle	Circle
Economic Impacts	Circle	Circle	Circle	Circle
Construction Costs	Circle	Circle	Circle	Circle
Overall	Not Recommended	Carry Forward to EA Study	Not Recommended	Carry Forward to EA Study



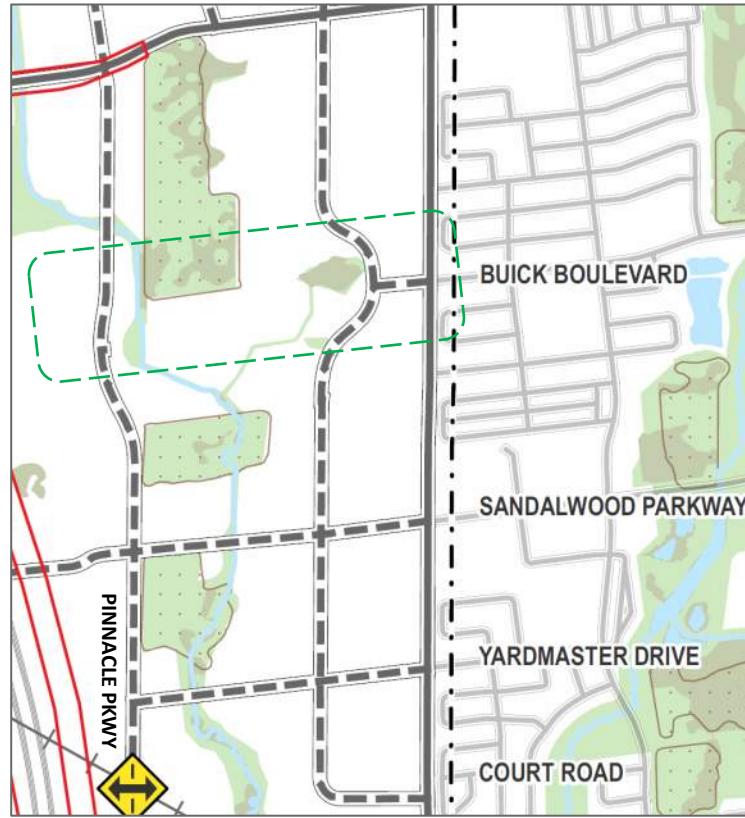
Source: 2010 HPBATS Study

- **Description of Modification:** Widening, By-Pass, or Do Nothing to address potential future traffic capacity constraints.
- **Description of Need/Impacts:** Modelling indicates capacity constraints in 2051 conditions. Modifications addressing capacity occur within Norval community and Greenbelt at inter-jurisdictional boundary
- Widening, By-Pass, and Do Nothing Alternatives Carried Forward for Further Evaluation given significant natural and cultural heritage impacts of potential alternatives

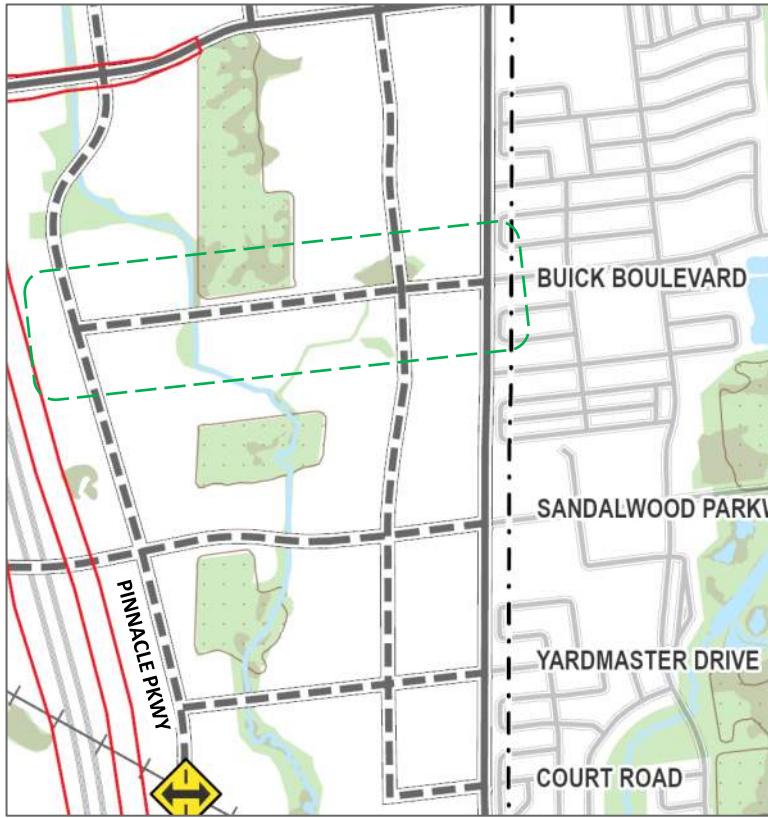


Long List Alternative F: Extension of Buick Boulevard (Crossing Natural Heritage System) 44

Secondary Plan

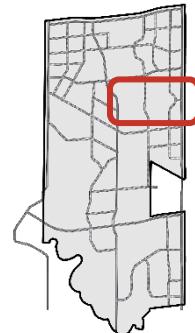


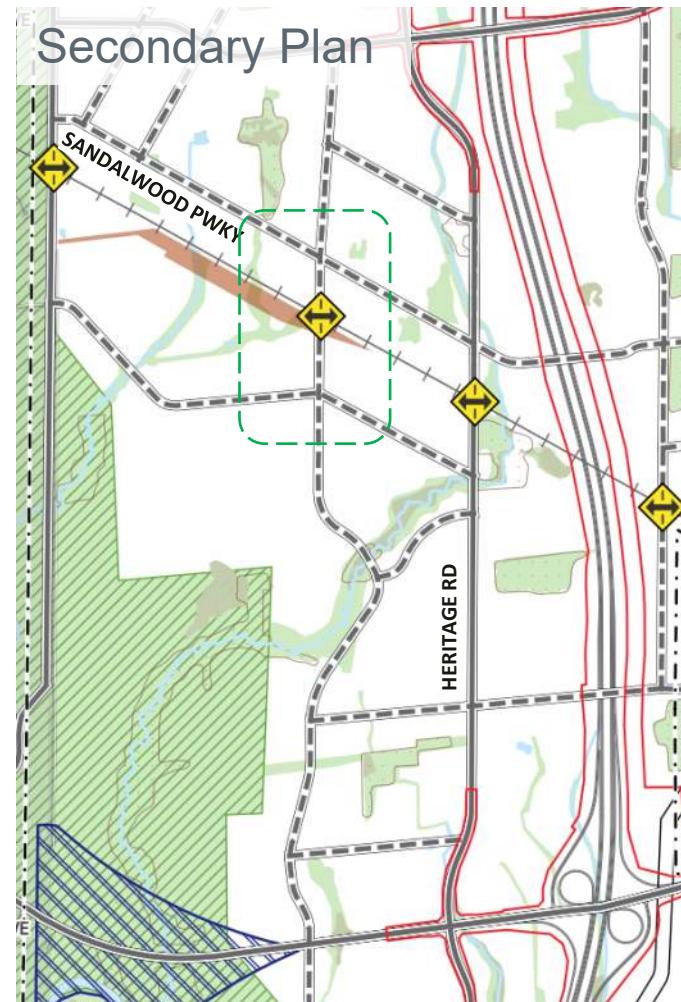
Modification



- **Description of Modification:** Extension of Buick Boulevard (Crossing Natural Heritage) to Pinnacle Parkway
- **Description of Need/Impacts:** Addresses fine-grained street connectivity. Crosses North-South watercourse, wetland, woodland, wildlife habitat.
- **Both Options Carried Forward for Further Evaluation** given significant natural heritage impacts of potential alternatives.

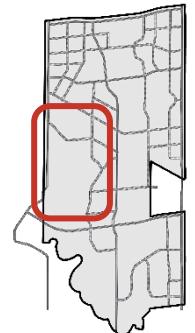
Both Options **Carried Forward for Further Evaluation**





Both Options Carried Forward for Further Evaluation

- **Description of Modification:**
Re-evaluation of Secondary Plan CN railway crossing and related road alignments.
- **Description of Need/Impacts:**
Addresses fine-grained street connectivity between arterial crossings.
- Crosses Metrolinx Heritage Road Layover Facility, impacts grading, drainage, developable area, and existing Headwater Drainage Feature
- Both Options **Carried Forward for Further Evaluation** given natural features, development impacts, and Metrolinx Heritage Road Layover Facility.

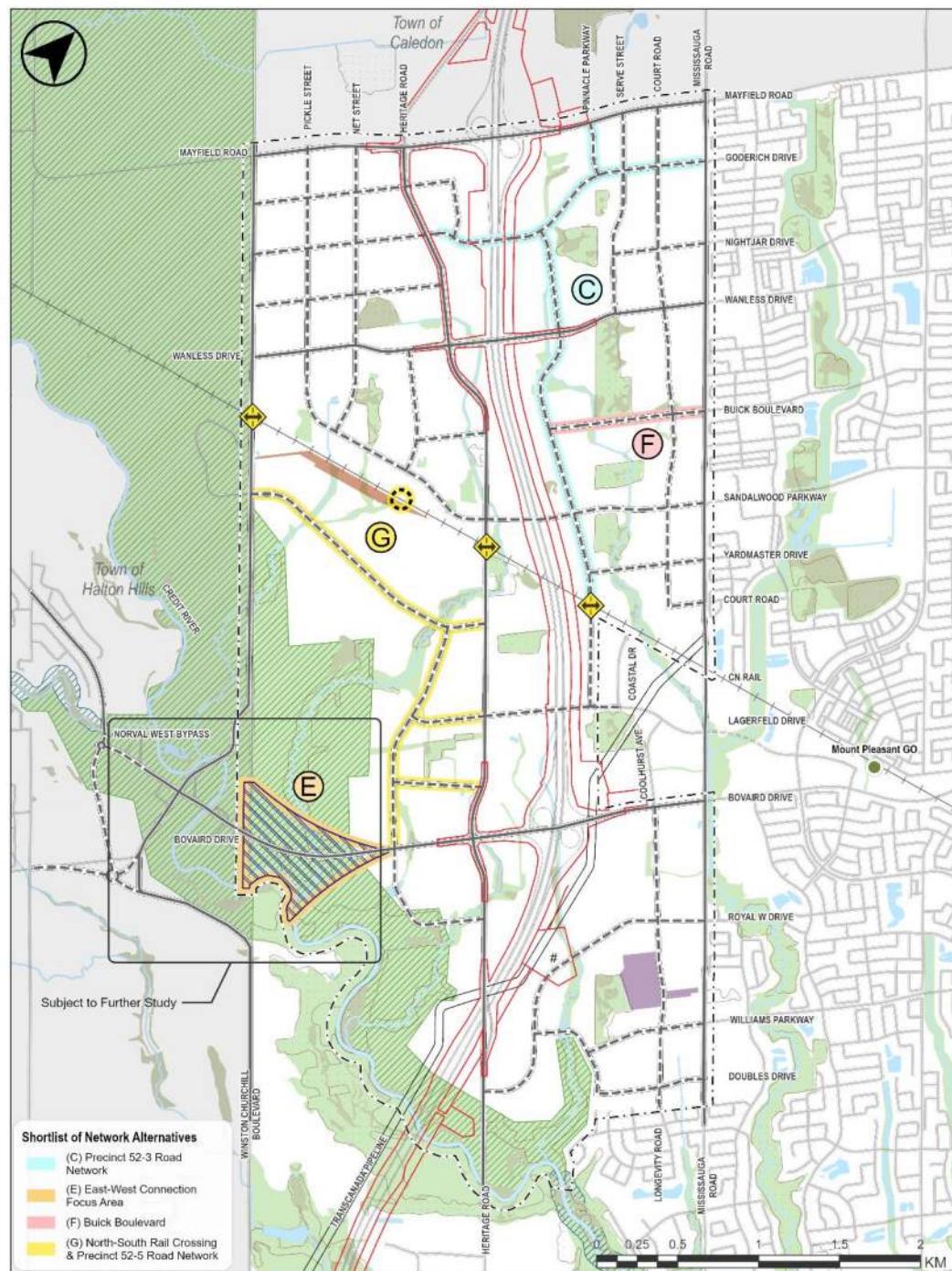


#	Long List Network Alternative	Type	Description	Assessment Result
A	Bovaird Drive & Mayfield Road Lane Widening (Regional Roads)	Road Widening	Widenings to 6-lanes along Mayfield Road and Bovaird Drive extended westward to Heritage Road	Recommended Adopted. Does not require further evaluation.
B	Sandalwood Pkwy & Winston Churchill Blvd (52-6, 52-7)	Road Alignment	Re-alignment of the planned Sandalwood Parkway terminating at Wanless Drive.	Recommended Adopted. Does not require further evaluation.
C	Precinct 52-3 Road Network	Road Alignment	Realignment of Nightjar Drive (Deuce Street), extension of Goderich Drive (Ace Avenue), and realignment of Pinnacle Parkway to avoid wetland features.	Carried Forward for Further Evaluation
D	Williams Extension / Doubles Drive (52-1)	Road Alignment	Realignment of Doubles Drive and Williams Parkway reflecting emerging Precinct Planning	Recommended Adopted. Does not require further evaluation.
E	East-West Connection Focus Area (Inter-Regional)	Crossings of Existing or Planned Constraints	Continuation of previously studied Norval Hamlet 'East-West Connection Focus Area' as identified in the 2010 Halton-Peel Boundary Area Transportation Study (HPBATS). Widening, By-Pass, or Do Nothing to address potential future traffic capacity constraints.	Carried Forward for Further Evaluation
F	Extension of Buick Boulevard (52-2)	Crossings of Existing or Planned Constraints	Extension of Buick Boulevard (Crossing Natural Heritage) to Pinnacle Parkway	Carried Forward for Further Evaluation
G	North-South Rail Crossing (52-5 / 52-6)	Crossings of Existing or Planned Constraints	Re-evaluation of Secondary Plan CN railway crossing and related road alignments.	Carried Forward for Further Evaluation

Short List Summary

Short List of Network Alternatives **Carried Forward for Further Evaluation :**

- ④ C Precinct 52-3 Road Network Realignment
- ④ E East-West Connection Focus Area (Inter-Regional Roads)
- ④ F Extension of Buick Boulevard (52-2)
- ④ G North-South Rail Crossing (52-5 / 52-6)



Transportation Criteria

- Enhance sustainable mobility and multi-modal travel options
- Address modelled traffic impacts
- Enhance connectivity for people and goods
- Integrate transportation and land use planning
- Protect public health and safety
- Leverage flexibility for future planning

Cultural Environment Criteria

- Archaeologic
- Cultural and Built Heritage

Natural Environment Criteria

- Terrestrial (species)
- Aquatic (watercourses, species at risk)
- Greenbelt & Natural Heritage Systems
- Stormwater & Surface Water (flooding, erosion)

Socio-Economic Criteria

- Transportation Equity
- Property Impacts/Requirements
- Nuisance Impacts (Noise, Vibration & Air Quality)
- Ease of Construction (Phasing, Geotechnical)
- Cost (Capital, Operating & Maintenance)

Short List Evaluation - Sample

- Each Short List Alternative was reviewed in detail based on the Environmental Assessment Criteria for Evaluation
- Evaluation of Transportation, Cultural Environment, Natural Environment, and Socio-Economic criteria determined an overall Recommendation for each Short List Alternative

Least Preferred

More/Less Preferred

Most Preferred

Evaluation Sample

Category	Measure	Option 1 – Do Nothing	Transportation Criteria			
			Option 2 – North Bypass	Option 3 – South Bypass	Option 4 – Widen Highway 7	
Enhance sustainable mobility and multi-modal travel options	Enhances public transit connectivity		Provides dedicated additional transit capacity in addition to maintaining Highway 7 through Norval.	Provides dedicated additional transit capacity in addition to maintaining Highway 7 through Norval. Not as effective as other options in addressing routing options of desired routes.	Provides transit capacity in mixed traffic with widening of Highway 7 through Norval.	
	Enhances Active transportation connectivity and Micromobility options		Provides dedicated additional active transportation routes in addition to maintaining Highway 7 through Norval.	Provides dedicated additional active transportation routes in addition to maintaining Highway 7 through Norval. Not as effective as other options in addressing routing options of desired routes.	May provide opportunity for dedicated active transportation with widening of Highway 7 through Norval.	
Traffic Impacts	Traffic operations & capacity impacts	Does not support the modelled traffic activity and EA Problem Statement	Provides the most effective relief of Norval traffic destined between Georgetown and Brampton.	Provides relief of Norval traffic destined between Georgetown and Brampton. Not as effective as other options in addressing capacity constraints of desired routes.	Traffic modelling shows the greatest operational improvements. However, traffic demand through Norval is induced with greater lanes.	
	Enhance connectivity for people and goods	Enhances General travel routing	Provides the most effective relief of Norval traffic destined between Georgetown and Brampton.	Provides relief of Norval traffic destined between Georgetown and Brampton. Not as effective as other options in addressing routing options of desired routes.	Traffic modelling shows the greatest operational improvements. However, traffic demand through Norval is induced with greater lanes.	
	Freight routing impacts		Provides dedicated goods movement corridor in addition to maintaining Highway 7 through Norval.	Provides dedicated goods movement corridor in addition to maintaining Highway 7 through Norval. Less direct than Option 2.	Provides goods movement in mixed traffic with widening of Highway 7 through Norval.	
Integrate transportation and land use planning	Compatibility with existing and proposed land uses.		Impact to existing Greenbelt and existing landowners north of Highway 7 / Bovaird Drive. (e.g. Old Pine Crest Road). Acts as a bypass of land uses in future, e.g. no interaction with future land uses.	Impact to existing Greenbelt and existing landowners south of Highway 7 / Bovaird Drive. Acts as a bypass of land uses in future, e.g. no interaction with future land uses.	Potential to impact existing land uses through Norval. Widening interacts with adjacent existing and future land uses.	
Protect public health and safety	Prioritizes vulnerable road users		Provides dedicated additional active transportation routes in addition to maintaining Highway 7 through Norval. Opportunities for enhanced pedestrian crossings.	Provides dedicated additional active transportation routes in addition to maintaining Highway 7 through Norval. Opportunities for enhanced pedestrian crossings. Limited opportunities for improvement for desired routes compared to other options.	Illustrative comparison Further detailed study recommended.	
	Promotes active living		Provides dedicated additional active transportation routes in addition to maintaining Highway 7 through Norval.	Provides dedicated additional active transportation routes in addition to maintaining Highway 7 through Norval. Limited opportunities for improvement for desired routes compared to other options.	May provide opportunity for dedicated active transportation with widening of Highway 7 through Norval. Opportunities for enhanced pedestrian crossings. Induced traffic demand with widening.	
Leverage flexibility for future planning	Protects for long range vision of Secondary Plan and flexible land use planning in adjacent Precincts		Provides options for routing that can respond to several land use scenarios.	Provides options for routing that can respond to several land use scenarios. Not as effective as other options in addressing routing options of desired routes.	May provide opportunity for dedicated active transportation with widening of Highway 7 through Norval. More direct than Option 3.	
Transportation Overall	Screened Out		Adjacent Precincts protect for connection	Adjacent Precincts protect for connection.	Options for widening would be closely tied to impacts/interface with existing Norval, while also providing new capacity, limiting flexibility.	
			Most Preferred	Less Preferred	Adjacent Precincts protect for connection.	More Preferred

Short List C: Precinct 52-3 Road Network

50



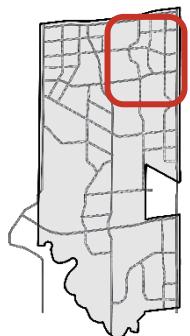
Secondary Plan



Option 2:
Continuous North-South
Connection



Option 3:
Continuous East-West
Connection

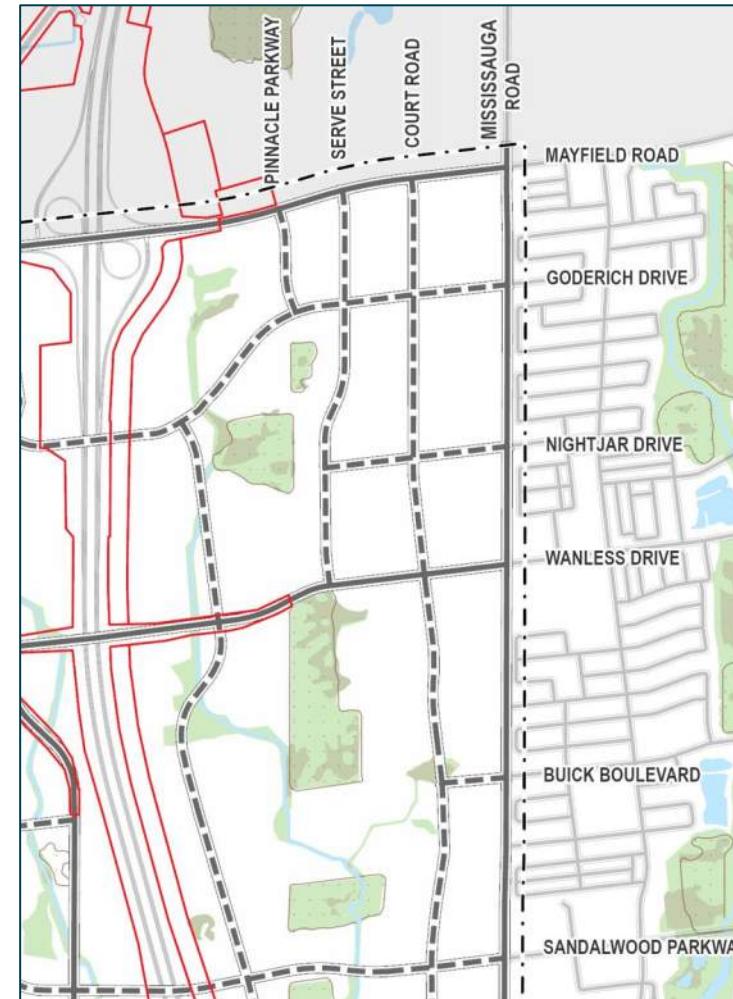


Precinct 52-3 Road Network

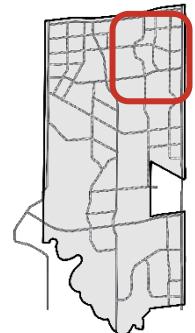
Categories	Option 1 – Secondary Plan Road Network	Option 2 – Continuous North-South Connection	Option 3 – Continuous East-West Connection
Transportation	<p>Provides the finest-grained connectivity for vehicular capacity, routing options, and transit connectivity.</p> <p>Provides two continuous north-south to/from the future highway and provincial transitway and one continuous east-west collector between employment areas.</p>	Maintains N-S transit connectivity. Introduces additional intersections along Goderich that breaks up continuity of east-west collector between employment areas.	<p>Maintains E-W transit connectivity. Introduces additional intersections along Pinnacle Parkway that breaks up continuity of north-south collector to/from the future highway and provincial transitway.</p> <p>Provides a continuous parallel traffic and transit relief corridor to Mayfield Road and Wanless Drive.</p>
Cultural Environment	Cultural Heritage Lands 2 and 4 (farmscape with brick farmhouses) crossed by future collector roads. To be mitigated/managed through detailed design.	Cultural Heritage Lands 2 and 4 (farmscape with brick farmhouse) crossed by future collector roads. To be mitigated/managed through detailed design.	Cultural Heritage Lands 2 and 4 (farmscape with brick farmhouse) crossed by future collector roads. To be mitigated/managed through detailed design.
Natural Environment	Pinnacle Parkway and Nightjar Drive extension crosses through wetland, woodlot, and Natural Heritage System that occupies a large portion of the west side of Precinct 52-3.	Eliminates E-W collector road going through wetland features and related buffer. No change to number of watercourse crossings compared with other options.	Eliminates E-W collector road going through wetland features and related buffer. No change to number of watercourse crossings compared with other options.
Socio-Economic	<p>Provides the finest grained connectivity for people by all modes.</p> <p>Geotechnical impacts related to crossing wetland, woodlot and Natural Heritage feature in Precinct 52-3.</p> <p>Most land acquisition compared to other options.</p>	<p>Greater vehicular activity on parallel corridors (Mayfield Road)</p> <p>Geotechnical impacts related to diversion of watercourse in Precinct 52-2W to match watercourse crossing to Precinct 52-3. No change to number of watercourse crossings compared with other options.</p>	<p>Greater vehicular activity on parallel corridors (Mayfield Road) mitigated by continuous east-west connection between Precinct 52-3 and 52-7 employment areas.</p> <p>Least land acquisition compared to other options.</p> <p>Connects east and west employment</p> <p>Geotechnical impacts related to diversion of watercourse in Precinct 52-2W to match watercourse crossing to Precinct 52-3. No change to number of watercourse crossings compared with other options</p>
Overall			Recommended

Recommendation:

- Maintain a continuous east-west crossing of future Highway 413 (Goderich Drive) between employment areas in Precincts 52-3 and 52-7.
- Maintain north-south collector road connectivity to Mayfield Road, serving Precincts 52-2 and 52-3 to/from future Highway 413 and Provincial Transitway while avoiding bisecting wetland and woodland feature.

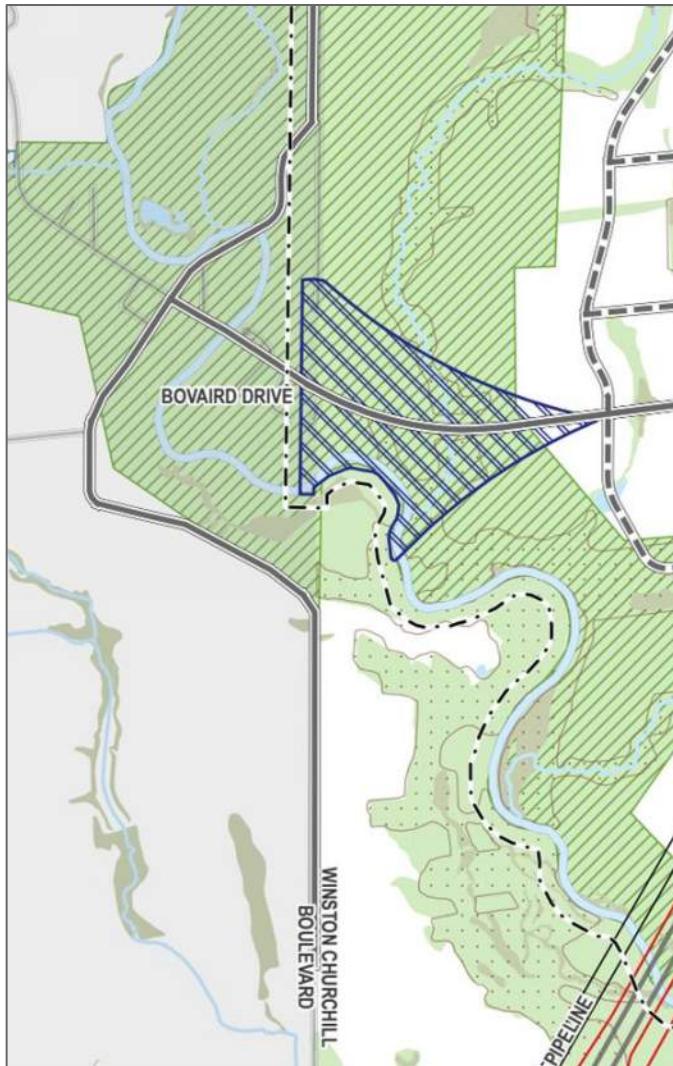


Option 3:
Continuous East-West
Connection

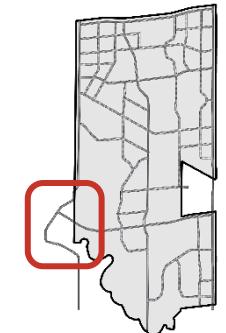
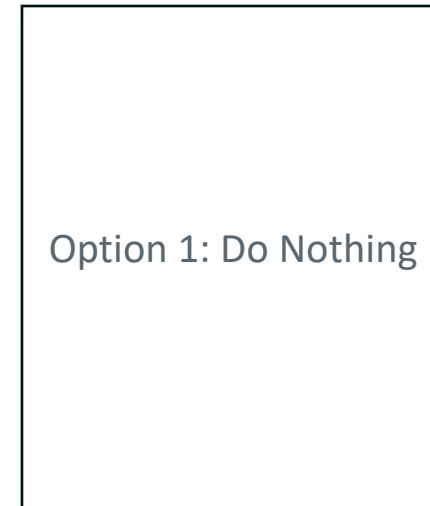


Short List E: East-West Connection Focus Area (Inter-Regional)

53



East-West Connection Focus Area



East-West Connection Focus Area (Inter-Regional) – Evaluation of Alternative Solutions Cont'd

54

Categories	Option 1 – Do Nothing	Option 2 – North Bypass	Option 3 – South Bypass	Option 4 – Widen Highway 7
Transportation	Screened Out Does not support the modelled traffic activity and EA Problem Statement	Provides dedicated facility for all modes, goods, and land uses.	Less direct and less effective traffic relief than North Bypass and Widening Options.	Relies on providing future capacity for all modes in mixed traffic through widening current Highway 7.
Cultural Environment	-	Avoids potential impacts to Cultural Heritage in Norval. Potential for archaeological impacts in Credit River Valley. <i>Further study required as part of future Class EA*</i>	Avoids potential impacts to Cultural Heritage in Norval. Potential for archaeological impacts in Credit River Valley. <i>Further study required as part of future Class EA*</i>	Impacts known Cultural Heritage in Norval. Potential for archaeological impacts in Norval. <i>Further study required as part of future Class EA*</i>
Natural Environment	-	Entirely within Greenbelt limits. High potential for impacts to the Credit Valley <i>Further study required as part of future Class EA*</i>	Entirely within Greenbelt limits. High potential for impacts to the Credit Valley <i>Further study required as part of future Class EA*</i>	Avoids impacting undisturbed areas of the Credit River Valley. <i>Further study required as part of future Class EA*</i>
Socio-Economic	-	Diverts traffic and related noise/air quality impacts to new route. High cost and geotechnical impacts associated with unique Credit River Valley crossing. <i>Further study required as part of future Class EA*</i>	Provides less effective diversion than North Bypass. High cost and geotechnical impacts associated with unique Credit River Valley crossing. <i>Further study required as part of future Class EA*</i>	Impacts Cultural Heritage properties in Norval. Directs traffic and related noise/air quality through Norval. Costs related to widening within existing Norval. Potential for moving, impacting or restoring existing land uses. <i>Further study required as part of future Class EA*</i>
Overall	Screened Out	No Recommendation – Further Study Required	No Recommendation – Further Study Required	No Recommendation – Further Study Required

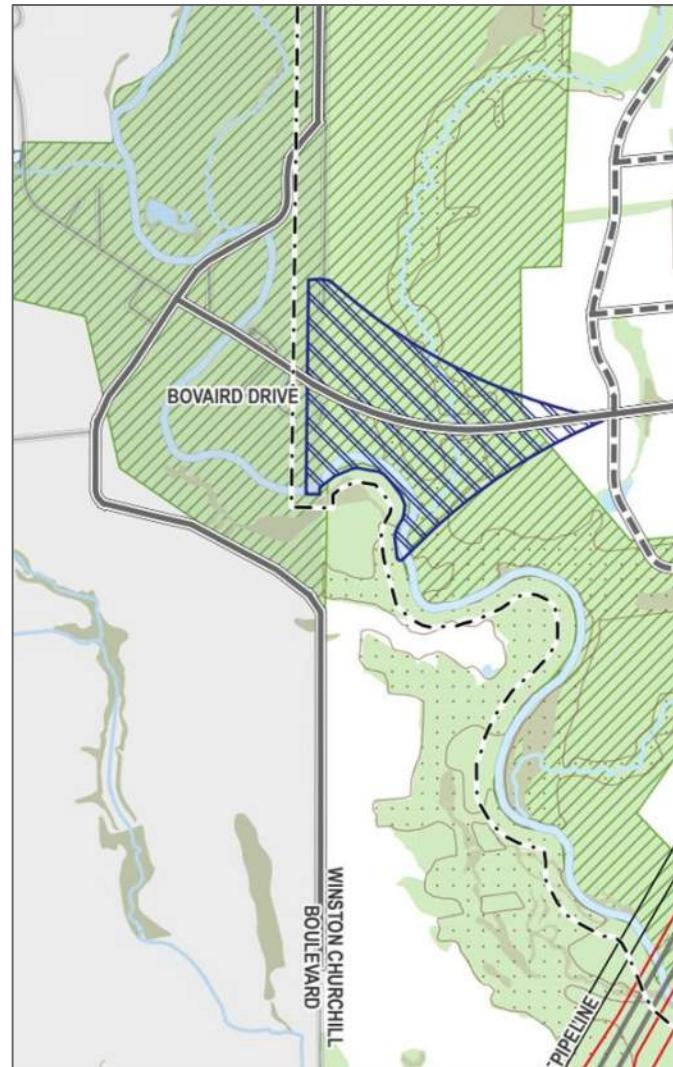
*Potential Cultural Environment, Natural Environment, and Socio-Economic Environment impacts reach outside of Secondary Plan Limits study area. EA evaluation shown for illustrative purposes.

Short List E: East-West Connection Focus Area (Inter-Regional)

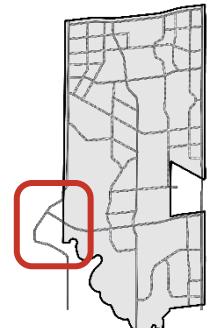
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Recommendation:

- Consistent with the findings of 2010 HPBATS study, future modelled traffic re-confirms capacity constraints are recommended to be addressed through Norval.
- Further detailed study within Norval and the Credit Valley (within the Greenbelt) and west of the Secondary Plan limits is required to fully assess Cultural Environment, Natural Environment, and Socio-Economic Environment criteria to determine a preferred solution.
- The East-West Connection Focus Area is recommended to be protected for to allow a future Schedule C EA to determine a preferred solution.

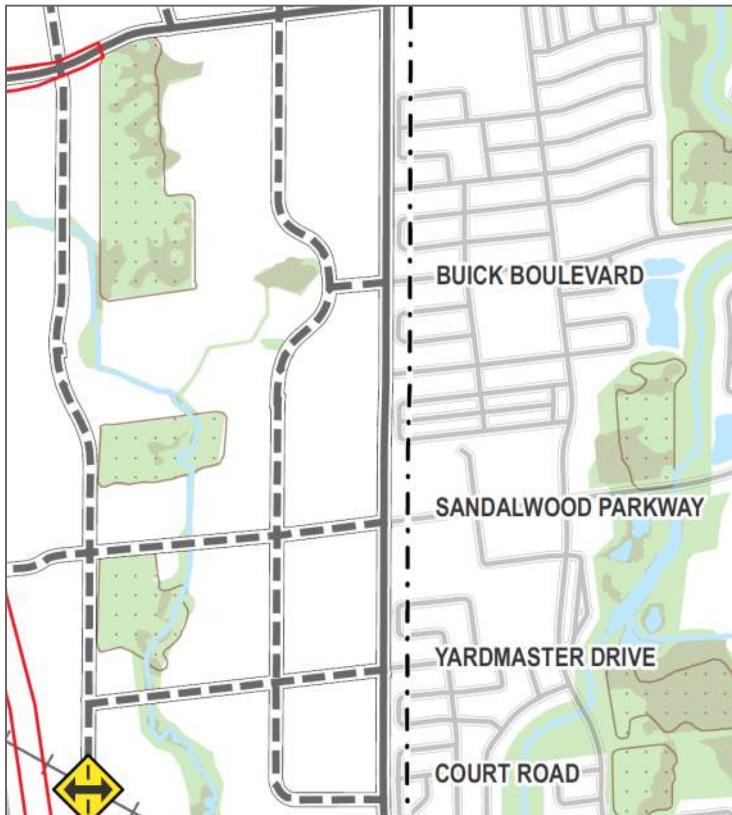


East-West Connection Focus Area



Short List F: Extension of Buick Boulevard (52-2)

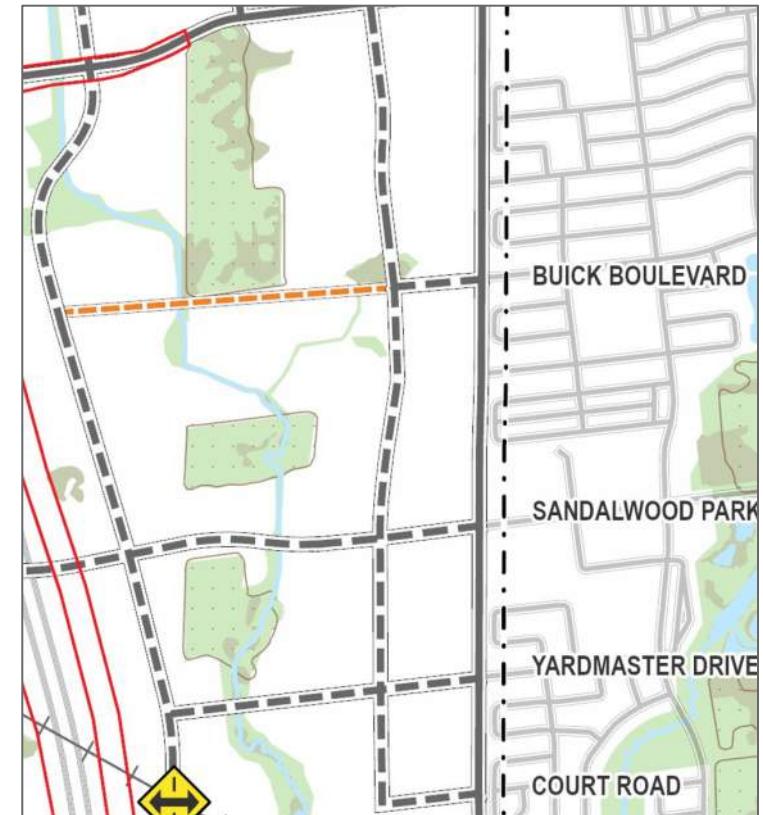
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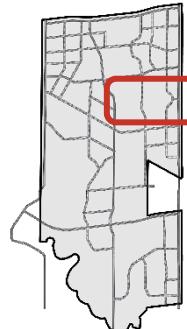
Option 1:
Secondary Plan Road Network
(Sandalwood + Yardmaster)



Option 2:
Secondary Plan + Buick



Option 3:
Secondary Plan +
Buick Active Transportation Only



Extension of Buick Boulevard (52-2)

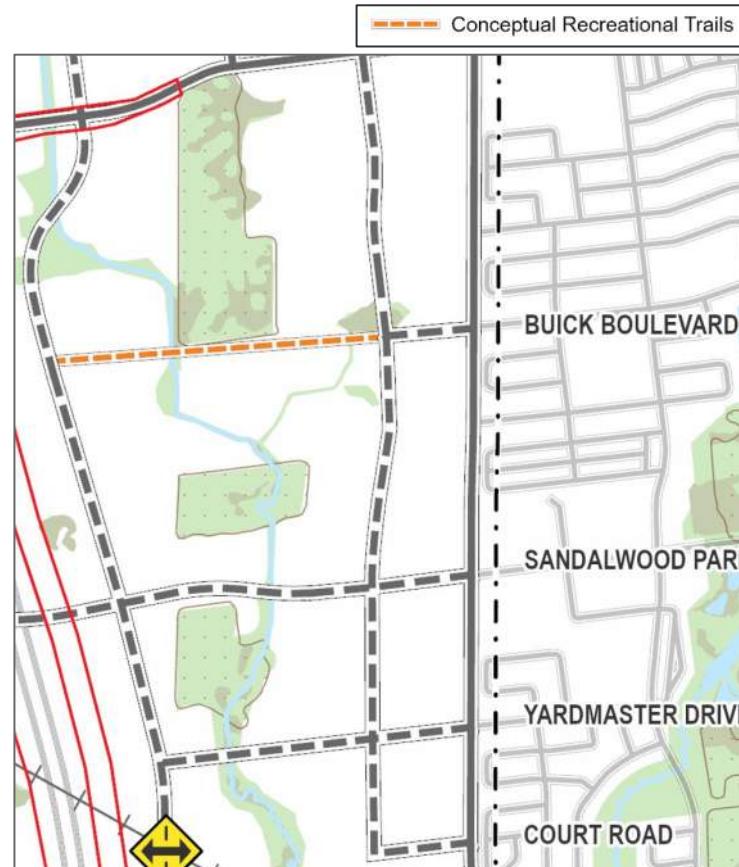
Categories	Option 1 – Secondary Plan Road Network (Sandalwood + Yardmaster)	Option 2 – Secondary Plan + Extension of Buick Boulevard	Option 3 – Secondary Plan + Buick Boulevard Active Transportation Only
Transportation	Less local east-west transportation options for all modes compared to other options.	Provides the finest grain connectivity for collector (transit-friendly) road network	Less east-west transit connectivity compared to Option 2. Provides fine-grained connectivity for walking and cycling.
Cultural Environment	Cultural and Built Heritage areas not identified within study area. Archaeology clearance to be assessed during detailed design.	Cultural and Built Heritage areas not identified within study area. Archaeology clearance to be assessed during detailed design.	Cultural and Built Heritage areas not identified within study area. Archaeology clearance to be assessed during detailed design.
Natural Environment	Avoids natural feature impacts related to extending Buick Boulevard.	Buick Boulevard would directly or indirectly impact: <ul style="list-style-type: none"> watercourse providing seasonal fish habitat and Terrestrial Crayfish Significant Wildlife Habitat. Provincially Significant Wetland (22) Significant woodland, significant wildlife, and species at risk habitat, North-south ecological linkage facilitating wildlife movement 	Avoids major natural feature impacts related to extending Buick Boulevard. <p>Opportunities to explore a variety of designs and crossing locations for active transportation that may avoid or mitigate impacts to property and Natural Heritage System features.</p>
Socio-Economic	Least kilometres and property impacts with some diversion of traffic on parallel corridors.	Would require the greatest overall property and cost to deliver road network, including three natural heritage bridge crossings in Precinct 52-2	Has the same kilometres of road network as Option 1, plus an active transportation corridor that advances transportation equity. Opportunities to explore a variety of designs and crossing locations for active transportation to mitigate both natural impacts and cost.
Overall			Recommended

Short List F: Extension of Buick Boulevard (52-2)

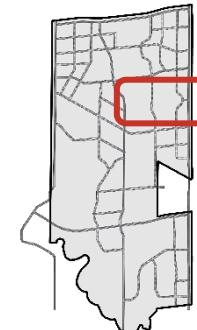
58

Recommendation:

- An active transportation crossing only along the Buick Boulevard alignment between the east and west sides of Precinct 52-2
- Provides fine-grained connectivity for walking and cycling between the east and west sides of Precinct 52-2
- Opportunities to explore a variety of designs and crossing locations for active transportation to mitigate both natural impacts and cost.



Option 3:
Sandalwood + Yardmaster +
Buick Active Transportation Only

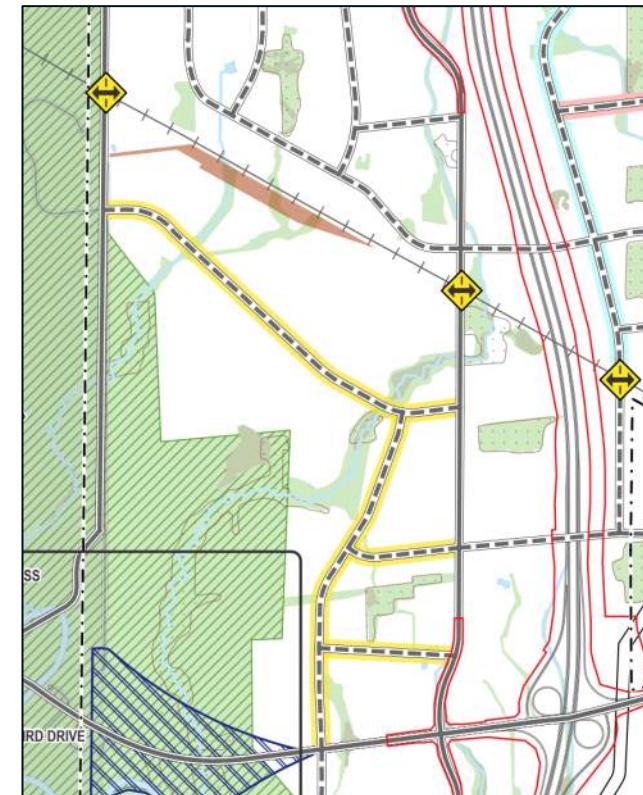


Short List G: North-South Rail Crossing (52-5 / 52-6) & Precinct 52-5 Road Network

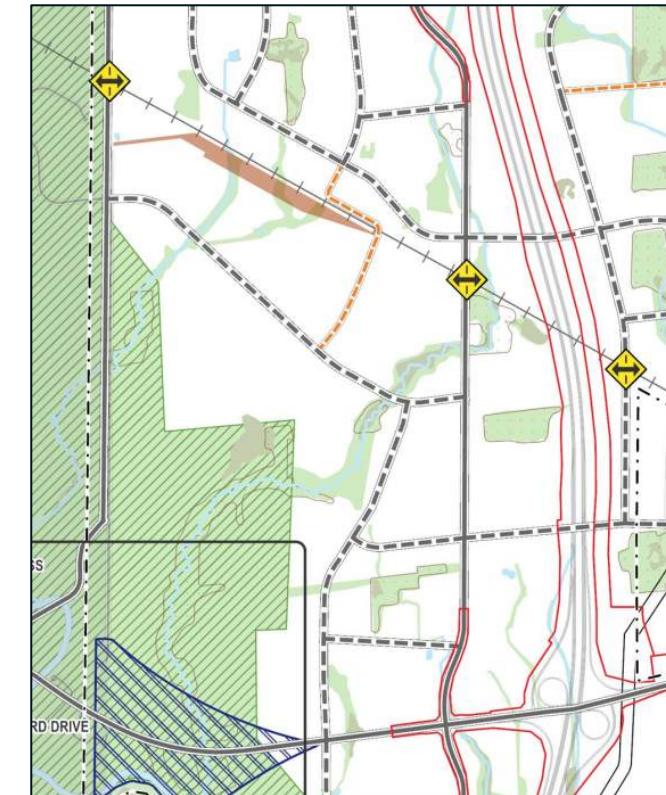
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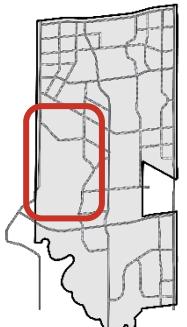
Option 1:
Secondary Plan Road Rail Crossing



Option 2:
Remove Rail Crossing



Option 3:
Active Transportation Rail
Crossing Only



North-South Rail Crossing (52-5 / 52-6) & Precinct 52-5 Road Network

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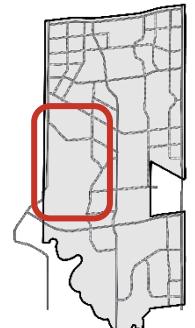
Categories	Option 1 – Secondary Plan Road Network	Option 2 – Road-Rail Crossing Removed	Option 3 – Active Transportation Rail Crossing Only
Transportation	Provides the finest grained connectivity for people by all modes.	Loss of local north-south routing options across all modes between Precinct 52-5 and Precinct 52-6.	Walking and cycling routing connectivity maintained for neighbourhood connectivity between Precinct 52-5 and Precinct 52-6.
Cultural Environment	Overpass/Underpass may impact McNichol's cemetery. Other archaeology findings may require mitigation/modification in coordination with road design.	No road over-under rail results in least risk of directly impacting existing Cultural Environment.	Smaller scale bridge affecting less surrounding area. Opportunities to explore a variety of designs and crossing locations.
Natural Environment	Impacts to Natural Heritage System with overpass/underpass. Headwater Drainage Feature with a floodplain near crossing location Low Point (underpass) would require pumping and other measures posing a challenge to mitigate for stormwater. Overpass is challenged by grading that has considerable impacts to future land uses.	Eliminates impacts from and costs related to building an overpass.	Smaller scale bridge affecting less of surrounding area. Opportunities to explore a variety of designs and crossing locations that may avoid or mitigate impacts to Natural Heritage System and grading/stormwater impacts.
Socio-Economic	Requires crossing of the Metrolinx Layover Facility. Grade separation would be required. Overpass restricts opportunities for local access and development areas in Precinct 52-5 and Precinct 52-6. Underpass would mitigate overpass impacts, with increased complexity/cost and would require groundwater pumping (e.g. low-point). Complex structure and high cost of options.	Greater vehicular activity on parallel corridors (Heritage Road and Winston Churchill Boulevard). Eliminates impacts from and costs related to building an overpass.	Improved walking and cycling experience between Precinct 52-5 and Precinct 52-6 compared to no bridge. Opportunities to explore a variety of designs and crossing locations that may mitigate phasing and geotechnical impacts.
Overall			Recommended

Recommendation:

- An active transportation crossing only crossing the railway between Precinct 52-5 and 52-6
- Provides fine-grained connectivity for walking and cycling between the north and south sides of the railway
- Align collector road network in Precinct 52-5 recognizing through corridor between Winston Churchill Boulevard and Heritage Road
- Potential 4-lane east-west collector in Precinct 52-5 to be refined as part of the Precinct Plan Transportation Study



Option 3:
Active Transportation Rail
Crossing Only



- The Preliminary Preferred Transportation Network includes four schedules:
 - Alignments and Widening
 - Road Classifications
 - Transit Network
 - Active Transportation Network

The **Transit** and **Active Transportation** networks fully integrate with the street network to support a complete street approach.

- The project team collaborated closely with Brampton Transit and Dillon Consulting to identify rapid transit and support corridors in the Heritage Heights area.
- These corridors will both help move people within Heritage Heights and provide connections to other rapid transit routes in Brampton, to GO Bus routes, to the Mount Pleasant GO Station, and to the proposed Highway 413 Transitway stations.
- Local transit routes, to be identified during precinct planning, will provide neighbourhood-level service in the Heritage Heights area.
- To support the proposed transit service and the development of complete streets, all arterial and collector roads will be designed to be transit-friendly.



Active Transportation

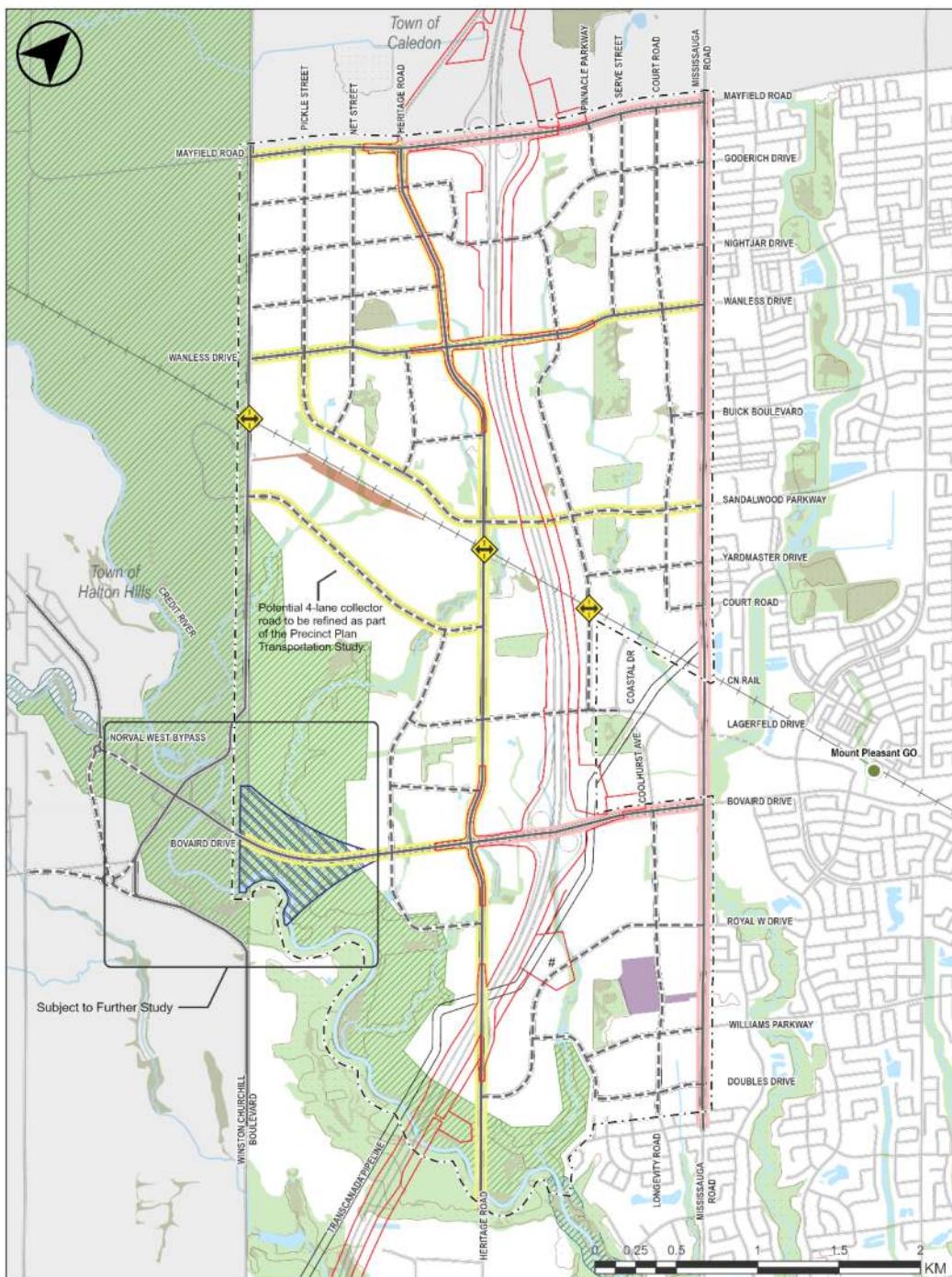
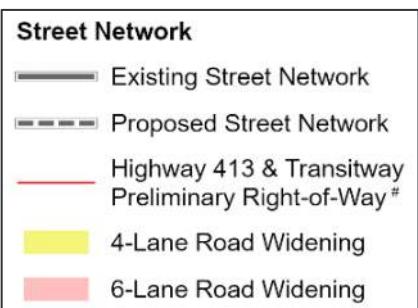
- All arterial and collector roads are recommended to have dedicated active transportation facilities, linking pedestrians and cyclists to key destinations.
- The project team collaborated closely with the City's Active Transportation team and other stakeholders to identify key cycling and trail connection opportunities, building on the network outlined on the Pedestrian and Cycling Network Schedule included in the Heritage Heights Secondary Plan.
- The proposed Recreational Trails and Active Transportation facilities will help connect new communities and destinations while enhancing access to the considerable natural features found in Heritage Heights.



Preliminary Preferred Network

Alignment and Widenings

- The preliminary preferred street network reflects the recommended road alignments from the Long List as well as those evaluated in the Short List.
 - **4-Lane Road Widening**
 - Heritage Road, Wanless Drive, Sandalwood Parkway, Precinct 52-5 collector road, Mayfield Road and Bovaird Drive west of Heritage Road.
 - **6-Lane Road Widening**
 - Mayfield Road and Bovaird Drive east of Heritage Road, Mississauga Road.



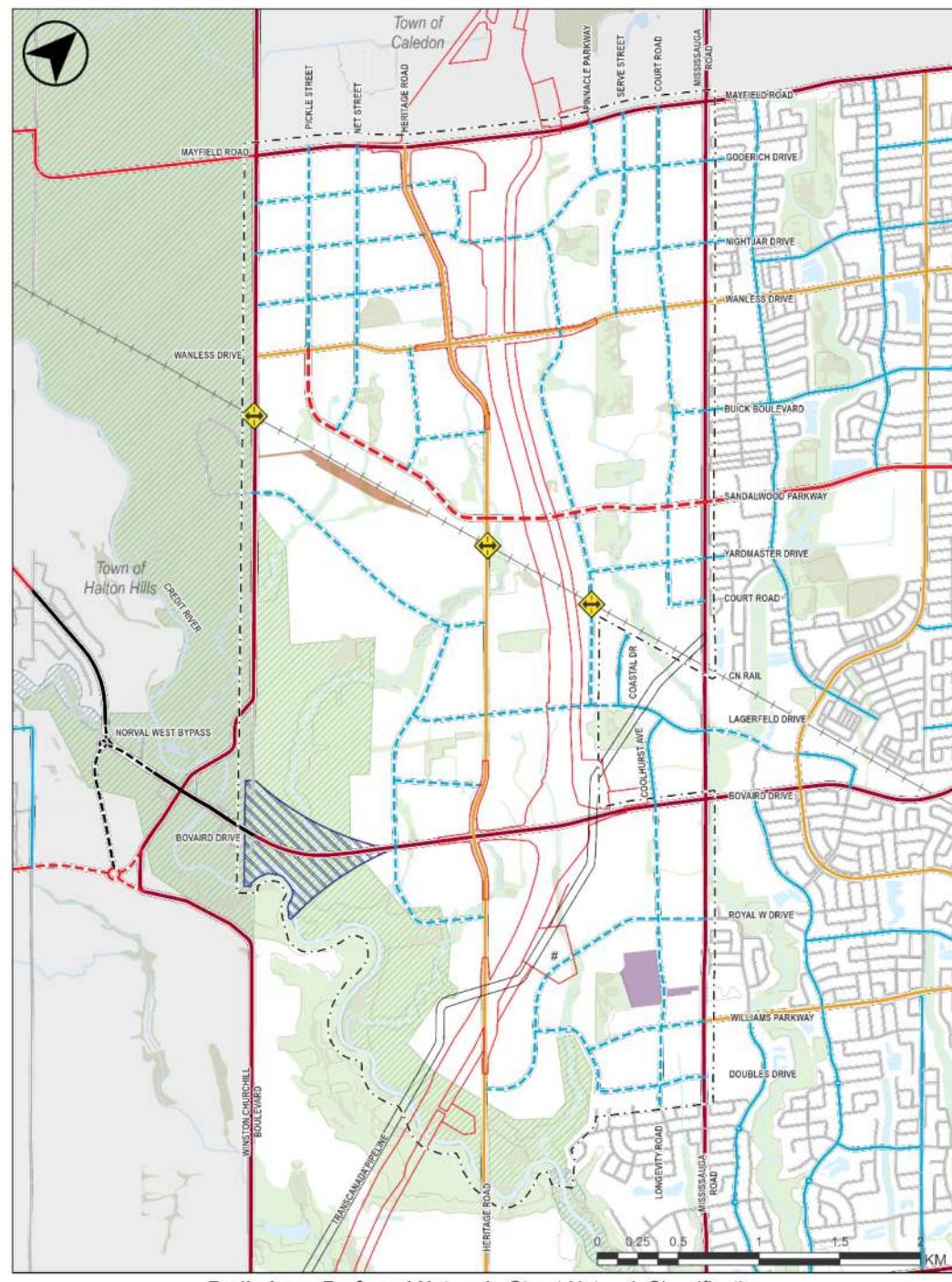
Preliminary Preferred Network

Road Network Classification

- The preliminary preferred street network maintains consistency with the existing City of Brampton road classification system.
- Sandalwood Parkway is extended through the Secondary Plan area as a **Major Arterial (City)** road.
- The majority of proposed streets within the Secondary Plan area are classified as **Collector** roads.
- **Local** roads will be determined through Draft Plan of Subdivision applications and built within individual precincts

Existing Street Network
Proposed Street Network

Road Classification	
Highway	
Major Arterial (Regional)	
Major Arterial (City)	
Minor Arterial (City)	
Collector (City)	
Local (City)	



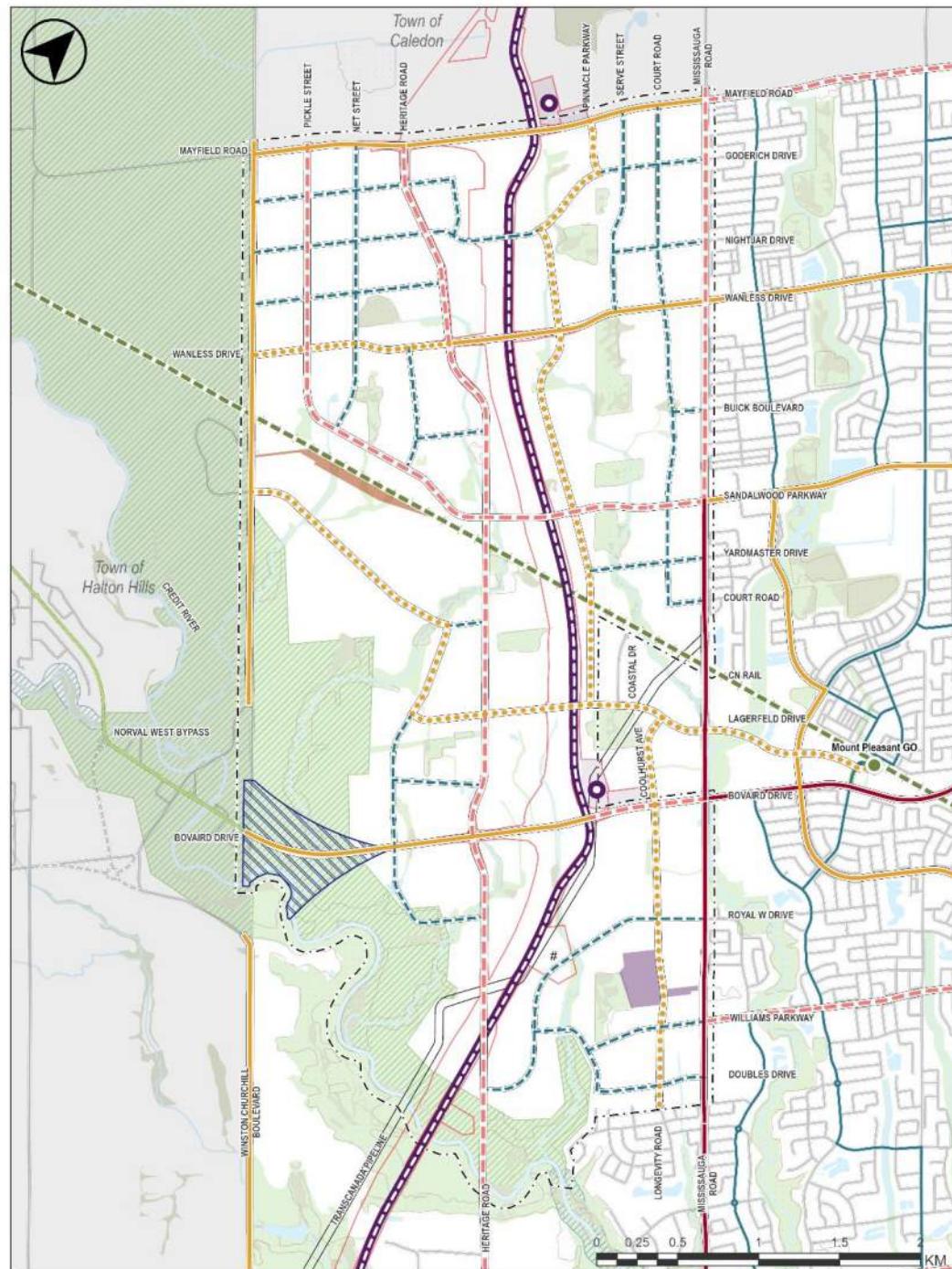
Preliminary Preferred Network

Transit Network

- The conceptual transit network is based on the **Brampton Official Plan** Schedule 3B – Transit Network to 2051 and has been realigned to reflect the preliminary preferred street network.
- A **dedicated transitway** is planned along the Highway 413 corridor.
- Opportunities for expanded transit service has been identified as:
 - **Proposed rapid transit service** on Heritage Road and Sandalwood Parkway.
 - **Suggested Support Corridors** along Longevity Road, Lagerfeld Drive, Pinnacle Parkway, and Wanless Drive.
 - **Potential Local Service** along proposed Collector roads.



See [Heritage Heights Secondary Plan Policy 2.4.1 k](#) related to exploration of a potential GO station in Heritage Heights.



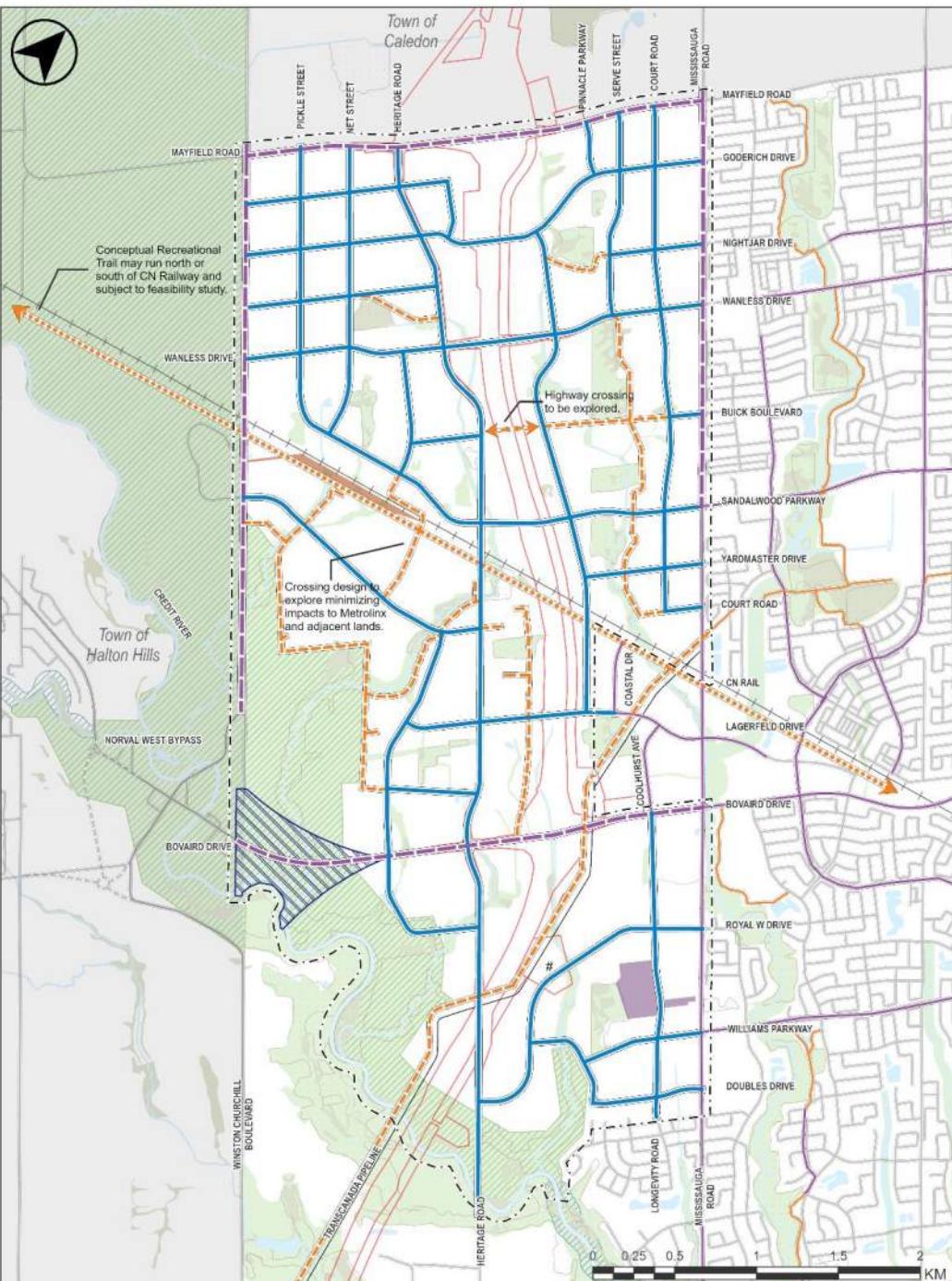
Preliminary Preferred Network

Active Transportation Network

- The active transportation network has been updated from the Secondary Plan to align with the preliminary proposed street network.
- Select streets will be planned to accommodate **on-road active transportation infrastructure**.
- Conceptual **recreational trail connections** are proposed to supplement road infrastructure to ensure continuous pedestrian and cyclist connectivity.
- All proposed active transportation routes are conceptual and subject to change as planning advances.



[†] Existing active transportation infrastructure includes those under construction.
All future / conceptual active transportation infrastructure is subject to change.



1. We want to hear from you!

Please submit any additional comments on the PIC material or further project requests by email to heritageheights@brampton.ca by **September 3, 2025**. Any input received by that date will be taken into consideration as part of the TMP report, which will be available for public comment when the TMP is completed.

1. We want to hear from you!
2. Confirmation of the preferred design based on feedback from the public, Indigenous communities, and stakeholders.
3. Preparation of the final Transportation Master Plan and Environmental Study Report.
4. Place report on public record and notify stakeholders of completion of the study.

Heritage Heights Transportation Master Plan Project Management:

Vikram Hardatt, RPP MCIP

Emily Ecker, P.Eng.

Principal Planner

Senior Associate

City of Brampton

BA Consulting Group Ltd.

Project Email: heritageheights@brampton.ca



MOVEMENT IN URBAN ENVIRONMENTS



Authorized commenting Agency for



September 4, 2025

Vikram Hardatt, RPP, MCIP
Principal Planner

City of Brampton

Via email: heritageheights@brampton.ca

Dear Vikram Hardatt:

**RE: Public Information Centre #1
Heritage Heights Transportation Master Plan**
MHBC File: PAR 50548

MacNaughton Hermsen Britton Clarkson Planning Limited ("MHBC") are the planning consultants for TransCanada PipeLines Limited ("TCPL"), an affiliate of TC Energy Corporation ("TC Energy"). This letter is in response to the materials presented in Public Information Centre (PIC) #1 for the Heritage Heights Transportation Master Plan (TMP) in the City of Brampton. TCPL has two (2) high pressure natural gas pipelines contained within its easement(s) ("TCPL Pipeline Right-of-Way") within the Study Area Boundary.

TCPL's pipelines and related facilities are federally regulated and are subject to the jurisdiction of the Canada Energy Regulator ("CER"). As such, certain activities must comply with the Canadian Energy Regulator Act ("Act") and associated Regulations. The Act and the Regulations noted can be accessed from the CER's website at www.cer-rec.gc.ca.

The Preliminary Preferred Network drawings identify a new collector road crossing the TCPL Pipeline Right-of-Way (constructed), widenings of Bovaird Drive and Heritage Road across the TCPL Pipeline Right-of-Way, and a Conceptual Recreational Trail within the TCPL Pipeline Right-of-Way. Upon review of the PIC #1 materials, we are providing the following comments:

1. No buildings or structures shall be installed anywhere on the TCPL Pipeline Right-of-Way. Permanent buildings and structures, including streetlights, bus stop structures, etc., are to be located a minimum of 7 metres from the edge of the TCPL Pipeline Right-of-Way. Temporary, moveable, or accessory structures, that are not affixed to the ground, are to be located a minimum of 3 metres from the edge of the TCPL Pipeline Right-of-Way.
2. A minimum setback of 7 metres from the nearest portion of the TCPL Pipeline Right-of-Way shall also apply to paralleling roads and any parking area or loading area, including any parking spaces, loading spaces, stacking spaces, bicycle parking spaces, and any associated drive aisle or driveway.
3. Written consent must be obtained from TCPL prior to undertaking the following activities:

- a. Constructing or installing a Facility across, on, along or under a TCPL Pipeline Right-of-Way. A Facility may include, but is not limited to: driveways, roads, access ramps, trails, pathways, utilities, berms, fences/fence posts ("Facility");
- b. Conducting a ground disturbance (excavation or digging) on the TCPL Pipeline Right-of-Way or within 30 metres of the centreline of the pipe (the "Prescribed Area");
- c. Driving a vehicle, mobile equipment or machinery across a TCPL Pipeline Right-of-Way outside the travelled portion of a highway or public road;
- d. Using any explosives within 300 metres of a TCPL Pipeline Right-of-Way; and
- e. Use of the TCPL Prescribed Area for storage purposes.

How to apply for written consent:

- Determine the location of your work relative to a TCPL Pipeline Right-of-Way.
 - When planning, and before any work or activities, listed above, can begin, a request for written consent must be submitted to TCPL through its online application form
 - Location of the work is required, along with the proximity to a TCPL Pipeline Right-of-Way
 - This information can be obtained through survey plans, or through a locate request
- Make a locate request online to the One-Call Centre: ClickBeforeYouDig.com or ontarioonecall.ca
 - The One-Call Centre will notify owners of buried utilities in your area, who will send representatives to mark these facilities with flags, paint or other marks, helping you avoid damaging them. Often written consent for minor activities can be obtained directly from a regional TCPL representative through a locate request.
- **Apply for written consent** using TCPL's online application form: writtenconsent.tcenergy.com or call 1-877-872-5177.
- Application assessment and consent: Once your information has been assessed and potential impacts have been evaluated, TCPL may:
 - Grant consent without any conditions
 - Grant consent that requires certain conditions to be met to assure safety, or
 - Not grant consent

4. During construction, temporary fencing must be erected and maintained along the limits of the TCPL Pipeline Right-of-Way by the Proponent to prevent unauthorized access by heavy machinery. The fence erected must meet TCPL's specifications concerning type, height and location. The Proponent is responsible for ensuring proper maintenance of the temporary fencing for the duration of construction.
5. Storage of materials and/or equipment on the TCPL Pipeline Right-of-Way is not permitted.
6. The following provides design guidance for crossings of TCPL pipeline(s):

General Requirements:

- The crossing shall occur as close as possible to 90 degrees.
- The crossing shall not occur at or within 7 metres of a bend in a TCPL pipeline.
- TCPL may require the installation of permanent protective measures depending on site-specific conditions and engineering analyses.

General Conditions for Crossings by Roads:

- Roads shall be constructed so that the travelled surface is no less than 1.5 metres above the top of the pipeline.

The bottom of the ditches adjacent to roads should not be less than 1.4 metres above the top of the pipeline.

General Conditions for Crossings by Utilities:

- The TCPL pipeline(s) shall retain the upper position in the crossing area.
- Minimum vertical separation between buried facilities shall be 0.6 metres for open cut excavations and 1 metre for horizontal directional drill or other trenchless installation methods.
- The utility depth shall be maintained for the entire width of the TCPL Pipeline Right-of-Way.
- The utility shall have no bends within the TCPL Pipeline Right-of-Way.
- The utility shall have no joints, splices or other connections within the TCPL Pipeline Right-of-Way.
- Pipeline crossings should not be placed within 7 metres of a pipeline bend.

For road crossings, including road widenings, TCPL is required by the CER to prepare a detailed engineering assessment of all loads expected during the construction and operation of the crossing, and, if required, determine the scope for appropriate mitigation. The cost of this engineering assessment, analysis and design work, the costs of any required mitigation or pipe modification will be 100% the responsibility of the Proponent. It is recommended that the Proponent consult with TCPL as soon as possible by contacting MHBC at [REDACTED] mhbcplan.com.

7. Landscaping within the TCPL Pipeline Right-of-Way and TCPL's Prescribed Area require written consent from TCPL and shall be done in accordance with TCPL's guidelines:
 - a. The TCPL Pipeline Right-of-Way is to be seeded with Canada #1 seed.
 - b. No trees or shrubs are permitted to be planted upon the TCPL Pipeline Right-of-Way, but may be allowed within TCPL's Prescribed Area.
8. Sidewalks/pathways within the TCPL Pipeline Right-of-Way require written consent from TCPL and shall be designed in accordance with TCPL's guidelines:
 - a. The width of the sidewalk/pathway shall not exceed 3 metres.
 - b. The sidewalk/pathway shall maintain a minimum separation of 5 metres from the edge of the pipeline at all points where the pathway travels in the same direction (i.e. paralleling) as the pipeline within the TCPL Pipeline Right-of-Way.
 - c. A sidewalk/pathway crossing the pipeline(s) shall be installed to cross the pipeline(s) as close as possible to 90 degrees, but no less than 45 degrees.
 - d. Crossings of the pipeline(s) shall be limited to 1 per approximately 200 metres.
 - e. The presence of the pipeline(s) shall be clearly visible through the installation of above-ground pipeline signage. Signage is to be installed at all road, pathway and other crossings, and throughout the development area at intervals of 100 metres. Signage for crossings will be supplied by TCPL and installed by a Third Party.
 - f. Expansion joints shall be installed 3 metres on either side of the TCPL pipeline(s) if the pathway is cement or asphalt.
9. Grading activities within the TCPL Pipeline Right-of-Way shall be kept to a minimum. TCPL requests that grading plans for lands within the Prescribed Area be circulated to TCPL for review prior to an application for written consent. Written consent must be obtained from TCPL prior to undertaking any grading activities within TCPL's Prescribed Area.
10. Where TCPL consents to any ground disturbances in proximity to any TCPL pipeline, the original depth of cover over the pipelines within the TCPL Pipeline Right-of-Way shall be restored after construction. This depth of cover over the pipelines shall not be compromised due to rutting, erosion or other means.

11. Facilities shall be constructed to ensure that drainage is directed away from the TCPL Pipeline Right-of-Way so that erosion that would adversely affect the depth of cover over the pipeline(s) does not occur. Catchment basins, drainage swales or berms are not permitted within the TCPL Pipeline Right-of-Way. All infrastructure associated with site servicing, grading, and stormwater management (e.g. subdrains, manholes, catch basins, retention walls, storm ponds, culverts/riprap) shall be setback a minimum of 7 metres from the edge of the TCPL Pipeline Right-of-Way.
12. Should pooling of water or erosion occur on the TCPL Pipeline Right-of-Way as a result of any Facility installation or landscaping, the Proponent will be responsible for the remediation or reclamation to TCPL's satisfaction.
13. Any large-scale excavation adjacent to the TCPL Pipeline Right-of-Way, which is deeper than the bottom of the pipe, must incorporate an appropriate setback from the edge of the TCPL Pipeline Right-of-Way and must maintain a slope of 3:1 away from the edge of the TCPL Pipeline Right-of-Way.
14. Mechanical excavation within 5 metres of the edge of a TCPL pipeline is prohibited. Hand or hydrovac excavation must be utilized within this distance.
15. In no event shall TCPL be held liable to the Proponent respecting any loss of or damage to the Proponent's Facility, which the Proponent may suffer or incur as a result of the operations of TCPL. The Proponent shall be responsible for all costs involved in replacing the Proponent's Facility damaged or removed during TCPL's operations and shall indemnify and save harmless TCPL from all actions, proceedings, claims, demands and costs brought against or incurred by TCPL as a result of the presence of or damage to the Proponent's Facility on the TCPL Pipeline Right-of-Way.
16. Vehicle barriers, of a design acceptable to TCPL or as may be required by law, shall be installed across the width of the TCPL Pipeline Right-of-Way, where public roads cross the TCPL Pipeline Right-of-Way. The location of these barriers must be approved by TCPL.
17. If the pipeline(s) experience contact damage or other damage as a result of construction, stop work immediately and notify TCPL at once. The TCPL Emergency Phone Number is 1-888-982-7222.
18. All associated work, signage or any other engineering protection measures to protect TCPL facilities must be completed by TCPL or its qualified contractors at the sole expense of the Proponent. The complete scope of work that may be required is subject to other conditions that may be necessary related to a finalized design that is approved by TCPL. Additionally, prior to TCPL or its contractors conducting any associated work, TCPL and the Proponent must execute a reimbursement agreement, including financial assurances, which provides that the entire cost of conducting this associated work is 100% reimbursable to TCPL.
19. The Proponent shall ensure through all contracts entered into, that all contractors and subcontractors are aware of and observe the foregoing terms and conditions.

Thank you for the opportunity to comment. Kindly forward future study details to [REDACTED]
If you have any questions, please do not hesitate to contact our office.

Sincerely,

MHBC



Kaitlin Webber, MA, RPP, MCIP
Intermediate Planner | MHBC Planning

on behalf of TransCanada PipeLines Limited



September 5, 2025

Vikram Hardatt, Advisor, Special Projects
Official Plan & Growth Management, Integrated City Planning
Planning, Building & Growth Management
City of Brampton
[REDACTED]

Dear Mr. Hardatt,

**RE: HERITAGE HEIGHTS TRANSPORTATION MASTER PLAN, PUBLIC INFORMATION
CENTRE- COMMENT LETTER
11722 MISSISSAUGA ROAD, CITY OF BRAMPTON
CITY FILE: OZS-2024-0063
OUR FILE: 23118A**

On behalf of our client, 11722 Mississauga GP Inc., the owner of the lands municipally addressed as 11722 Mississauga Road in the City of Brampton (the "Subject Lands"), we are pleased to submit this letter to the City, outlining our comments on the proposed road network for the Heritage Heights Secondary Plan ("HHSP"), as presented during the Public Information Centre ("PIC") held on August 20, 2025. Our comments highlight the merits of the proposed revisions to the road network, identified through a technical review conducted by Ainley Group, in support of the planned redevelopment of the Subject Lands with an industrial subdivision.

EXISTING SITE AND CONTEXT

The Subject Lands are located at the south-west corner of Mayfield Road and Mississauga Road, with frontage of approximately 526 metres on Mayfield Road and 593 metres on Mississauga Road (**Figure 1**). The Subject Lands are generally square in shape and approximately 37.9 hectares (93.65 acres) in size. North of the Subject Lands lies mainly agricultural lands with scattered industrial uses, to the east of the Subject Lands are residential uses, agricultural lands and wooded areas are located both south and west of the site.

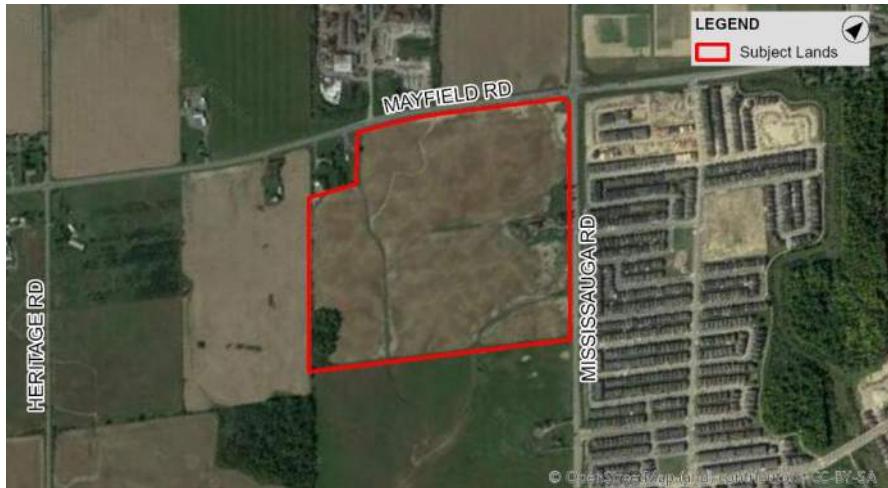


Figure 1- Location Map

HISTORY OF THE PROPOSAL

A Pre-Submission Review for an Official Plan Amendment ("OPA"), Zoning By-law Amendment ("ZBA"), and Site Plan Application for the Subject Lands was submitted to the City of Brampton on June 3rd, 2024, prior to the approval of the HHSP. The intent of the application was to facilitate the redevelopment of the Subject Lands into an industrial subdivision that consisted of five warehouse buildings with ancillary office uses, supported by surface parking, loading areas, and private roads. A formal submission to the City was later made on October 30, 2024. A copy of the Site Plan prepared by Ware Malcomb submitted with the application is provided below. The submission was deemed incomplete by the City on December 6, 2024.

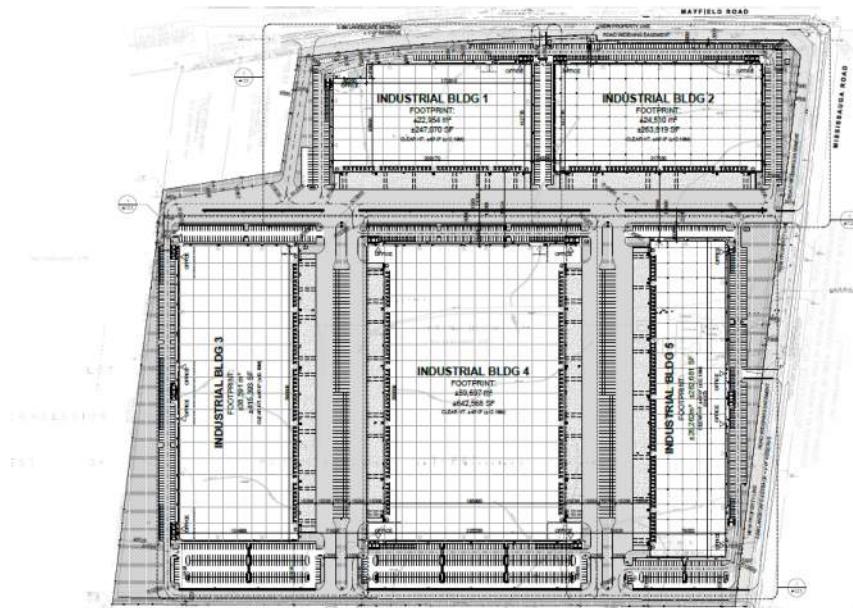


Figure 2- 1st Submission, Site Plan

Official Plan Amendment

The Subject Lands are currently designated as "N-W Brampton Urban Development Area" and "Open Space", with a "Corridor Protection Area" overlay as per Schedule A, General Land Use Designations of the City of Brampton Official Plan. The proposed OPA submitted with the first submission sought to add a Special Land Use Policy exception to the Subject Lands in order to permit the proposed industrial development, in advance of the approval of the HHSP. The intent of the proposed OPA was to implement the employment uses planned for the Subject Lands in the Region of Peel Official Plan, the Draft City of Brampton Official Plan, and the under-appeal HHSP.

Zoning By-law Amendment

An amendment to the City of Brampton Zoning By-law was also required to re-zone the Subject Lands from "Agricultural (A)" to "Industrial (M2)", with site-specific provisions to permit the proposed development. The proposed ZBA was intended to bring the zoning permissions of the Subject Lands into conformity with the designation of the lands proposed through the OPA.

APPROVAL OF THE HHSP

Since the first development application for the Subject Lands, the appeals to the HHSP have been resolved and the HHSP is now in-force and effect. As per Schedule 52-6, Land Use Structure of the HHSP, the majority of the Subject Lands are designated as "Employment", while the western border of the lands is designated as "Natural Heritage System" (**Figure 3**).

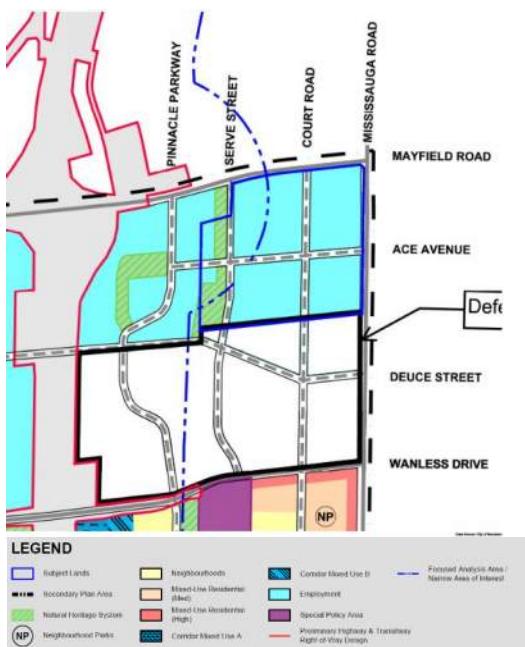


Figure 3- HHSP, Schedule 52-6, Land Use Structure

As per the HHSP, lands designated as "Employment" are intended to maximize the economic potential of the area, particularly given its strategic proximity to the Provincial Transportation Corridor. Development within Employment Areas should support economic resilience and growth, provide local and regional light industrial employment opportunities, and promote uses within the green technology, health, creative, and technology and innovation sectors.

Schedule 52-8, Street Network (**Figure 4**) and Schedule 52-9, Right-of-Way Widths (**Figure 5**) of the HHSP illustrate the conceptual public street network proposed for the HHSP. As per **Figure 4** and **5** below, the Subject Lands is proposed to contain the following public roads:

- Two N-S Public Roads (Serve Street and Court Road), with right-of-way widths of 20.0-30.0 metres; and,
- One E-W Public Road (Ace Avenue), with a right-of-way width of 20.0-30.0 metres.



PUBLIC INFORMATION CENTRE, TRANSPORTATION MASTER PLAN FOR THE HHSP

At the PIC for the Heritage Heights Transportation Master Plan, the City presented two "Network Alternatives" for the road network in Precinct 52-3 of the HHSP, where the Subject Lands are located. These alternatives, shown in **Figure 6** below, differ from the road layout shown in the HHSP. The design revisions realign Deuce Street, extend Ace Avenue, and adjust Pinnacle Parkway to avoid wetland areas. The City explained that these changes were driven by the need to maintain Pinnacle Parkway and Court Road as key north-south connectors to Mayfield Road, as well as to ensure east-west employment connectivity across Highway 413. Option 2, Continuous North-South Connection, maintains the north-south transit route but introduces additional intersections along Ace Avenue, which disrupt the continuity of the east-west collector

road between employment areas. Option 3, Continuous East-West Connection, maintains east-west connectivity and introduces additional intersections along Pinnacle Parkway, which disrupt the continuity of the north-south collector roads connecting to the future highway. Option 3 offers a continuous parallel traffic relief corridor to Mayfield Road and Wanless Drive and requires the least land acquisition compared to the other options. Both of these options have been carried forward for further evaluation, however, the City has recommended that Option 3, Continuous North-South Connection, be pursued.

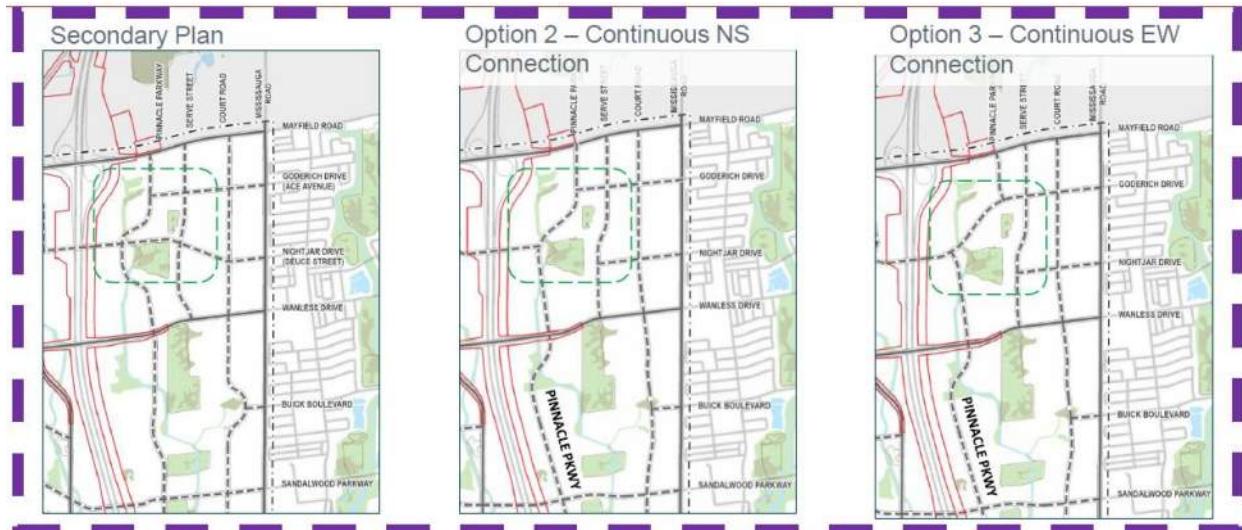


Figure 6- Heritage Heights TMP, Public Information Centre, Alternative Road Network for Precinct 52-3

REVISED PROPOSAL

The proposed industrial development has been revised in response to feedback received from the City, as well as the approval of the HHSP and the ongoing discussions regarding the TMP. As illustrated in **Figure 7** below, the updated plan refines the initial Site Plan—which included five large warehouse buildings and a series of private roads—with a revised layout consisting of nine employment blocks, supported by a combination of public and private roads and realigned NHS lands. Specifically, the proposed road network consists of:

- A 23.0 metre wide private/public road running east-to-west, connecting to Ace Avenue (Goderich Drive) in the east and includes a cul-de-sac in the west, that can connect to Serve Street once the road becomes public; and,
- A private road that connects to Mississauga Road in the east and Serve Street in the west.



Figure 7- Updated Site Plan

The intent of the design changes to the development is to introduce flexibility to allow for smaller-format industrial uses that are more compatible with the road network and land uses envisioned through the HHSP, respond to current market demand for employment uses, and continue to support the objectives of the Employment designation—to maximize the area's economic potential and foster long-term growth. These design changes will still provide flexibility to accommodate larger tenants if the demand for these types of employment uses exist. The detailed design of the proposed development, including the delineation of the NHS, the parking and loading spaces, and refinements to the road network is ongoing, and a revised submission will be made to the City in the imminent future.

JUSTIFICATION FOR REVISIONS TO THE HHSP TRANSPORTATION NETWORK

Policy 10.6.6 of the HHSP states that an Official Plan Amendment will not be necessary to alter the existence, location, alignment, and/or right-of-way width of streets shown on Schedule 52-6, Land Use Schedule, Schedule 52-8, Street Network, and Schedule 52-9, Street Right of Way Widths, as determined through the Precinct Planning or site specific development application approval process. This policy is necessary to allow for flexibility in the design of the road network to meet the needs of future redevelopment as the HHSP evolves over time.

Through the settlement process for the HHSP at the Ontario Land Tribunal ("OLT"), the Subject Lands were re-designated in the HHSP from "Employment & Light Industrial" (northern half of the site), "Low Density," and "High Density" (southern half of the site) to solely "Employment". However, the public road network illustrated in the HHSP was never updated to reflect the functional needs of large-scale employment uses, as envisioned by the approved Employment policies. Design and transportation analyses conducted as part of the development application for

the proposed industrial subdivision on the Subject Lands have determined that the grid-style public road network envisioned by the HHSP does not support the efficiency of the sites intended use. The economic potential of the designated Employment Area should be maximized, and the road network should prioritize efficient connectivity to the Provincial Transportation Corridor. In addition, the internal road network should be hierarchical, with roads capable of handling heavy truck traffic, including wider turning radii. Special attention should be given to freight routes while also accommodating employee traffic and, where applicable, multimodal options like transit and active transportation.

Instead of incorporating three public roads that would redirect traffic through the employment area, the proposed development refines the road network to better support internal employment functions, while still maintaining key connections that align with the broader objectives of the HHSP. The following revisions have been made to the road network contemplated through the HHSP:

- Shift the north-south public road (Serve Street), further west, outside the Subject Lands;
- Realign Pinnacle Parkway, north of the NHS lands, to connect with Serve Street at Goderich Drive (Ace Avenue), creating a single connection to Mayfield Road to the north; and,
- Remove the north-south public road (Court Road) from the Subject Lands, through terminating the road north of Nightjar Drive (Deuce Street).

The proposed revisions to the road network align with the recommended changes for the Precinct 52-3 Road Network, as presented by the City at the PIC for the TMP. By maintaining the east-west public road (Ace Avenue) through the Subject Lands, the proposed road network supports the goal of maintaining east-west transit connectivity by providing a continuous, parallel traffic relief corridor to Mayfield Road and Wanless Drive. The revisions also incorporate the realignment of Pinnacle Parkway, as presented by the City at the PIC, which maintains the north-south collector road connectivity to Mayfield Road while avoiding bisecting NHS lands.

Shifting Serve Street to the west will maintain its connectivity within the planned road network while avoiding conflicts with sensitive environmental features. Since Serve Street is not extended south of Wanless Drive, the proposed adjustment to the alignment will not alter the north-south travel pattern or add undue pressure on Mayfield Road. **This proposed shift to Serve Street to the west is not anticipated to have any significant impact on traffic operations or overall network performance.**

Realigning Pinnacle Parkway north of the NHS lands to connect with Serve Street at Goderich Drive (Ace Avenue), is preferable for traffic operations. The resulting single consolidated connection to Mayfield Road provides adequate spacing from adjacent intersections, which helps to minimize weaving and lane-changing movements. This is particularly beneficial for drivers traveling from Highway 413 and continuing southbound on Serve Street.

Court Road runs north–south and can be divided into two distinct segments. **Segment 1** extends between Mayfield Road and Wanless Drive, while **Segment 2** extends between Wanless Drive and Buick Boulevard.

For Segment 1, traffic patterns traveling between Mayfield Road and Wanless Drive are more efficiently accommodated by Mississauga Road, which provides a higher-capacity north–south connection. Alternative local routes, such as Pinnacle Parkway and Serve Street, are also available to distribute traffic demand. With multiple access points proposed through Mayfield Road, Mississauga Road, and Serve Street, acceptable levels of service are anticipated at site accesses, ensuring sufficient capacity to manage projected total traffic volumes. **Accordingly, the removal of Court Road in this segment is not expected to negatively affect overall connectivity or traffic performance.**

Segment 2, which traverses a natural heritage area and is planned to serve institutional, residential, and park-related land uses, will remain part of the network. Most of the traffic generated in this area will be internal or will utilize Mississauga Road to reach Mayfield Road. It is also highly unlikely that northbound traffic would use Court Road as a through route to Mayfield Road, given the presence of the employment area. Importantly, limiting Court Road to Segment 2 would reduce traffic volumes near schools, residential neighborhoods, and future parks, thereby improving safety for pedestrians, cyclists, and students. **The removal of Court Road between Mayfield Road and Wanless Drive (Segment 1) from the proposed roadway network is expected to have minimal impact on overall traffic operations while providing notable environmental and safety benefits.**

CONCLUSION

During the design process for the proposed employment subdivision on the Subject Lands, it was determined that the public road network, as illustrated in the HHSP, could be further refined to enhance efficiency and better support the intended use of the lands as a dedicated employment area. These proposed changes include the elimination of the north–south public road (Court Road–Segment 1), and the realignment of Serve Street and Pinnacle Parkway. These modifications will maintain overall connectivity and network performance while providing notable environmental and safety benefits. They also directly align with the objectives of the HHSP, supporting long-term mobility, sustainability, and safety priorities without compromising efficiency.

Yours Truly,
MHBC



Oz Kemal, BES, MCIP, RPP
President

SEPTEMBER 4, 2025

PROJECT NO: 1674-6979

SENT VIA EMAIL

Vikram Hardatt, RPP, MCIP
Principal Planner/Supervisor
Integrated City Planning
Planning, Building & Growth Management
City of Brampton

Attention: Vikram Hardatt, RPP, MCIP

**RE: TRANSPORTATION OPINION LETTER
HERITAGE HEIGHTS TRANSPORTATION MASTER PLAN
PIC 1 STAKEHOLDER COMMENTS**

Dear Mr. Hardatt,

C.F. Crozier & Associates (Crozier) has been retained to provide transportation services by Catholic Cemeteries & Funeral Services – Archdiocese of Toronto ((CCFS) to review the Heritage Heights Transportation Master Plan (HHTMP) with respect to the lands owned by CCFS for the purposes of a cemetery, municipally known as 11175 Heritage Road, located in northeastern area of the HHTMP referred to as Precinct 52-3 (hereinafter referred to as the "subject lands" or "CCFS lands"). It is noted that these lands are within the OLT deferred area within Precinct 52-3.

The subject lands are generally bound by the proposed future Goderich Drive to the north, the proposed future Serve Street to the east, Wanless Drive to the south, and the future Highway 413 to the west.

The purpose of this Transportation Opinion Letter is to provide feedback and comments on the Public Information Centre 1 (PIC 1) material prepared by BA Group for the HHTMP dated August 20, 2025, specifically related to the proposed north-south collector road, Pinnacle Parkway, through the CCFS lands.

Crozier is of the opinion that this road is unnecessary given that the CCFS lands will be developed for cemetery uses, which, unlike other land uses, does not have the potential for redevelopment. In addition to being unnecessary from a traffic operations perspective, the road would also be detrimental to the proposed cemetery use from a land use planning perspective. Given the nature and need for the cemetery to be a contiguous parcel, a collector road through the site would create adverse impacts and is not aligned with good planning to allow the cemetery use to function as a whole, contiguous use and site.

Detailed comments related to the strong objection to this public road through the subject lands are provided below for your review and consideration as part of the HHTMP preferred design refinement process.

Pinnacle Parkway Extension between Proposed Goderich Drive and Wanless Road

This transportation opinion letter will focus on a review of the collector road transportation network that is currently proposed for Precinct 52-3 with specific attention to the proposed Pinnacle Parkway segment between Wanless Drive and the proposed Goderich Drive extension.

Cemetery Use and Pinnacle Parkway Justification

Crozier would like to understand what the expected demand along Pinnacle Parkway is and what land yield assumptions were used for the CCFS lands for the purposes of conducting the capacity assessment? Crozier notes that the principle of the CCFS lands being used for cemetery use is established and that CCFS has indicated that its long-term planning for the interment needs of the Catholic faithful in Peel and Halton Region relies upon these lands being developed for cemetery purposes.

It is requested that an updated analysis be conducted that assumes the cemetery use, which would be expected to generate significantly less trips during the peak periods than general employment lands.

As part of the future analysis, it is also requested that a scenario without Pinnacle Parkway between Goderich Drive and Wanless Drive be considered given a public roadway is incompatible with a cemetery use.

Goderich Drive Highway 413 Crossing Needs Assessment

In reviewing the proposed collector road network within Precinct 52-3, Crozier would like to understand the rationale for proposing the Highway 413 crossing of the Goderich Drive extension?

In reviewing the outputs of the modelling results shown in the PIC 1 HHTMP presentation, it is difficult to determine whether the crossing is justified from a capacity perspective, especially given the proposed widening of Mayfield Road to 6 lanes and Wanless Drive to 4 lanes, which would provide additional east-west capacity.

It is requested that a sensitivity analysis be conducted without the Goderich Drive crossing that includes modified trip generation for the proposed CCFS lands to understand whether the transportation network could function without a crossing of Highway 413, particularly since this crossing is not shown in the current Highway 413 design.

Pinnacle Parkway and Serve Street intersection spacing along Mayfield Road

Crozier has reviewed the proposed location of the collector road intersections of Pinnacle Parkway and Serve Street along Mayfield Road and notes that neither of the locations would be expected to meet the MTO's desired spacing standard of 800 metres from the Highway 413 off-ramps per the MTO Highway Corridor Management Manual. It is noted that both would be expected to meet the minimum spacing of 400 metres with Pinnacle Parkway likely spaced approximately at 400 metres from the Highway 413 northbound off-ramp.

Crozier would like to understand if the location of the collector road intersections with Mayfield Road, specifically Pinnacle Parkway and Serve Street, have been confirmed to be acceptable with the MTO?

Summary

Crozier and CCFS are generally in support of the larger area-wide recommended transportation network improvements proposed within the updated HHTMP, such as the larger segment of 6-lane widening along Mayfield Road, as well as most of the collector road network re-alignments.

However, Crozier does not support the Pinnacle Parkway extension through the CCFS lands given it would be detrimental to the use of the site as a planned cemetery and that a scenario without Pinnacle Parkway has not been considered.

As a result, we request that the future work being completed as part of the HHTMP consider the following items in the Precinct 52-3 Road Network evaluation as part of the further evaluation of the alternatives:

- Perform a modified trip generation estimate to account for the cemetery use instead of any other employment uses on the CCFS site.
- Consider removal of the Goderich Drive crossing of Highway 413 to understand if the crossing is justified from a capacity and cost perspective in conjunction with the modified cemetery use. It is noted that this crossing does not appear to be considered on publicly available Highway 413 plans.
- Consider whether the segment of Pinnacle Parkway between Goderich Drive and Mayfield Road is feasible/necessary given MTO spacing requirements in conjunction with the modified cemetery use, especially if the Goderich Drive crossing is shown to not be warranted from a cost-benefit perspective.
- Consider whether any portion of Pinnacle Parkway north of Wanless Drive is necessary to support the Precinct 52-3 build-out given the proposed cemetery use and the comments noted above.

We trust that the concerns identified herein will be heard and addressed with the utmost care recognizing that the safe and efficient movement of all modes of transportation remains paramount for the many future residents and visitors within the Heritage Heights development area.

Sincerely,

C.F. CROZIER & ASSOCIATES INC.



Brandon Bradt, M.Eng.CEM, P.Eng.

Manager, Transportation Planning

J:\1600\1674 - Catholic Cemeteries & Funeral Services - Archdiocese of Toronto\6979 - Heritage Heights Secondary Plan
OLT\Letters\2025.09.04 - Transportation Opinion Letter - Heritage Heights TMP - FINAL.docx

September 5, 2025

Our File No. 332861

By E-Mail to: heritageheights@brampton.ca

The Corporation of the City of Brampton
2 Wellington Street West
Brampton, ON L6Y 4R2

Attention: Genevieve Scharback, City Clerk

Dear Ms. Scharback:

Re: JW Facilities Comments on the PIC Materials regarding the Heritage Heights Transportation Master Plan - 2594 Bovaird Drive West, Brampton

Introduction

We are legal counsel to JW Facilities, a Federal not-for-profit corporation and registered charity, regarding planning and potential land acquisition issues affecting the property located at 2594 Bovaird Drive West, Brampton. We are making these comments on behalf of our client regarding certain aspects of the Heritage Heights Transportation Master Plan which was considered at a public information meeting on August 20, 2025.

The Subject Property

The property at 2594 Bovaird Drive West is approximately 42 acres in size, and has frontage on both Heritage Road and Bovaird Drive. The facility is improved with a large auditorium/meeting facility with 1,700 seats used for religious events throughout the year. The facility serves the approximately 25,000 Jehovah's Witnesses in the Greater Toronto Area. Most persons using the facility arrive by car. The facility is also used for training purposes which include training classrooms as well as three (3) apartments that are occupied by full-time ministers. There are also numerous out buildings, on site servicing infrastructure, parking, and landscaping. The facility is currently undergoing a multi million dollar major renovation and expansion. Completion is anticipated for June of 2026. Given the nature of the facility and its use, unimpeded vehicular access at all times of the week is necessary. As well, intrusive noise and vibration through highway construction is extremely detrimental to the function and use of the property.

Concerns Regarding the Heritage Heights Transportation Master Plan Proposals

Our client has not agreed to nor consented to the use or taking of their property by the City of Brampton or any other public authority in that regard. Of particular concern to our client is the apparent realignment and widening of Heritage Road, as well as the widening of portions of Bovaird Drive. This appears to be utilizing portions of 2594 Bovaird Drive West.

September 5, 2025

Page 2

In addition, our client is concerned with a loss of land, disruption to access and egress during and after completion of construction of the Heritage Road realignment and widening and the Bovaird widening. Our client is also concerned with intrusive noise and vibration during those times when the facility is in use. Further comments may be forthcoming regarding some additional aspects of the Transportation Master Plan and as the process evolves.

Conclusion

Please accept these as our comments regarding proposals to realign and widen Heritage Road and to widen Bovaird Drive in the vicinity of our client's property and what appear to be land requirements of our client's property. Please let us know if you require further information.

Please keep our office informed of any further meetings, reports, documents or decisions regarding the Transportation Master Plan process.

Yours truly,

AIRD & BERLIS LLP



Christopher J. Williams
Partner

CJW:am

Copy: Client

A. Skinner, Aird & Berlis LLP
P. Van Loan, Aird & Berlis LLP

September 3, 2025

GSAI File: 936-002
936-003

City of Brampton
Planning & Development Department
2 Wellington Street
Brampton, ON
L5B 3C1

Attention: **Vikram Hardatt**
Advisor, Special Projects, Integrated City Planning
Planning, Building and Growth Management

Re: **HERITAGE HEIGHTS TRANSPORTATION MASTER PLAN –
PUBLIC INFORMATION CENTRE (PIC) MATERIALS**

On behalf of Primont Homes (Heritage Heights 1) Inc., Primont Homes (Heritage Heights 3) Inc., and Primont Homes (Heritage Heights 4) Inc., landowners within the Heritage Heights Secondary Plan Area, we wish to acknowledge receipt and review of the materials presented at the Public Information Centre held virtually on August 20, 2025.

We appreciate the City of Brampton's and the Heritage Heights Landowners Group's efforts in advancing the Transportation Master Plan and the opportunity to provide comments at this stage. Given the importance of transportation infrastructure to the success of the Secondary Plan, Primont has a particular interest in staying engaged in the TMP process as it moves forward.

In particular, we request to be kept informed regarding:

- The identification and refinement of transportation network crossings of existing or planned constraints that may directly influence the configuration and connectivity of our clients' lands; and
- Any updates related to the evaluation and potential location of a future GO Station within the Secondary Plan area, given the significant implications this would have on land use, mobility, and long-term planning for our holdings.

We look forward to continued consultation and collaboration with the City and the project team to ensure the TMP supports a well-connected, transit-oriented, and complete community in Heritage Heights.



Please confirm receipt of this letter and ensure that we are included on the circulation list for all future correspondence and updates related to the Heritage Heights Transportation Master Plan.

Sincerely,

GLEN SCHNARR & ASSOCIATES INC.



Herman Wessels
Planner



Planning, Building and Growth Management Department
City of Brampton
2 Wellington Street West,
Brampton, Ontario
L6Y 4R2

September 5, 2025

Attn: Mr. Steve Ganesh, MCIP, RPP Commissioner

Re: **2212 and 2214 Wanless Drive**

**Heritage Heights Transportation Master Plan Public Information
Centre - August 2025**

I represent the 231790 Ontario Limited the owner of 2212 and 2214 Wanless Drive, Brampton, Ontario. These properties are on the north side of Wanless Drive, west of Mississauga Road.

This letter is provided in response to the Heritage Heights Transportation Master Plan Public Information Centre - August 2025, and the presentation provided on the City of Brampton web site for the Heritage Heights Secondary Plan.

In reviewing the presentation, we agree with Recommendation on page 52, Option 3 as it related to 2212 and 2214 Wanless Drive. We would request that the north/south "Serve Street" be located abutting, but not on 2212 Wanless Drive. Please see the plan below locating the properties on Wanless Drive as they relate to Option 3.

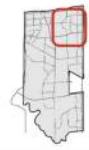
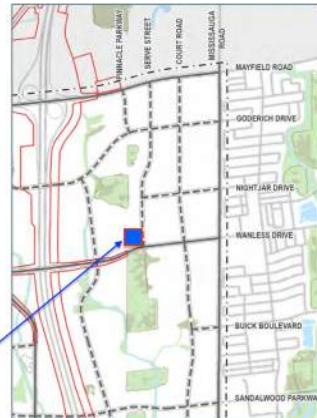
Short List C: Precinct 52-3 Road Network

52

Recommendation:

- Maintain a continuous east-west crossing of future Highway 413 (Goderich Drive) between employment areas in Precincts 52-3 and 52-7.
- Maintain north-south collector road connectivity to Mayfield Road, serving Precincts 52-2 and 52-3 to/from future Highway 413 and Provincial Transitway while avoiding bisecting wetland and woodland feature.

2212 & 2214 Wanless Drive



Road Alignments

We would also request any information you have on the road widening for Wanless Drive from 2 to 4 lanes. In particular, the road geometry of the 4-lane realignment and the construction timing for this work would be most appreciated.

We would be happy to meet to discuss any questions you may have regarding our comments.

Respectfully submitted,

Dan Kraszewski

Dan Kraszewski, R.P.P. M.C.I.P.
D.J.K. Land Use Planning



cc. 231790 Ontario Limited
Henrik Zbogar, Director, Integrated City Planning
Vikram Hardatt, Integrated City Planner
Emily Ecker, BA Consulting

Michael C. Giallonardo

From: Hardatt, Vikram [REDACTED]
Sent: October 9, 2025 11:42 AM
To: Emily J. Ecker; Michael C. Giallonardo
Subject: FW: [EXTERNAL]Heritage Heights Mobility Plan

Hi Emily and Mike,

I think I may have missed circulating these comments to you guys after the PIC. My apologies. See below.

Thanks,

Vikram

From: Sylvia Menezes Roberts [REDACTED]
Sent: 2025/08/20 10:49 PM
To: Hardatt, Vikram [REDACTED]
Cc: [REDACTED]
Subject: [EXTERNAL]Heritage Heights Mobility Plan

Caution: This email originated from outside the organization. Do not click links or open attachments that you do not trust or are not expecting.

Hi Vikram,

I apologize if I sounded hostile, I am not very good at regulating the pitch or tone of my voice. Here are some distilled thoughts

1) The City should be looking at Zum connecting into Georgetown, unless Halton Hills explicitly says they oppose it. Limitations on the CN mainline, which the Kitchener GO Line runs on, means we probably won't see frequent GO service connecting Georgetown to Brampton for decades to come. Having frequent bus service integrates Georgetown with the City of Brampton allowing the City to bring it into Brampton's orbit as a suburb. If we do not plan a good transit connection then they will still visit Brampton by car. The GTA has a "just add lake water" approach to growth, the South Peel Water scheme brought lake water to Brampton, and then we boomed, then the pipe was extended to Milton, and then they boomed, and now the pipe is extending lake water to Georgetown, and they too will boom. So should we plan for them to visit Brampton by transit? If we do not, then they will visit by car.

2) The bike infrastructure really needs better thinking on design for bike lanes and MUPs. MUPs have fairly limited capacity, and narrow bike lanes also have significant issues, such as how overtaking will occur, and how it will accommodate cyclists when many can easily go 30 km/h due to e-bikes. Some trunk corridors will need to be designed to have higher capacity cycle tracks if you genuinely wish cycling to have meaningful mode share.

3) Ultimately what I saw on the maps was a significant lack of transportation capacity by non-car means, and I am concerned this is going to lead to still very dominant car mode share, only without the road

widths to handle such car capacity. Nowhere near enough transit to get people out and about. The basic problem in the City is this, the average commute distance means only automobiles or transit are able to travel such a distance in reasonable time, which means if the transit isn't good enough then they will drive to work. If they are going to drive to work, then why not drive to the shops too? If you want to change mode share, you have to lean very hard on transit first, then you can get the density for it to be non-car oriented. If you can get the people to not need a car for going to work, then you can get them to get out and about walking, taking transit, or biking for their daily needs.

Sylvia

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Michael C. Giallonardo

From: Ivan Drewnitski [REDACTED]
Sent: September 5, 2025 2:37 PM
To: Hardatt, Vikram; Dave, Richa; Lakeman, Brian; Cadete, Nelson
Cc: Maureen Van Ravens; Bronwyn Parker; Jeff Markowiak; Melissa Ricci; Josh Salisbury; Roumen Kotev; Partridge, Shelley; Krusto, Matt; Scattolon, Walter; Morgan, Andrew
Subject: RE: Heritage Heights Transportation Master Plan External Agency Engagement Session Slides

Hi Vikram and co.,

Thank you for allowing us to review and provide feedback on the Heritage Heights Transportation Master Plan. Please see the Towns comments below.

- The Town of Halton Hills continues to express concerns regarding the projected traffic volumes associated with the Heritage Heights Transportation Master Plan. These volumes are expected to have a significant and negative impact on the hamlet of Norval, which is a gateway into Halton Hills, and a sensitive and historically significant community.
- These concerns are consistent to the commitments outlined in the Memorandum of Understanding (MOU), which emphasized the need to protect Norval from adverse transportation impacts and establish a framework for the timely implementation of transportation improvements recommended in the Halton-Peel Boundary Area Transportation Study.
- In light of this, the Town is strongly urging continued and meaningful collaboration with the City of Brampton and the Region of Peel to develop a comprehensive, long-term transportation network that reflects the shared objectives of all municipal partners involved, consistent with the 2012 MOU.
- This network must not only accommodate future growth but also safeguard the character and livability of existing communities like Norval. We are looking for a coordinated approach that respects the commitments made and ensures that mitigation strategies are in place before any major infrastructure decisions are finalized.
- The Town believes that additional information is required as to the details of next steps and timeline which should be integrated into the Master Plan.
- Town staff participated in the mediation process with the City of Brampton and landowners within the (HH) Secondary Plan. City of Brampton and landowners supported the inclusion of policy 13.1.11 as requested by the Town and Region of Halton.

Policy 13.1.11

- a) In order to ensure provisions for adequate transportation infrastructure are in place to support development in the Heritage Heights Secondary Plan Area, prior to the City of Brampton adopting Precinct Plans for Precincts 52-4 and 52-5, proponents of development shall demonstrate satisfaction of the transportation policies under the North West Brampton Urban Development Area section of the City of Brampton Official Plan.
- b) Within Precincts 52-4 and 52-5 on Schedule 52-2, future study is required as identified by the Halton Peel Boundary Area Transportation Study Memorandum of Understanding (HPBATS MOU, 2012) to determine the extent of transportation network improvements that may be required to support east-west connections across municipal boundaries. This study will inform Precincts 52-4 and 52-5 and shall occur as part of the Precinct Planning process and may be undertaken by affected landowners. The Study shall be undertaken based on the terms identified in the HPBATS MOU and must be to the satisfaction of the City, in consultation with the Town of Halton Hills and Region of Halton.

c) Any development proposed within the East-West Connection Focus Area as shown on Schedule 52-6, in advance of adoption of the Precinct Plans for Precincts 52-4 and 52-5 must be reviewed by the City in consultation with the Town of Halton Hills and Halton Region. No development shall be approved by the City within Precincts 52-4 and 52-5, within the East-West Connection Focus Area as shown on Schedule 52-6, that may preclude the opportunity for the future east-west cross boundary transportation network improvements identified in the Study identified in Policy 13.1.11 b) above.

Please confirm that the City will continue to pursue the required further studies through the precinct planning process in order to ensure that any transportation mitigation improvements are adequately addressed and resolve any capacity constraints within the area.

Thanks,



Ivan Drewnitski, C.E.T.
Transportation Planning Coordinator
[REDACTED]
haltonhills.ca

Subject: Heritage Heights Transportation Master Plan External Agency Engagement Session Slides

[EXTERNAL EMAIL]

Hello everyone,

Thank you to those who were able to join us for the Heritage Heights Transportation Master Plan external agency engagement session on August 14th.

As discussed, I have attached the slides and you will receive a separate email shortly with a link to the recording if you were unable to make it.

We are accepting comments on the alternatives and the preliminary preferred networks until September 3rd, 2025. Please send any comments to heritageheights@brampton.ca.

Thank you,

Vikram

Vikram Hardatt RPP, MCIP

Principal Planner/Supervisor
Integrated City Planning
Planning, Building & Growth Management
City of Brampton



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Michael C. Giallonardo

From: Partridge, Shelley [REDACTED]
Sent: September 5, 2025 3:00 PM
To: HeritageHeights
Cc: Hardatt, Vikram; Scattolon, Walter; Krusto, Matt; Larkin, Ann; Morgan, Andrew
Subject: [EXTERNAL]Halton Region Comments re Heritage Heights Transportation Master Plan

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Good Afternoon:

We appreciate being invited to the August 14th agency meeting and being asked for comments on the materials presented at that meeting. Regional staff are available for further meetings/discussions on the items noted below, if that would assist in Brampton's process.

- We understand from the presentation and the provided slides that the Master Plan will recommend that Area E (Norval area) be carried forward for further study. Please indicate the next steps for this work to occur and the anticipated timelines for this additional study. Can more specifics on future study timing be included in the Master Plan document itself?
- The Region has concerns with the projected volumes anticipated in the Heritage Heights Transportation Master Plan and the impacts this will have on the road network in the boundary area between Peel and Halton. The 2012 Memorandum of Understanding amongst local and regional municipalities in Peel and Halton was meant to establish a framework for the timely implementation of transportation improvements recommended in the HPBAT Study. The future EA for the East-West connection continues to be a priority for Halton Region to see move forward in a timely fashion. As such, Regional staff wish to continue to collaborate with the City of Brampton and the Region of Peel to establish a long-term transportation network for this area.
- Based on the Slide Deck content for Area E (specifically Slide 43), the Region finds it unclear if the 'DO NOTHING' option will be 'Carried Forward' or if it has been Ruled Out. During the meeting, it was communicated that the "DO NOTHING" option would be screened out, as it is not a viable option, but Slide 43 seems to reference that this option may be carried forward. Please confirm. This may need clarification in the report itself.
- Please advise when there will be a draft Transportation Report circulated for review and comment.

Thanks very much,

Shelley

Shelley Partridge, (she/her/hers), MPI, MCIP, RPP

Senior Planner – Halton Hills

Development Services

Public Works

Halton Region

[REDACTED]



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7.0 PLANNING AND DEVELOPMENT COMMITTEE MEETING

The draft HHTMP was brought forward for City Council endorsement at the City of Brampton Planning and Development Committee Meeting on January 19th, 2026. At the meeting, City staff presented the draft report and the appendices. There were three delegations, and one letter submitted to the Committee. The delegations and letter received was referred to staff.

The HHTMP was unanimously endorsed by the Planning and Development Committee, and this decision was ratified at the January 21st, 2026 City Council meeting.

In this section, a copy of the letter submitted to the Planning and Development Committee on January 16th and a follow-up letter submitted to the City Clerk's office on January 26th, 2026 is included.



January 16, 2026

David Tang

Sent via E-mail
cityclerksoffice@brampton.ca

City Clerk's Office and Planning and Development Committee
Corporation of the City of Brampton
2 Wellington Street West
Brampton, ON L6Y 4R2

Dear Chair Palleschi and Members of the Planning and Development Committee:

**Re: Item 8.1 Heritage Heights Transportation Master Plan
Planning and Development Committee Meeting Monday, January 19, 2026
Comments from Catholic Cemeteries & Funeral Services – Archdiocese of
Toronto**

We are the solicitors for Catholic Cemeteries & Funeral Services – Archdiocese of Toronto (“CCFS”) and are writing on behalf of our client with respect to the proposed Pinnacle Parkway Extension between Goderich Drive and Wanless Road. Our client owns the lands through which the proposed Pinnacle Parkway Extension is to run; bounded on the south by Wanless Road (the “CCFS Cemetery Lands”). The location of that Pinnacle Parkway Extension proposed by the Heritage Heights Transportation Master Plan report before you as Item 8.1 has the effect of bisecting the CCFS Cemetery Lands.

Our client is strongly opposed to this portion and alignment of the Pinnacle Parkway Extension. It has repeatedly advised the City of its concerns and sought a meaningful discussion about alternatives given the devastating impact it would have on its planned cemetery. The undersigned and CCFS will seek to depute at this meeting.

There is significant background to this matter. CCFS purchased the CCFS Cemetery Lands in 2001 for use as a cemetery. That planned cemetery was however put on hold by the Interim Control By-Law put in place to hold the proposed Highway 413 alignment and CCFS has not been able to proceed for over two decades.

The City’s Heritage Heights Secondary Plan followed proposing a Grand Avenue, but has been forced to undergo revisions given the Province’s decision to move forward with Highway 413 through that Secondary Plan area. The Secondary Plan was appealed to the Ontario Land Tribunal and as part of that process, earlier discussions with the City led to a clear land use planning decision that the CCFS Cemetery Lands had to be preserved for use as a cemetery to serve the Catholic population in the City and beyond. Even Highway 413 was realigned to accommodate the cemetery as detailed in Section 3.4.1 of the Draft Environmental Impact Assessment Report.

With the cemetery land use well-established, our client sought and expected meaningful consultation and dialogue regarding the proposed Pinnacle Parkway Extension division of its lands as proposed in the Heritage Heights Secondary Plan. That concern was identified very

early on as the most significant problem to the cemetery. CCFS' long-established cemetery plans occupied all these lands.

The expropriation of land for Highway 413 has already reduced the amount of land available for interment. Even without Pinnacle Parkway running through it, these lands are now just barely large enough for a functional cemetery. It can only function with private internal roads.

Furthermore, a major collector road dividing the operating area of the cemetery would make effective operations almost impossible and dangerous. Cemeteries operate with constant heavy machinery moving unpredictably from interment site to interment site to prepare, excavate, fill and carry out landscaping activities in correspondence to funeral demands. It will be remarkably difficult for them to safely traverse a wide collector road. Bereaved families would also somehow have to be able to cross that road. CCFS simply cannot see how a cemetery could function with its integrated activities separated by such major transportation infrastructure.

On top of those concerns, CCFS is advised by its transportation engineers that the road is not even necessary. C.F. Crozier & Associates provided comments on its concerns with the draft Transportation Master Plan to City staff and the City's consultant, BA Consulting, in its September 4th, 2025 letter. That letter has gone completely unanswered. Mr. Bradt, the author of that letter, has not been contacted at all prior to the release of this report. It was Crozier's clearly expressed opinion that the road is unnecessary. Crozier questioned the adequacy of the analysis that led to the Pinnacle Parkway extension running through CCFS' lands; expressly requesting an analysis of the expected demand for this portion of Pinnacle Parkway and information on what land yield assumptions were used (if any) for determining that this Pinnacle Parkway extension is even needed in the first place. Crozier had additional questions and requests, including a request for the rationale for the Goderich Drive extension crossing of Highway 413, which appears to be solely put in place to allow a diversion of the Pinnacle Parkway Extension through CCFS' lands westward after earlier comments Crozier had provided about the problems with the initial alignment set out in the Heritage Heights Secondary Plan, and requests for a sensitivity analysis and justification, from a capacity perspective, for that Goderich Drive - Highway 413 crossing. Those additional questions highlight road patterns and highway crossings which appear designed primarily to enable this awkward alignment to even exist in the first place. Particularly important is a justification for the Goderich Drive – Highway 413 crossing considering it is not currently included in the Highway 413 design. In all, Mr. Bradt's letter sought confirmation on four major issues which are necessary to even justify the existence of this Pinnacle Parkway Extension through the CCFS Cemetery Lands.

CCFS is very disappointed that there has been no consideration, no further analysis and no consultation with it or C.F. Crozier & Associates on the one matter that most significantly affects the usability of these lands, which it is crucial to remember have been definitively recognized as cemetery lands.

Request

In short, the Master Transportation Plan's recommendation of this Pinnacle Parkway Extension make it impossible for CCFS to actually build and operate the cemetery for no clearly enunciated reason, despite requests for that analysis and dialogue.



As a result, our client asks that your Committee refer this Heritage Heights Transportation Master Plan report and its recommendations back to City staff and BA Consulting for consideration of the need and appropriateness of the Pinnacle Parkway Extension through CCFS' lands.

Thank you for your consideration.

Yours very truly,

MILLER THOMSON LLP

Per:



David Tang
Partner
DT/ac





MILLER THOMSON LLP
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MILLERTHOMSON.COM

January 26, 2026

Sent via E-mail

David Tang

Mayor Patrick Brown and City Council
Heritage Heights Transportation Master Plan Class EA Project Manager and
Steve Ganesh, Commissioner of Planning, Building and Growth Management
Corporation of the City of Brampton
Brampton City Hall
2 Wellington Street West
Brampton, ON L6Y 4R2

Attention: Vikram Hardatt

Dear Mayor Brown and Members of City Council:

**Re: Heritage Heights Transportation Master Plan
Notice of Completion of Study
Comments from Catholic Cemeteries & Funeral Services – Archdiocese of
Toronto**

We are the solicitors for Catholic Cemeteries & Funeral Services – Archdiocese of Toronto (“CCFS”) and are writing on behalf of our client to provide comments on the Heritage Heights Transportation Master Plan Report for which there is a Notice of Completion of Study. Our client, through its consultant C.F. Crozier & Associates (“Crozier”) in September of 2025 and most recently on January 15, 2026 has written and appeared before the Planning and Development Committee on January 19, 2026 to express concerns with and opposition to the proposed Pinnacle Parkway Extension between Goderich Drive and Wanless Road. Our client owns the lands through which the proposed Pinnacle Parkway Extension is to run; bounded on the south by Wanless Road with the municipal address of 11175 Heritage Road (the “CCFS Cemetery Lands”). The location of that Pinnacle Parkway Extension proposed by the Heritage Heights Transportation Master Plan report has the effect of bisecting the CCFS Cemetery Lands.

Our client expressed its strong opposition to this portion and alignment of the Pinnacle Parkway Extension. The cemetery use for these lands has been recognized by the Ontario Land Tribunal in the Heritage Heights Secondary Plan appeal process and further by the Province in adjustments to its Highway 413 plans to minimize the taking of cemetery lands.

CCFS is grateful that the Chair of the Committee responded at that January 19, 2026 meeting positively to CCFS’ concerns and request, indicating that “You’re absolutely right, we don’t want to put a road through your lands. We’re happy to support [that].” Following the Chair’s indication that the Committee was “happy to send your delegation to staff to come up with

some wording that, certainly, helps support what you're here asking of us today", the recommendations from the Committee to Council were as follows:

That the following delegations re: Heritage Heights Transportation Master Plan, to the Planning and Development Committee Meeting of January 19, 2026, be referred to staff for consideration; and

1. *Ron Hendrix, Catholic Cemeteries and Funeral Services*
2. *David Tang, Miller Thomson LLP, on behalf of Catholic Cemeteries and Funeral Services*

That the correspondence from David Tang, Miller Thomson LLP, on behalf of Catholic Cemeteries and Funeral Services, dated January 16, 2026, re: Heritage Heights Transportation Master Plan, to the Planning and Development Committee Meeting of January 19, 2026, be referred to staff.

CCFS's Follow-up

CCFS wants to make it clear that CCFS has maintained its intentions to pursue a cemetery use on the CCFS Cemetery Lands since it bought them in 2001 and (as discussed later in this letter) wishes to find ways to more definitively clarify that from a land use perspective. It was stymied by the Interim Control By-law and like many other landowners within the area, has waited for final designs of Highway 413 from the Province in order to proceed, along with the lifting of the Interim Control By-law. As outlined in detail in the Highway 413 Draft Environmental Impact Assessment Report dated December 2025, Section 4.3.1, the alignment of the Highway has now been revised to accommodate the proposed cemetery use on the CCFS Cemetery Lands.

The letter submitted by Crozier on September 4, 2025 provides a transportation opinion on the proposed north-south collector road, Pinnacle Parkway, through the CCFS lands, concluding that the road is unnecessary given that the CCFS Cemetery Lands will be developed for a cemetery and will also therefore not redevelop in the future. As outlined in the letter, given the nature and need for the cemetery to be a contiguous parcel, a collector road through the site would create adverse impacts and would not allow the cemetery use to function as a whole, contiguous site. With the expropriation of land for Highway 413, the parcel is barely sufficient in size to support the proposed cemetery use. Any further reduction in size to the cemetery would significantly impact and jeopardize the CCFS Cemetery Lands from fulfilling the interment needs of the Catholic faithful in Peel Region who are relying on these lands to be developed for cemetery purposes.

Modification Request

To provide clarity on what CCFS believes would resolve its concerns in accordance with the Planning Committee's recommendations for further staff response, CCFS requests either a modification to the HHTMP to:

1. Remove the section of Pinnacle Parkway through the CCFS Cemetery Lands entirely between Wanless and Goderich Drive or at least through the CCFS Cemetery Lands, or



2. Add a Schedule with a map of this area showing an alternative road network that does not contain a Pinnacle Parkway Extension through the CCFS lands as the transportation plan if the identified CCFS lands are not developed for employment, commercial or residential uses.

We would be pleased to meet with City staff further if the decision is to proceed with Option 2 in order to review an alternative road network and the policy language needed. CCFS does wish to reiterate that its preference remains Option 1, with the complete deletion of that Pinnacle Parkway Extension through the CCFS Cemetery Lands as this extension remains unwarranted given the cemetery land use and the approval of the HHTMP with that extension jeopardizes the implementation of the cemetery contrary to the OLT order noted above and the Provincial intention for the creation of this cemetery here, as expressed in and through the Highway 413 Environmental Assessment process.

Zoning

To move this process along and to provide certainty with respect to CCFS' longstanding intention to use these lands as a cemetery, CCFS would also like to pursue and discuss with staff opportunities to proceed with an amendment to the Zoning By-law to permit **only cemetery uses** on the CCFS Cemetery Lands. That change to the land use regime helps emphasize that the Pinnacle Parkway Extension is unnecessary on these lands, by clearly establishing that no other land use is envisioned for these lands. We understand that a future update is imminent on the Interim Control By-law applicable to the area and we request that at this time how best the lands can be zoned only for the cemetery use, as determined through the OLT appeal and as envisioned since 2001 by not only CCFS, but all levels of government.

Thank you for the opportunity to comment. Should you have any questions or require further information with respect to the above, please do not hesitate to contact the undersigned.

Thank you for your consideration.

Yours very truly,

MILLER THOMSON LLP

Per:



David Tang
Partner
DT/ac

c. Client
B. Bradt
MHBC

